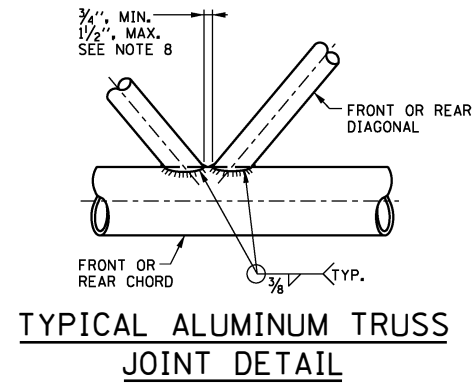
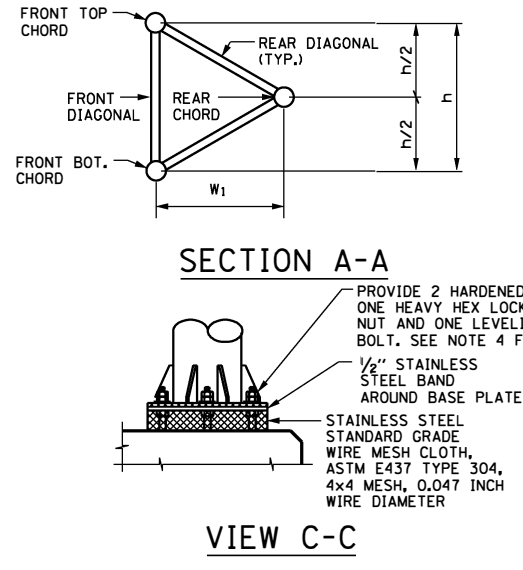
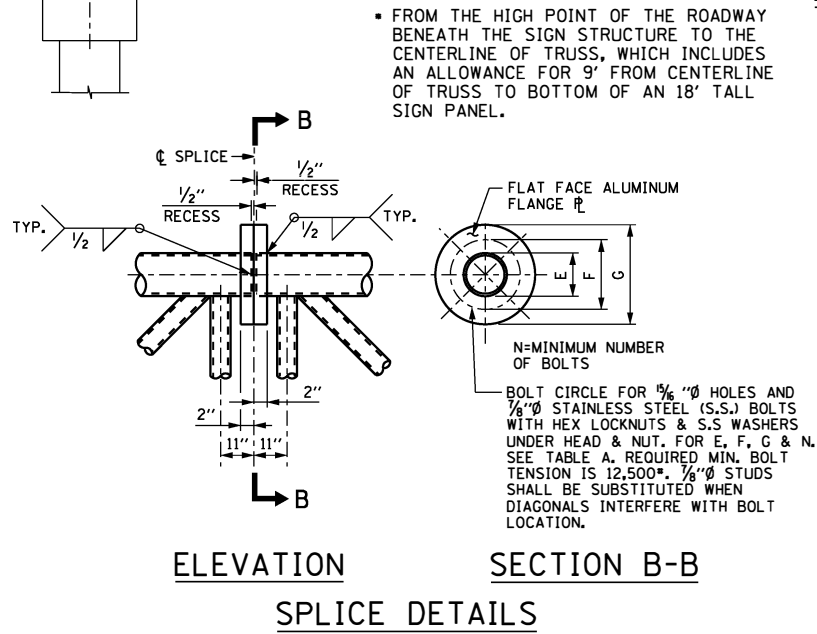
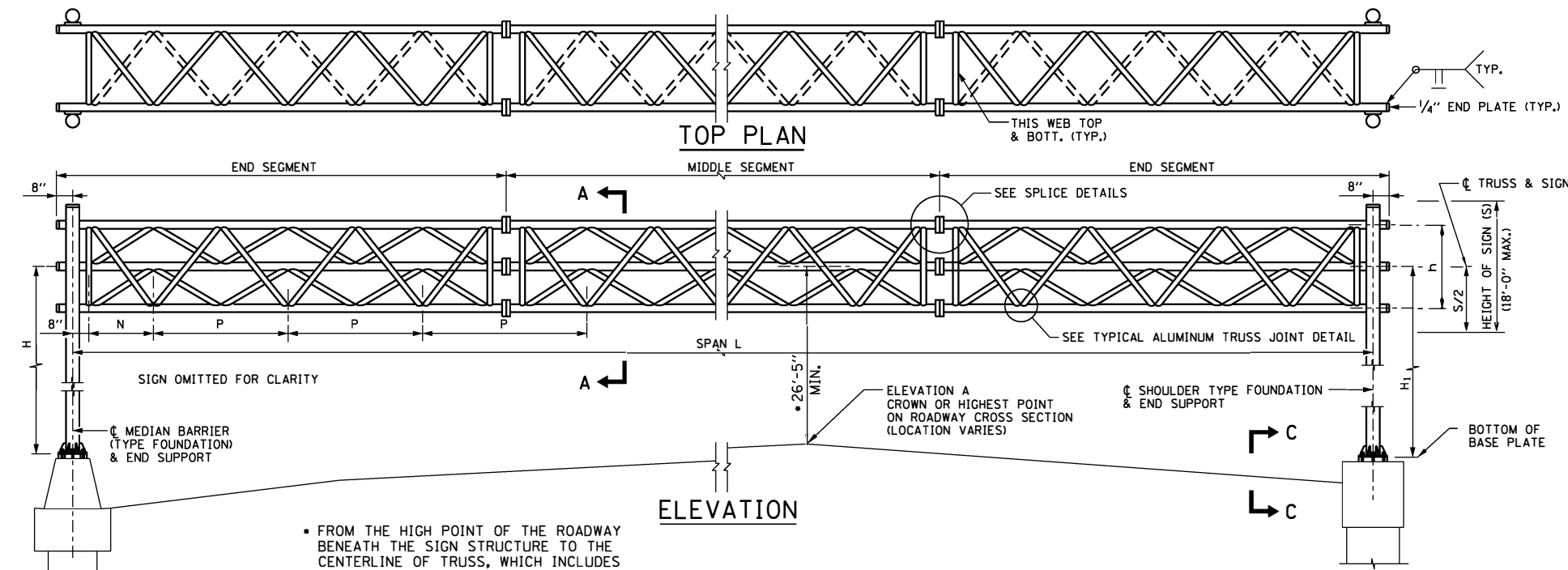


Illinois Tollway Standard Drawings Revisions

| Section F Sign Structure | | |
|--------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------|
| Drawing | Modification Summary | Effective: 03-01-2019 |
| F1 | OVERHEAD SIGN STRUCTURE SPAN TYPE STRUCTURE DETAILS | |
| Sheet 1 | Update barrier shape to constant slope. | |
| Sheet 3 | Revised anchor bolt length for taller barrier. Update X values "Design Table for Drilled Shafts in Cohesive Soils" and added Qu > 1.25 Ton/Sq. Ft. to the drilled shaft design table. | |
| Sheet 4 | Update barrier shape, details and quantities for constant slope. Update X values "Design Table for Drilled Shafts in Cohesive Soils" and added Qu > 1.25 Ton/Sq. Ft. to the drilled shaft design table. Revised callout and quantities for grade beam to be class SI concrete. | |
| Sheet 5 | Revised shape of t1(E) bar in Bar List table and updated barrier shape, details and quantities for constant slope. Update X values "Design Table for Drilled Shafts in Cohesive Soils" and added Qu > 1.25 Ton/Sq. Ft. to the drilled shaft design table. Revised callout and quantities for grade beam to be class SI concrete. | |
| F4 | OVERHEAD SIGN STRUCTURE CANTILEVER TYPE STRUCTURE DETAILS | |
| Sheet 7 | Update barrier shape to constant slope | |
| F8 | OVERHEAD SIGN STRUCTURE SIGN AND LUMINAIRE SUPPORTS | |
| Sheet 1 | Revised Note 2 reference to designer | |
| F9 | BREAKAWAY SIGN SUPPORT DETAILS | |
| Sheet 1 | Clarified the design stress for soil pressure and separated into a new category 'Foundation' | |
| F11 | MILEPOST MARKER | |
| Sheet 2 | Removed 'WALL' from Barrier Wall Mount Detail title | |
| F12 | MOUNTING DETAILS FOR RETROFITTING NEW EXIT SIGN PANELS | |
| Sheet 1 | This sheet has been removed from to Standard to the 720 OHS Base Sheets | |
| F13 | OVERHEAD SIGN STRUCTURE MONOTUBE TYPE (STEEL) MAINLINE STRUCTURE DETAILS | |
| Sheet 1 | Update barrier shape to constant slope. Revised dimension note for minimum clearance. | |
| Sheet 2 | Update barrier shape to constant slope. Revised dimension note for minimum clearance. | |
| Sheet 3 | Update barrier shape to constant slope. | |
| Sheet 6 | Update barrier shape and reinforcing details for constant slope. | |
| Sheet 7 | Update barrier shape and reinforcing details for constant slope. | |
| Sheet 8 | Update barrier shape and reinforcing details for constant slope. | |
| F14 | OVERHEAD SIGN STRUCTURE BUTTERFLY TYPE STRUCTURE DETAILS | |
| Sheet 5 | Revised Note 2 for protective coat to include the perimeter of the column | |
| Sheet 6 | Update barrier shape to constant slope. | |
| F15 | OVERHEAD SIGN STRUCTURE MONOTUBE TYPE (STEEL) STRUCTURE DETAILS FOR AET RAMP | |
| Sheet 1 | Update barrier shape to constant slope. Updated Elevation and table to indicate "I-Pass or Pay Online Sign" | |
| Sheet 2 | Update barrier shape to constant slope. Revised elevation to show square bar continuous. Updated dimension note for minimum clearance. | |
| Sheet 3 | Update barrier shape to constant slope. Updated dimension note for minimum clearance. | |
| Sheet 5 | Section H-H dimensions are updated. | |
| Sheet 6 | Update barrier shape and reinforcing details for constant slope. | |
| Sheet 7 | Update barrier shape and reinforcing details for constant slope. | |
| F16 | OVERHEAD SIGN STRUCTURE MONOTUBE TYPE (STEEL) STRUCTURE DETAILS FOR CASH-IPO RAMP | |
| Sheet 1 | Update barrier shape to constant slope. | |
| Sheet 2 | Update barrier shape to constant slope. Updated dimension note for minimum clearance. | |
| Sheet 5 | Update barrier shape and reinforcing details for constant slope. | |
| Sheet 6 | Update barrier shape and reinforcing details for constant slope. | |
| F17 | OVERHEAD SIGN STRUCTURE SPAN STYPE (STEEL) STRUCTURE DETAILS | |
| Sheet 8 | Update barrier shape to constant slope and revised transition length from 10'-0" to 21'-3". | |
| Sheet 9 | Update barrier shape to constant slope and revised transition length from 10'-0" to 21'-3". | |

 New Sheet

 Retired Standard



- ### GENERAL NOTES:
1. WORK THIS SHEET WITH OVERHEAD SIGN STRUCTURES SPAN TYPE SUMMARY AND TOTAL BILL OF MATERIAL.
 2. AFTER ADJUSTMENTS TO LEVEL TRUSS AND ENSURE ADEQUATE VERTICAL CLEARANCE, ALL TOP AND LEVELING NUTS SHALL BE TIGHTENED AGAINST THE BASE PLATE WITH A MINIMUM TORQUE OF 200 LB.-FT. STAINLESS STEEL MESH SHALL THEN BE PLACED AROUND THE PERIMETER OF THE BASE PLATE. SECURE TO BASE PLATE WITH STAINLESS STEEL BANDING.
 3. SIGN SUPPORT STRUCTURES MAY BE SUBJECT TO DAMAGING VIBRATIONS AND OSCILLATIONS WHEN SIGN PANELS ARE NOT IN PLACE DURING ERECTION OR MAINTENANCE OF THE STRUCTURE. TO AVOID THESE, ATTACH TEMPORARY BLANK SIGN PANELS OR OTHER BRACING TO THE STRUCTURE UNTIL PERMANENT SIGNS ARE INSTALLED.
 4. TRUSS SEGMENTS SHALL BE SHIPPED INDIVIDUALLY WITH ADEQUATE PROVISION TO PREVENT DETRIMENTAL MOTION DURING TRANSPORT. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE CONFIGURATION AND PROTECTION OF THE TRUSSES.
 5. ONLY SIGN PANELS ARE PERMITTED TO BE MOUNTED ON THIS TRUSS.

- ### DESIGN SPECIFICATIONS:
1. 2013 AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS, 6TH EDITION.

- ### CONSTRUCTION SPECIFICATIONS:
1. ALL MATERIALS, EXCEPT AS SHOWN, FABRICATION, ERECTION AND CONSTRUCTION REQUIREMENTS SHALL BE IN ACCORDANCE WITH SECTION 733 OF THE LATEST ILLINOIS TOLLWAY SUPPLEMENTAL SPECIFICATIONS.

- ### LOADING:
1. BOTH END SUPPORTS ARE DESIGNED FOR 60% OF THE TOTAL LOAD.
 2. WIND LOADING SHALL BE A MINIMUM OF 35 PSF ON SIGN PANELS AND 10 PSF ON GROSS AREAS DEFINED BY THE PERIMETER OF TRUSS MEMBERS NOT COVERED BY SIGN PANEL AREAS.
 3. THE AASHTO GROUP II AND III ALLOWABLE STRESS SHALL BE 133% (ALLOWABLE STRESS DESIGN).

- ### FABRICATION NOTES:
1. NO SPLICES SHALL BE LOCATED WITHIN 0.1xL OF THE CENTERLINE OF THE SPAN.
 2. MATERIALS: ALUMINUM SHALL CONFORM TO ASTM B221, ALLOY 6061 TEMPER T6. ALL STRUCTURAL STEEL PIPE SHALL BE ASTM A53 GRADE B OR A106 GRADE B. ALL STRUCTURAL STEEL PLATES AND SHAPES SHALL CONFORM TO AASHTO M270 GR. 36 OR GR. 50. STAINLESS STEEL FOR SHIMS, SLEEVES AND HANDHOLE COVERS SHALL BE ASTM A240, TYPE 302 OR 304, OR ANOTHER ALLOY SUITABLE FOR EXTERIOR EXPOSURE AND ACCEPTABLE TO THE ENGINEER. THE STEEL PIPE AND STIFFENING RIBS AT THE BASE PLATE FOR THE COLUMN SHALL HAVE A MINIMUM LONGITUDINAL CHARTY V-NOTCH (CVN) ENERGY OF 15 LB.-FT. AT 40° F. (ZONE 2) BEFORE GALVANIZING.
 3. WELDING: ALL WELDS TO BE CONTINUOUS UNLESS OTHERWISE SHOWN. ALL WELDING TO BE DONE IN ACCORDANCE WITH CURRENT AWS D1.1 AND D1.2 STRUCTURAL WELDING CODES (STEEL AND ALUMINUM) AND THE IDOT STANDARD SPECIFICATIONS. ALUMINUM WELD FILLER SHALL BE ALLOY 5556.
 4. FASTENERS FOR ALUMINUM TRUSSES: HIGH STRENGTH BOLTS SHALL SATISFY THE REQUIREMENTS OF AASHTO M164 (ASTM A325), OR APPROVED ALTERNATE, AND SHALL HAVE MATCHING LOCK NUTS. THREADED STUDS FOR SPLICES (IF MEMBERS INTERFERE) SHALL SATISFY THE REQUIREMENTS OF ASTM A449, ASTM A193, GRADE B7, OR APPROVED ALTERNATE, AND SHALL HAVE MATCHING LOCK NUTS. BOLTS AND LOCK NUTS NOT REQUIRED TO BE HIGH STRENGTH SHALL SATISFY THE REQUIREMENTS OF ASTM A307. ALL BOLTS AND LOCK NUTS SHALL BE HOT DIP GALVANIZED PER AASHTO M232, EXCEPT STAINLESS STEEL FASTENERS, NUTS AND WASHERS. THE LOCK NUTS SHALL HAVE NYLON OR STEEL INSERTS. A STAINLESS STEEL FLAT WASHER CONFORMING TO ASTM A240 TYPE 302 OR 304, IS REQUIRED UNDER BOTH HEAD AND NUT OR UNDER BOTH NUTS WHERE THREADED STUDS ARE USED. HIGH STRENGTH BOLT INSTALLATION SHALL CONFORM TO ARTICLE 505.04 (f)(2)(d) OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. ROTATIONAL CAPACITY ("ROCAP") TESTING OF BOLTS WILL NOT BE REQUIRED.
 5. U-BOLTS: U-BOLTS SHALL BE PRODUCED FROM ASTM A276 TYPE 304, 304L, 316 OR 316L, CONDITION A, COLD FINISHED STAINLESS STEEL, OR AN EQUIVALENT MATERIAL ACCEPTABLE TO THE ENGINEER. ALL NUTS FOR U-BOLTS SHALL BE LOCK NUTS EQUIVALENT TO ASTM A307 WITH NYLON OR STEEL INSERTS AND HOT DIP GALVANIZED PER AASHTO M232. A STAINLESS STEEL FLAT WASHER CONFORMING TO ASTM A240, TYPE 302 OR 304, IS REQUIRED UNDER EACH U-BOLT LOCK NUT.
 6. GALVANIZING: ALL STEEL GRATING, PLATES, SHAPES AND PIPE SHALL BE HOT DIP GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH AASHTO M111. PAINTING IS NOT PERMITTED.
 7. SEE TABLE "SIGN STRUCTURE MEMBER SCHEDULE" FOR "W" AND "W₁".
 8. DIAGONALS SHALL BE DETAILED TO MINIMIZE OFFSET FOR THEORETICAL PANEL POINT AND PROVIDE 3/4" TO 1 1/2" INCH CLEARANCE BETWEEN DIAGONALS AND PROVIDE CLEARANCE FOR U-BOLT CONNECTIONS OF SIGNS OR WALKWAY BRACKETS.
 9. FOR ANY DESIGN SPAN LENGTH THAT FALL BETWEEN TWO CONSECUTIVE SPANS, PROVIDED IN COLUMN 2 OF TABLE "SIGN STRUCTURE MEMBER SCHEDULE", THE LARGER DESIGN SPAN LENGTH SHALL BE USED (I.E. FOR A 92' SPAN LENGTH FALLING BETWEEN 90' AND 95' DESIGN SPAN LENGTHS IN TABLE, THE 95' DESIGN SPAN LENGTH TRUSS AND POST DETAILS SHALL BE USED).

| SIGN STRUCTURE MEMBER SCHEDULE | | | | | | | | | | | | | | |
|--------------------------------|---------------------|--------|--------|--------|----------------|-----------------------------------|-----------------------|-------------------------------|----------|-----------------|----------|-----------------------------------|--------------------------------|--------------------------|
| TRUSS NO. | D I M E N S I O N S | | | | | A L U M I N U M T R U S S * | | | | | | S T E E L E N D S U P P O R T | | |
| | TRUSS SPAN L | P | N | h | W ₁ | MAXIMUM ALLOWABLE SIGN PANEL AREA | DL (TRUSS) DEFLECTION | MIDDLE SEGMENT OR END SEGMENT | | | | W | PIPE COLUMN (NOMINAL DIAMETER) | |
| | | | | | | | | CHORD (O.D.) | | DIAGONAL (O.D.) | | | 10" X.X.S. (104.13*/FT.) | 12" X.X.S. (125.49*/FT.) |
| | | | | | | | | FRONT | REAR | FRONT | REAR | | | |
| T-80 | 80'-0" | 9'-0" | 3'-4" | 4'-6" | 3'-10¾" | 900 S.F. | 1" | 5½"ø x½" | 5½"ø x½" | 2½"ø x¼" | 2½"ø x¼" | 5'-9" | 32'-0" (MAX) | 38'-0" (MAX) |
| T-85 | 85'-0" | 9'-6" | 3'-10" | 4'-9" | 4'-1⅜" | 955 S.F. | 1/16" | 6⅞"ø x½" | 6⅞"ø x½" | 3"ø x¼" | 3"ø x¼" | 6'-7" | 31'-0" (MAX) | 38'-0" (MAX) |
| T-90 | 90'-0" | 10'-0" | 4'-4" | 5'-0" | 4'-4" | 1010 S.F. | 1/8" | 6⅞"ø x½" | 6⅞"ø x½" | 3"ø x¼" | 3"ø x¼" | 6'-7" | 31'-0" (MAX) | 38'-0" (MAX) |
| T-95 | 95'-0" | 10'-6" | 4'-10" | 5'-3" | 4'-6⅝" | 1065 S.F. | 1⅜" | 6⅞"ø x½" | 6⅞"ø x½" | 3"ø x¼" | 3"ø x¼" | 6'-7" | 31'-0" (MAX) | 38'-0" (MAX) |
| T-100 | 100'-0" | 11'-4" | 4'-0" | 5'-8" | 4'-10⅞" | 1125 S.F. | 1/4" | 7"ø x½" | 7"ø x½" | 3½"ø x¼" | 3½"ø x¼" | 7'-5" | 31'-0" (MAX) | 38'-0" (MAX) |
| T-105 | 105'-0" | 12'-0" | 3'-10" | 6'-0" | 5'-2⅜" | 1180 S.F. | 1⅝" | 7"ø x½" | 7"ø x½" | 3½"ø x¼" | 3½"ø x¼" | 7'-5" | 31'-0" (MAX) | 38'-0" (MAX) |
| T-110 | 110'-0" | 12'-6" | 4'-4" | 6'-3" | 5'-5" | 1200 S.F. | 1⅝" | 7"ø x½" | 7"ø x½" | 3½"ø x¼" | 3½"ø x¼" | 7'-5" | 31'-0" (MAX) | 38'-0" (MAX) |
| T-115 | 115'-0" | 13'-0" | 4'-10" | 6'-6" | 5'-7⅝" | 1200 S.F. | 1½" | 7½"ø x½" | 7½"ø x½" | 3½"ø x¼" | 3½"ø x¼" | 10'-2" | 34'-0" (MAX) | 40'-0" (MAX) |
| T-120 | 120'-0" | 13'-8" | 4'-8" | 6'-10" | 5'-11" | 1200 S.F. | 1⅝" | 7½"ø x½" | 7½"ø x½" | 3½"ø x¼" | 3½"ø x¼" | 10'-2" | 34'-0" (MAX) | 40'-0" (MAX) |
| T-130 | 130'-0" | 15'-0" | 4'-4" | 7'-6" | 6'-5⅝" | 1200 S.F. | 1⅝" | 9"ø x½" | 9"ø x½" | 4"ø x¼" | 4"ø x¼" | 10'-2" | NOT APPLICABLE | 40'-0" (MAX) |
| T-140 | 140'-0" | 16'-3" | 4'-4" | 8'-2" | 7'-0⅞" | 1200 S.F. | 1/16" | 10"ø x½" | 10"ø x½" | 4"ø x¼" | 4"ø x¼" | 10'-2" | NOT APPLICABLE | 40'-0" (MAX) |
| T-150 | 150'-0" | 17'-6" | 4'-4" | 8'-10" | 7'-7¾" | 1200 S.F. | 1⅜" | 11"ø x½" | 11"ø x½" | 4½"ø x¼" | 4½"ø x¼" | 10'-2" | NOT APPLICABLE | 40'-0" (MAX) |

| CAMBER | |
|--------------|------------------|
| SPAN IN FEET | CAMBER IN INCHES |
| 80 THRU 95 | 1 1/2" |
| 96 THRU 110 | 1 3/8" |
| 111 THRU 120 | 1 7/8" |
| 121 THRU 130 | 1 7/8" |
| 131 THRU 140 | 2" |
| 141 THRU 150 | 2 1/8" |

PROVIDE THE ABOVE CAMBER AT MIDDLE OF SPAN OF STRUCTURES

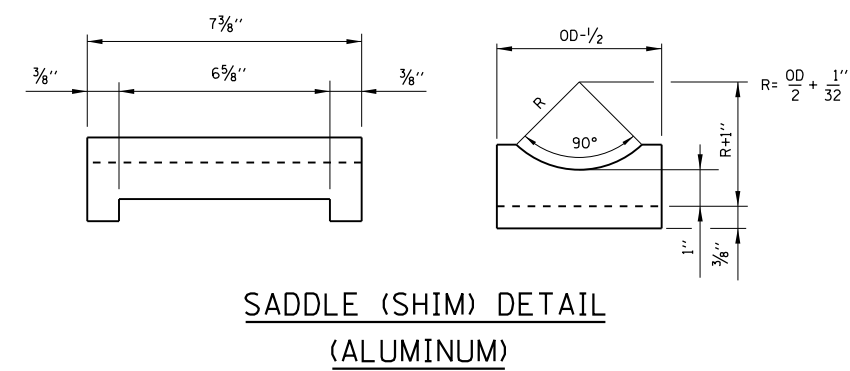
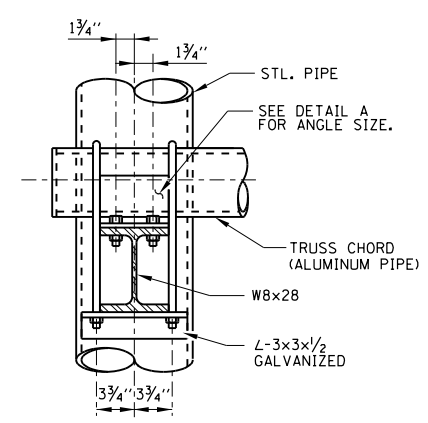
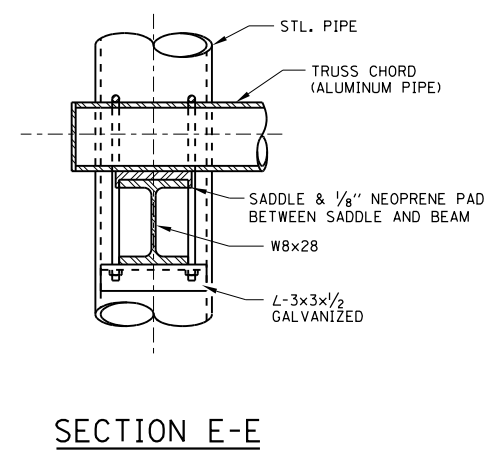
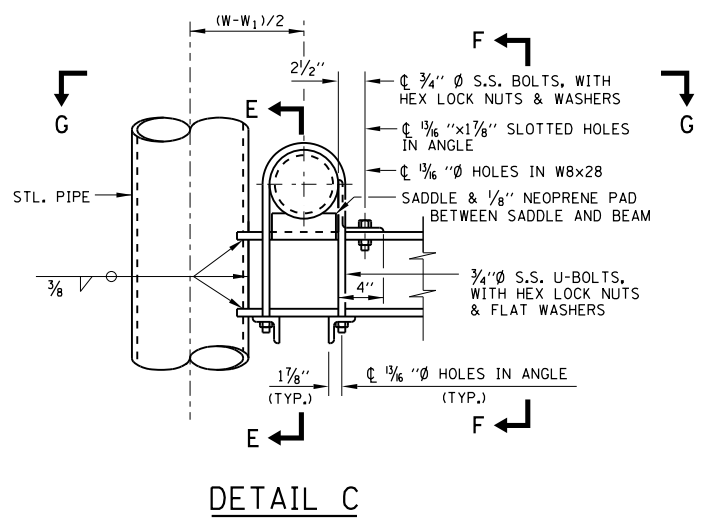
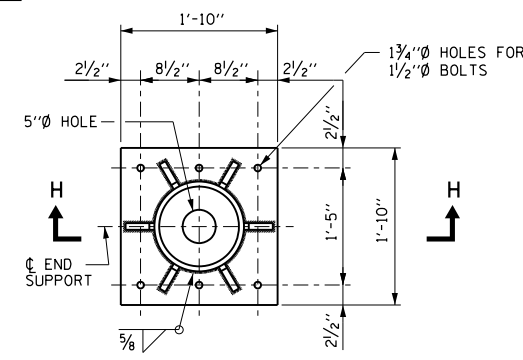
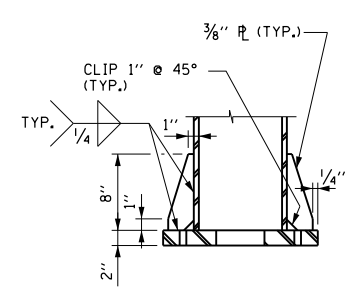
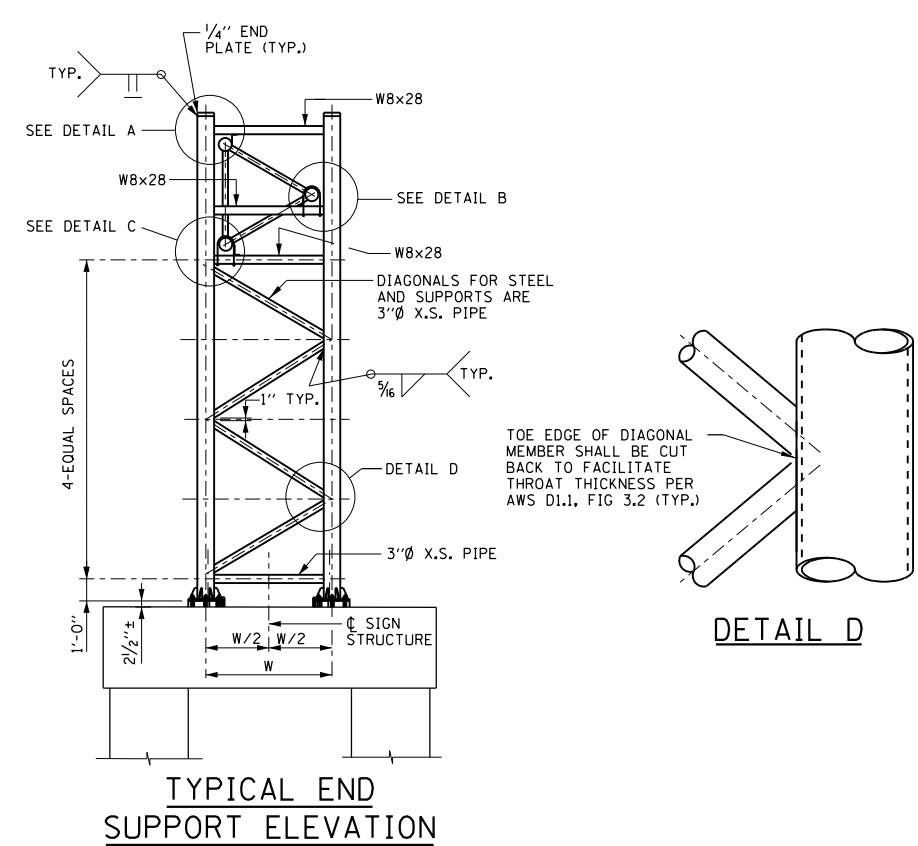
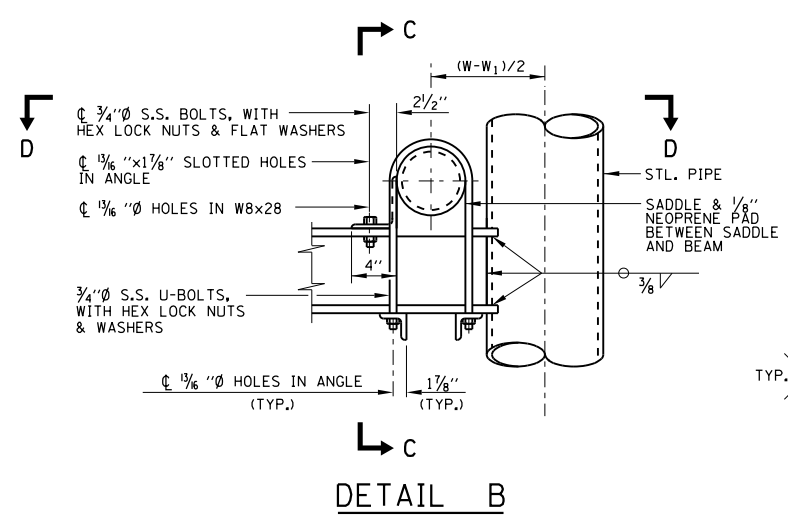
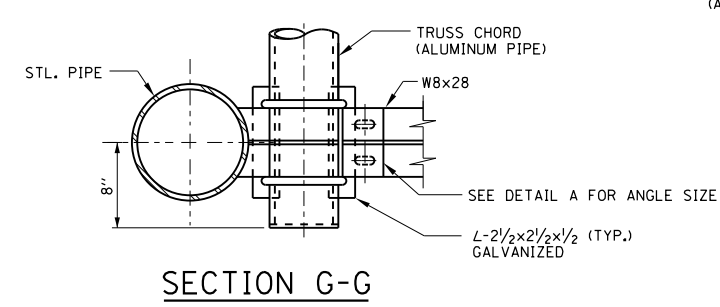
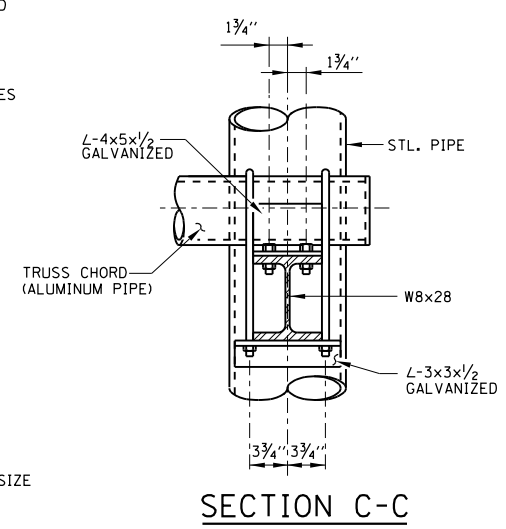
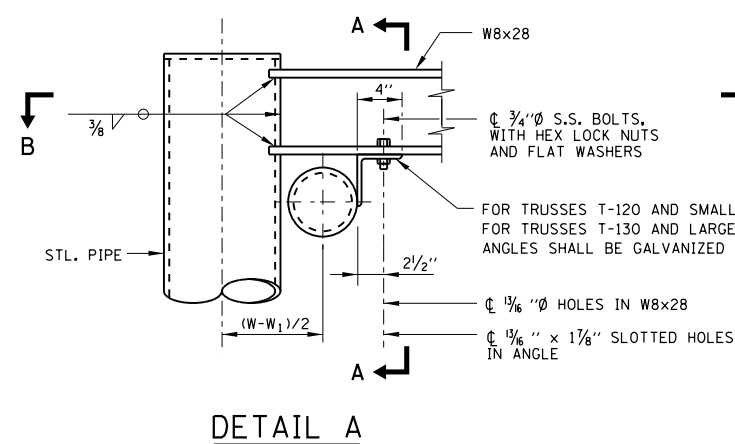
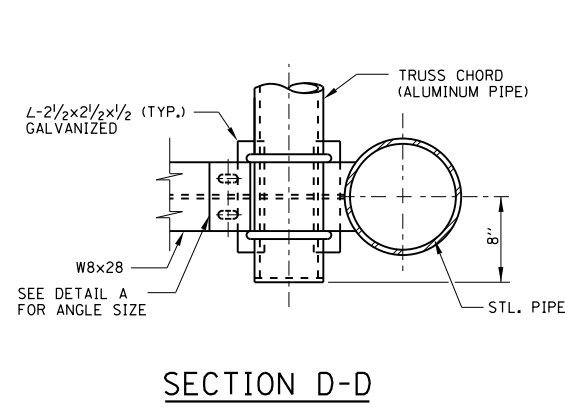
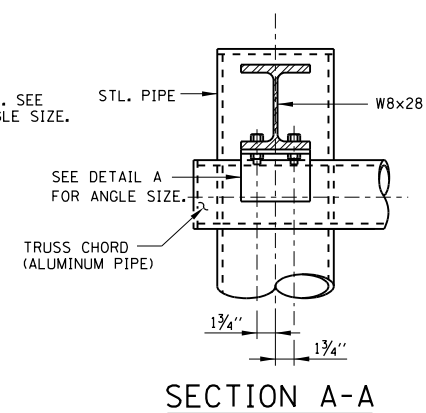
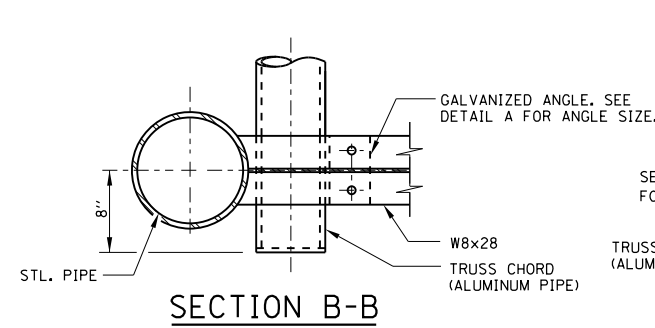
| TABLE A | | | |
|-----------------|---------|---------|----|
| CHORD O.D. | E | F | G |
| 5 1/2" φ | 10" | 13" | 8 |
| 6 3/8" φ & 7" φ | 11 1/2" | 14 1/2" | 10 |
| 7 1/2" φ | 12 1/2" | 15 1/2" | 12 |
| 9" φ | 13 1/2" | 16 1/2" | 14 |
| 10" φ | 15 1/2" | 18 1/2" | 16 |
| 11" φ | 17 1/2" | 20 1/2" | 18 |

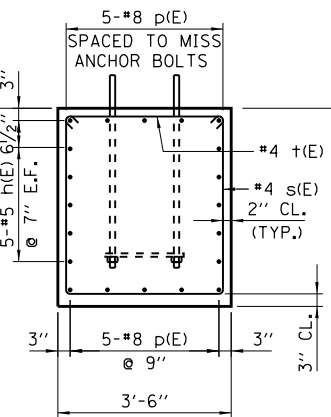
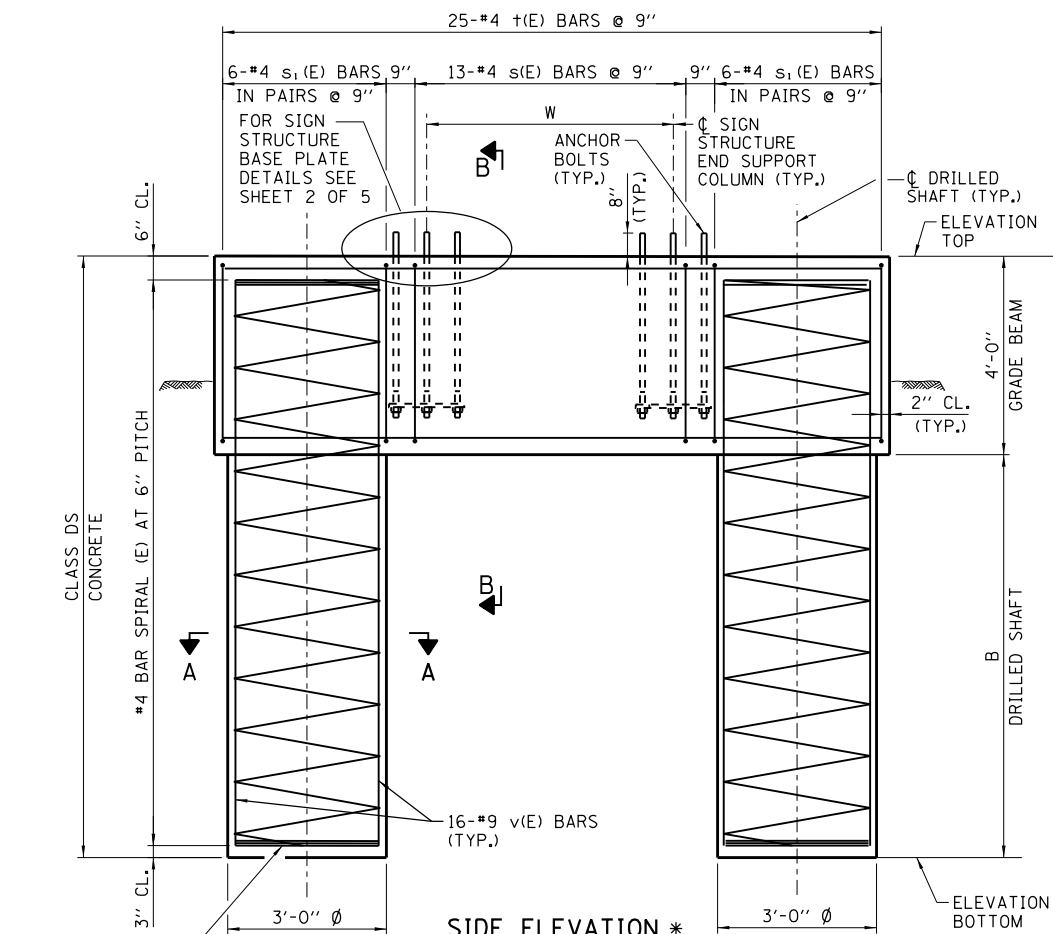
- ### NOTES:
1. XXS DENOTES DOUBLE EXTRA STRONG PIPE.
 2. A PAIR OF MAIN PIPE COLUMN SIZES FOR EACH SUPPORT SHALL BE SELECTED INDEPENDENTLY BASED ON SPECIFIC NEEDS.

APPROVED: *Paul Kovacs* DATE 2-07-2012
CHIEF ENGINEERING OFFICER

* SUBSTITUTION OF LARGER TRUSS SIZE IS ACCEPTABLE.

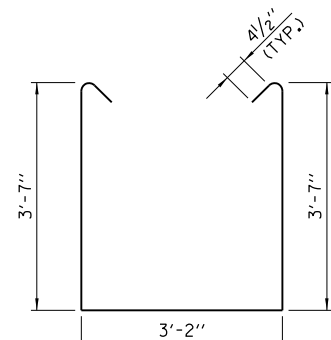
| DATE | REVISIONS |
|-----------|------------------------------------------------------------------------|
| 7-01-2014 | REVISED FOUNDATION CONCRETE. |
| 3-11-2015 | REVISED NOTES. |
| 3-31-2016 | REVISED FOUNDATION NOTE AND REVISED BASE PLATE DIMENSIONS. |
| 3-31-2017 | COLUMN MEMBER ADJUSTMENTS AND FOUNDATION REINFORCEMENT. |
| 3-01-2018 | REVISED VER. CLEARANCE, AND ADDED NOTE |
| 3-01-2019 | UPDATE BARRIER SHAPE, CHANGED GRADE BEAM TO CLASS II CONCRETE, REVISED |
| | +1(E) BAR IN BAR LIST |
| | ADDED NOTE 9 FOR DESIGN SPAN LENGTH |
| 2-13-2020 | ADDED WASHER & NUTS CALLOUT-VIEW C-C. |



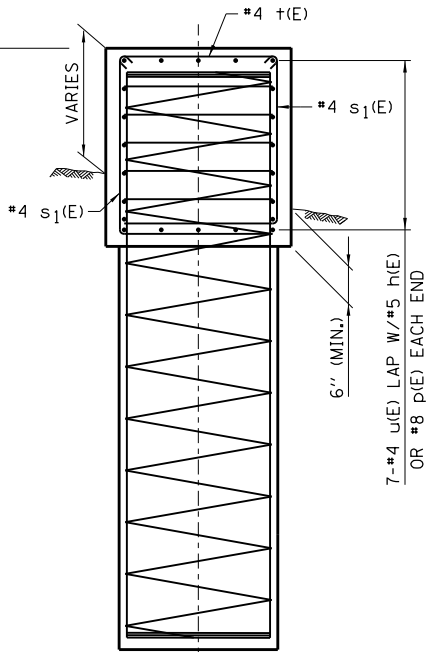


SECTION B-B

* REINFORCEMENT IN GRADE BEAM NOT SHOWN FOR CLARITY



BAR s (E)



END VIEW

NOTES:

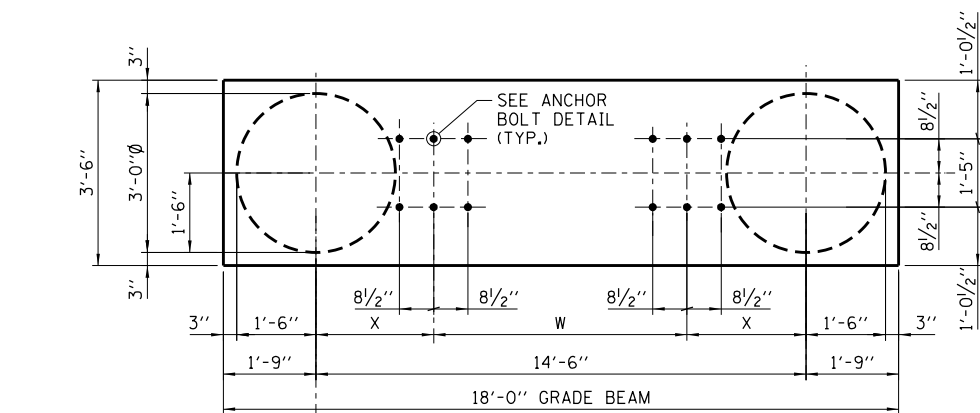
1. THE FOUNDATION DETAILS SHOWN ARE BASED ON THE PRESENCE OF MOSTLY COMMON COHESIVE SOIL CONDITIONS (SILTY OR SANDY CLAY), WITH AN AVERAGE UNCONFINED COMPRESSIVE STRENGTH (QU) > 1.25 TON/SQ. FT. WHICH SHALL BE DETERMINED BY PREVIOUS SOIL INVESTIGATIONS AT THE JOBSITE. WHEN OTHER CONDITIONS ARE INDICATED, THE FOUNDATION DIMENSIONS SHOWN SHALL BE INCLUDED IN THE PLANS AND THE FOUNDATION DIMENSIONS SHOWN SHALL BE THE RESULT OF SITE SPECIFIC DESIGNS. IF CONDITIONS ENCOUNTERED IN THE FIELD ARE DIFFERENT THAN THOSE INDICATED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER TO DETERMINE IF THE FOUNDATION DIMENSIONS NEED TO BE MODIFIED.
2. ANCHOR BOLTS SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M314 OR ASTM F1554 GRADE 55, WITH A MINIMUM TENSILE STRENGTH OF 75,000 PSI. ALL OTHER MATERIAL, FABRICATION, AND CONSTRUCTION REQUIREMENTS SHALL BE IN ACCORDANCE WITH SECTION 734 OF THE ILLINOIS TOLLWAY SUPPLEMENTAL SPECIFICATIONS.
3. CONCRETE SHALL BE PLACED MONOLITHICALLY, WITHOUT CONSTRUCTION JOINTS UNLESS NOTED OTHERWISE.
4. BACKFILL SHALL BE PLACED PER SECTION 502 OF THE IDOT STANDARD SPECIFICATION AND PRIOR TO ERECTION OF SUPPORT COLUMN.
5. A NORMAL SURFACE FINISH FOLLOWED BY A CONCRETE SEALER APPLICATION WILL BE REQUIRED ON CONCRETE SURFACES ABOVE THE LOWEST ELEVATION 6" BELOW FINISHED GROUND LINE.
6. ALL REBAR DESIGNATED (E) SHALL BE EPOXY COATED. REBAR SHALL BE POSITIONED SO THAT THERE WILL BE NO INTERFERENCE BETWEEN VERTICAL REINFORCEMENT AND ANCHOR BOLTS.
7. SITE GROUNDING ELECTRODE SYSTEM TO BE PROVIDED AS INDICATED ON THE PLANS.
8. NO SONOTUBES OR DECOMPOSABLE FORMS SHALL BE USED 6" BELOW THE FINISHED GROUND LINE. PERMANENT METAL FORMS OR OTHER SHIELDING SHALL NOT BE LEFT IN PLACE BELOW THE ELEVATION WITHOUT THE ENGINEER'S WRITTEN PERMISSION. EXCAVATIONS SHALL BE DEWATERED BEFORE CONCRETE PLACEMENT IF DIRECTED BY THE ENGINEER AT NO ADDITIONAL COST.
9. IF NECESSARY TO INCREASE STEEL END SUPPORT HEIGHT ABOVE THE LIMITATIONS SHOWN IN SIGN STRUCTURE MEMBER SCHEDULE ON SHEET 1 OF THIS SERIES, GRADE BEAM DEPTH SHALL BE INCREASED UP TO 6'-0" WITHOUT CHANGES TO THE DRILLED SHAFT DESIGN. GRADE BEAM REINFORCEMENT, CONCRETE VOLUME AND LENGTH OF ANCHOR BOLTS SHALL BE REVISED ACCORDINGLY.

| DESIGN TABLE FOR DRILLED SHAFTS IN COHESIVE SOILS (QU > 1.25 TON/SQ. FT.) | | | | | |
|---------------------------------------------------------------------------|--------|------------|--------|-------------------|-------------------|
| TRUSS No. | W | X | B | CLASS DS CONC. CY | REINF. BARS POUND |
| T-80 | 5'-9" | 4'-4 1/2" | 40'-0" | 30.3 | 6650 |
| T-85 | 6'-7" | 3'-11 1/2" | 50'-0" | 35.5 | 7940 |
| T-90 | 6'-7" | 3'-11 1/2" | 50'-0" | 35.5 | 7940 |
| T-95 | 6'-7" | 3'-11 1/2" | 50'-0" | 35.5 | 7940 |
| T-100 | 7'-5" | 3'-6 1/2" | 50'-0" | 35.5 | 7940 |
| T-105 | 7'-5" | 3'-6 1/2" | 50'-0" | 35.5 | 7940 |
| T-110 | 7'-5" | 3'-6 1/2" | 50'-0" | 35.5 | 7940 |
| T-115 | 10'-2" | 2'-2" | 50'-0" | 35.5 | 7940 |
| T-120 | 10'-2" | 2'-2" | 50'-0" | 35.5 | 7940 |
| T-130 | 10'-2" | 2'-2" | 55'-0" | 38.1 | 8590 |
| T-140 | 10'-2" | 2'-2" | 55'-0" | 38.1 | 8590 |
| T-150 | 10'-2" | 2'-2" | 55'-0" | 38.1 | 8590 |

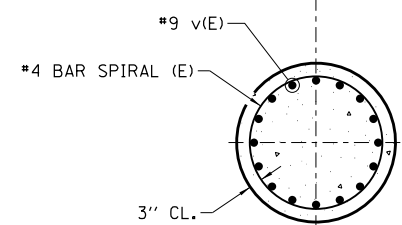
BAR LIST - EACH FOUNDATION (2 SHAFT AND 1 GRADE BEAM)

| BAR | NUMBER | SIZE | LENGTH | SHAPE |
|--------|--------|------|-------------|-------|
| h (E) | 10 | #5 | 17'-8" | — |
| p (E) | 10 | #8 | 17'-8" | — |
| s (E) | 13 | #4 | 11'-1" | ┌┐ |
| s1 (E) | 24 | #4 | 6'-11 1/2" | ┌┐ |
| + (E) | 25 | #4 | 3'-11" | ┌┐ |
| u (E) | 14 | #4 | 7'-0" | ┌┐ |
| v (E) | 32 | #9 | B ADD 3'-3" | — |

*4 BAR SPIRAL (E) - SEE SIDE ELEVATION

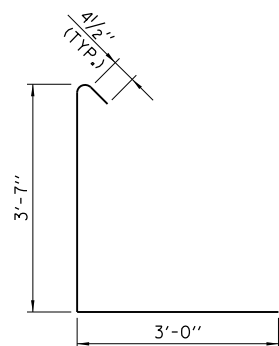


PLAN *

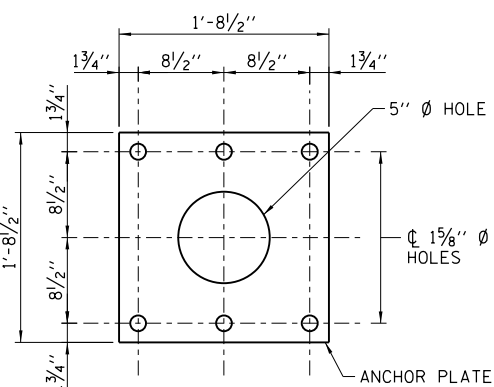


(TYPICAL BOTH SHAFTS)

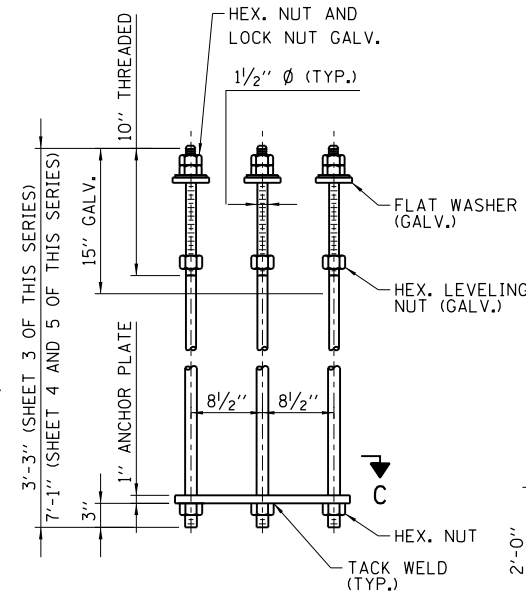
SECTION A-A



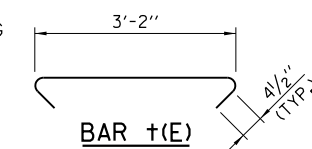
BAR s1 (E)



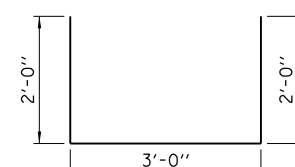
SECTION C-C



ANCHOR BOLT DETAIL
(TYP. FOR ALL FOUNDATIONS)

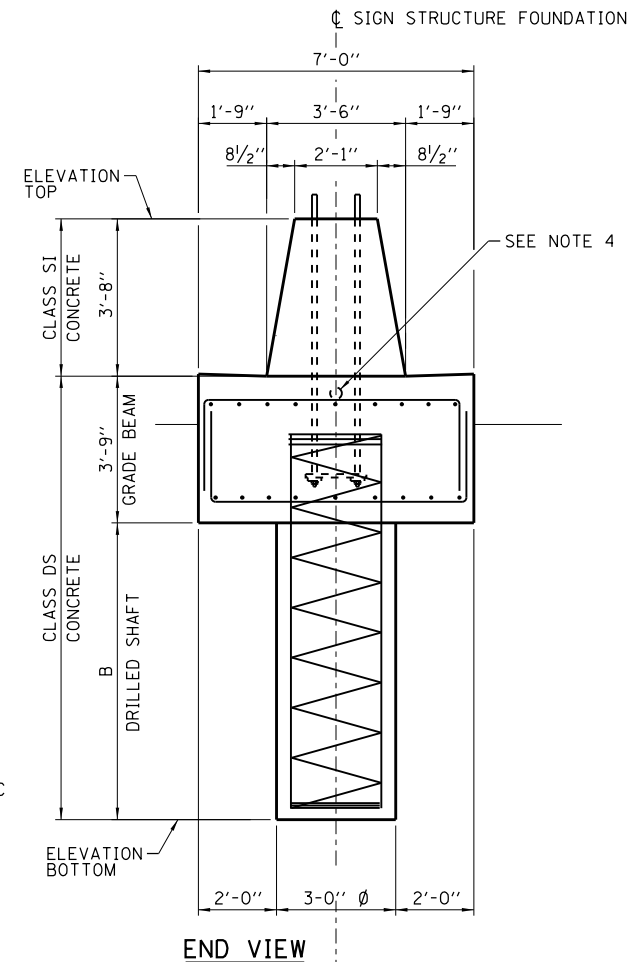
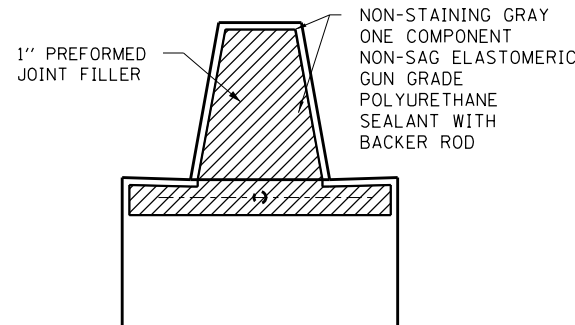
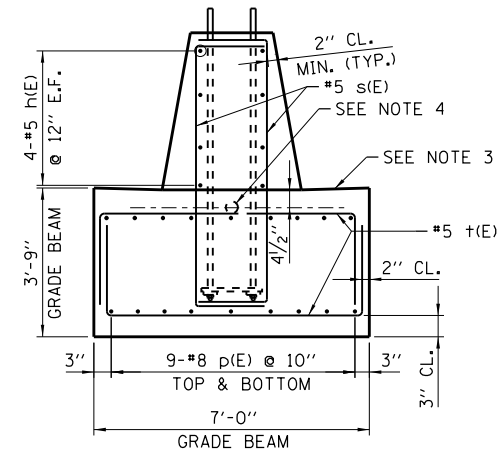
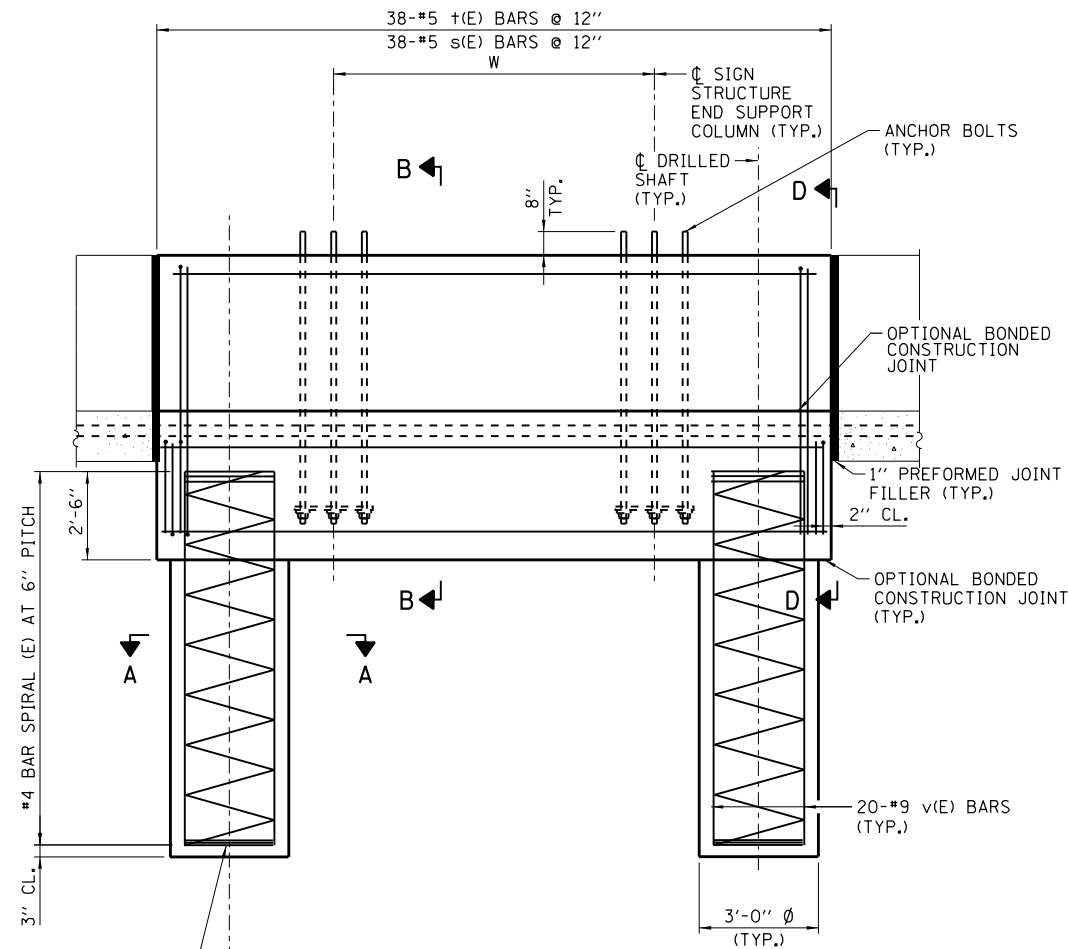


BAR + (E)



BAR u (E)

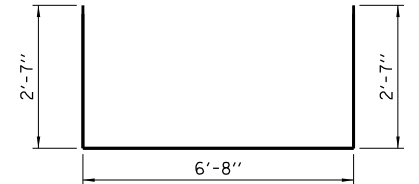
APPROVED... *Paul Kovacs* DATE 2-7-2012
CHIEF ENGINEERING OFFICER



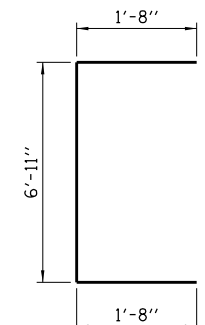
BAR LIST - EACH FOUNDATION

| BAR | NUMBER | SIZE | LENGTH | SHAPE |
|------|--------|------|-------------|-------|
| h(E) | 8 | #5 | 17'-8" | |
| p(E) | 18 | #8 | 17'-8" | |
| s(E) | 38 | #5 | 10'-3" | C |
| t(E) | 38 | #5 | 11'-10" | |
| v(E) | 40 | #9 | B ADD 2'-3" | |

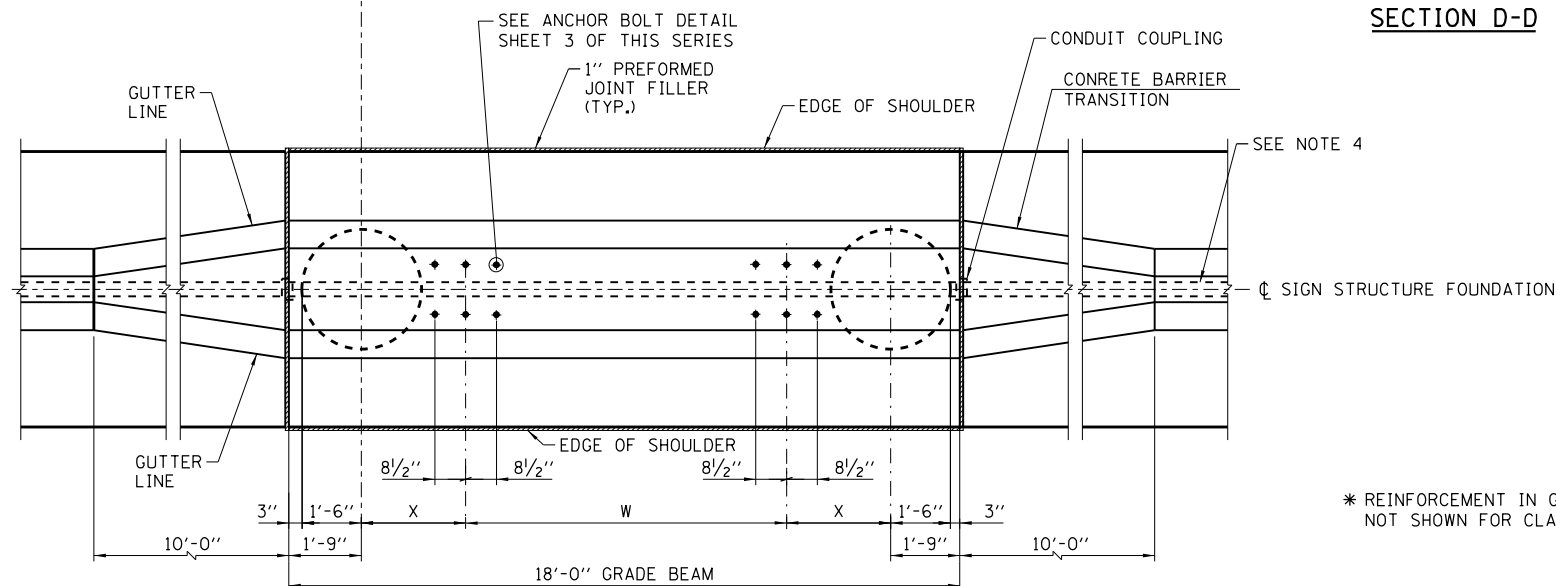
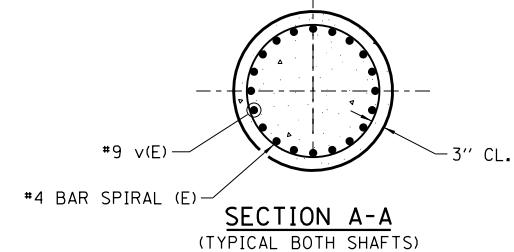
#4 BAR SPIRAL (E) - SEE SIDE ELEVATION



BAR t(E)



BAR s(E)



PLAN *
(REINFORCEMENT NOT SHOWN FOR CLARITY)

* REINFORCEMENT IN GRADE BEAM
NOT SHOWN FOR CLARITY

| DESIGN TABLE FOR DRILLED SHAFTS IN COHESIVE SOILS ($C_u > 1.25$ TON/SQ. FT.) | | | | | | | |
|-------------------------------------------------------------------------------|--------|------------|--------|------------------------|------------------------|-------------------|-------------------------|
| TRUSS No. | W | X | B | CLASS DS CONC. CU. YD. | CLASS SI CONC. CU. YD. | REINF. BARS POUND | PROTECTIVE COAT SQ. YD. |
| T-80 | 5'-9" | 4'-4 1/2" | 50'-0" | 43.7 | 6.8 | 10110 | 26.1 |
| T-85 | 6'-7" | 3'-11 1/2" | 55'-0" | 46.3 | 6.8 | 10890 | 26.1 |
| T-90 | 6'-7" | 3'-11 1/2" | 55'-0" | 46.3 | 6.8 | 10890 | 26.1 |
| T-95 | 6'-7" | 3'-11 1/2" | 55'-0" | 46.3 | 6.8 | 10890 | 26.1 |
| T-100 | 7'-5" | 3'-6 1/2" | 55'-0" | 46.3 | 6.8 | 10890 | 26.1 |
| T-105 | 7'-5" | 3'-6 1/2" | 55'-0" | 46.3 | 6.8 | 10890 | 26.1 |
| T-110 | 7'-5" | 3'-6 1/2" | 55'-0" | 46.3 | 6.8 | 10890 | 26.1 |
| T-115 | 10'-2" | 2'-2" | 55'-0" | 46.3 | 6.8 | 10890 | 26.1 |
| T-120 | 10'-2" | 2'-2" | 55'-0" | 46.3 | 6.8 | 10890 | 26.1 |
| T-130 | 10'-2" | 2'-2" | 55'-0" | 46.3 | 6.8 | 10890 | 26.1 |
| T-140 | 10'-2" | 2'-2" | 55'-0" | 46.3 | 6.8 | 10890 | 26.1 |
| T-150 | 10'-2" | 2'-2" | 55'-0" | 46.3 | 6.8 | 10890 | 26.1 |

NOTES:

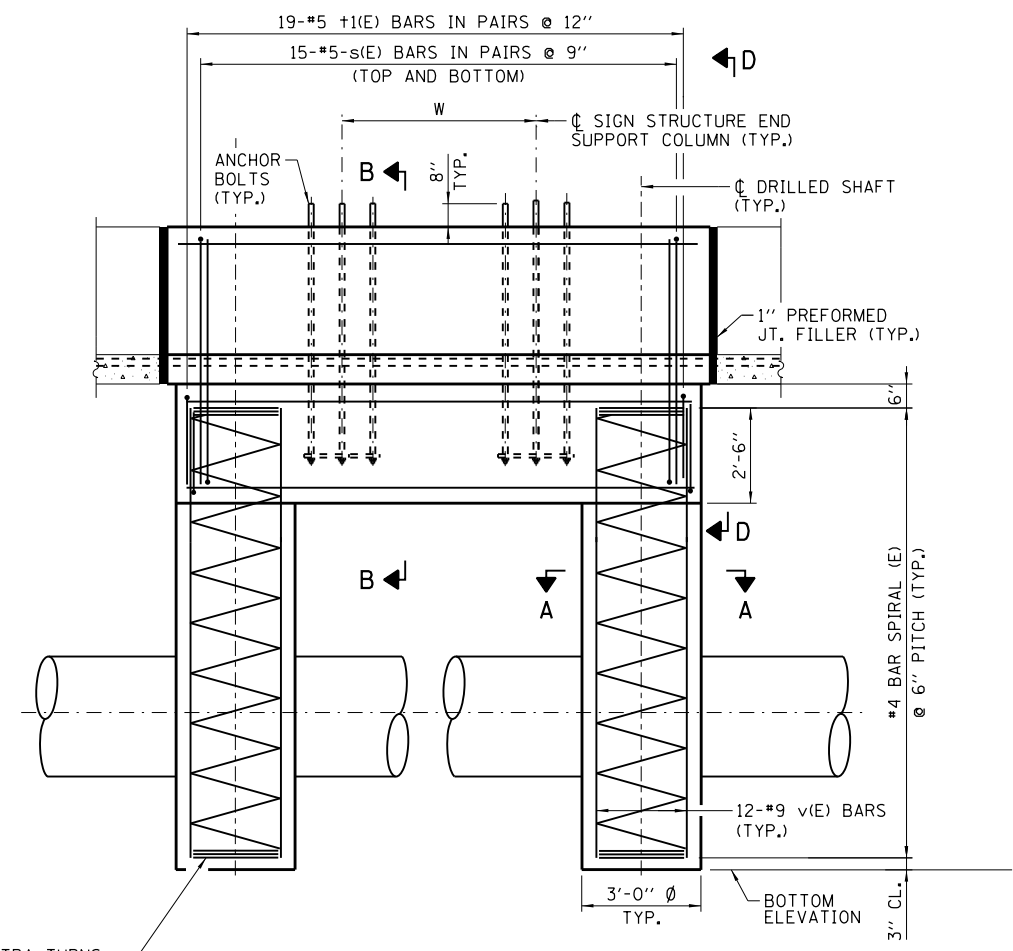
- SEE SHEET 3 OF THIS SERIES FOR GENERAL NOTES AND DESIGN CRITERIA.
- FOR SIGN STRUCTURE BASE PLATE DETAIL, SEE SHEET 2 OF THIS SERIES.
- REFERENCE ILLINOIS TOLLWAY STANDARD DRAWING C5 FOR GUTTER SLOPE.
- COORDINATE CONDUIT SIZE, LOCATION AND QUANTITY WITH ELECTRICAL PLANS, CONDUITS SHALL BE PLACED TO MISS REINFORCEMENT BARS, DO NOT CUT REINFORCEMENT BARS.
- PROTECTIVE COAT SHALL BE APPLIED TO THE TRAFFIC AND TOP FACES OF THE BARRIER AND TOP FACE OF GUTTER.



OVERHEAD SIGN STRUCTURE
SPAN TYPE
STRUCTURE DETAILS

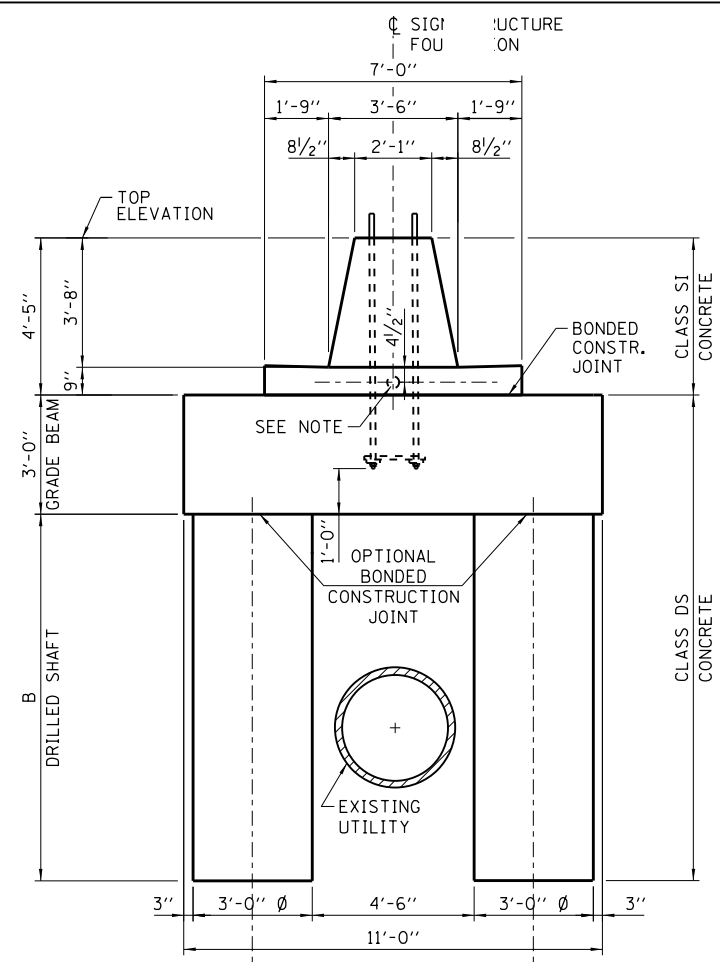
STANDARD F1-10

APPROVED: *Paul Kovacs* DATE: 2-7-2012
CHIEF ENGINEERING OFFICER

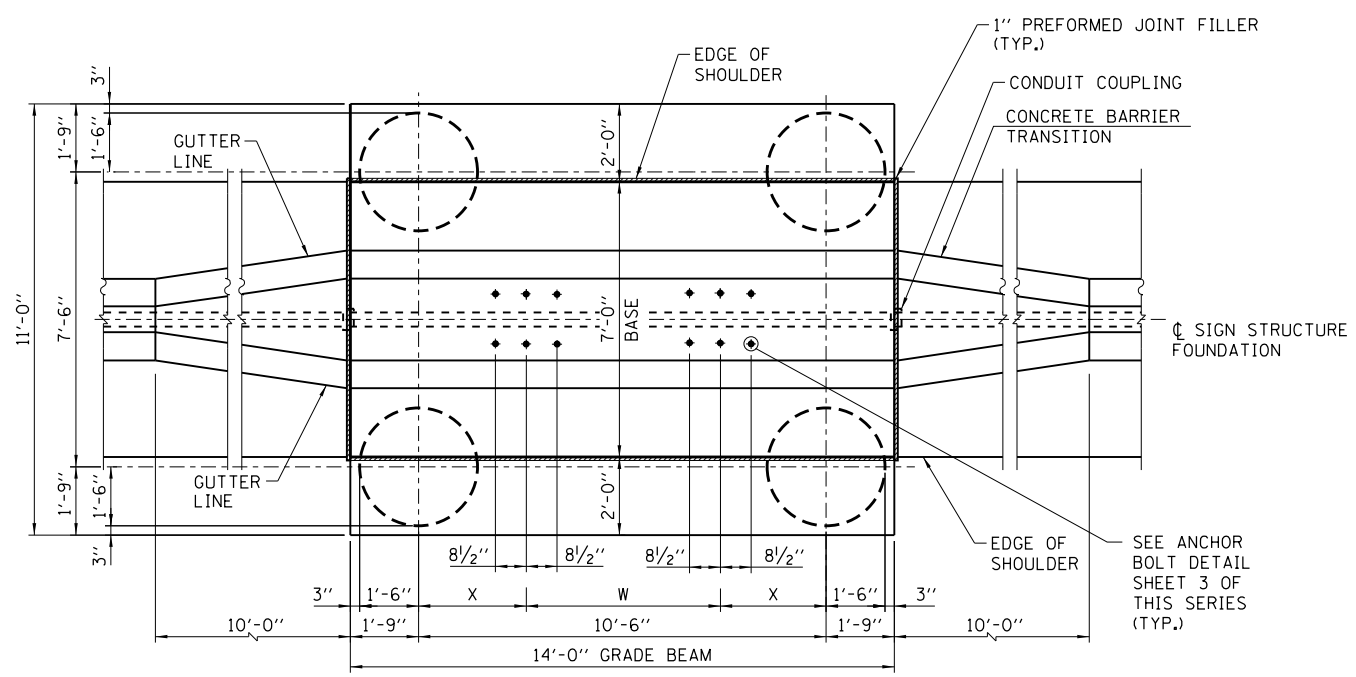


SIDE ELEVATION *

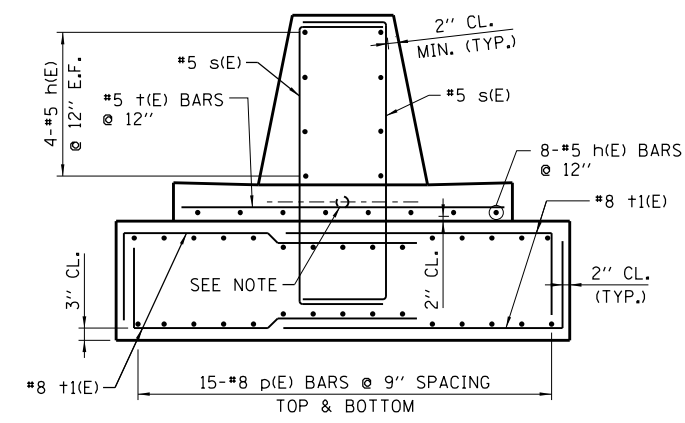
* REINFORCEMENT IN GRADE BEAM NOT SHOWN FOR CLARITY



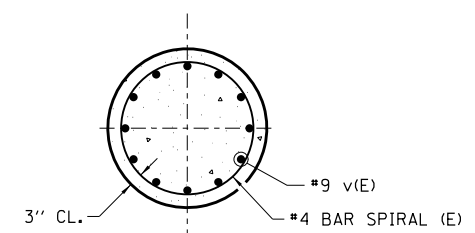
END VIEW



PLAN *



SECTION B-B



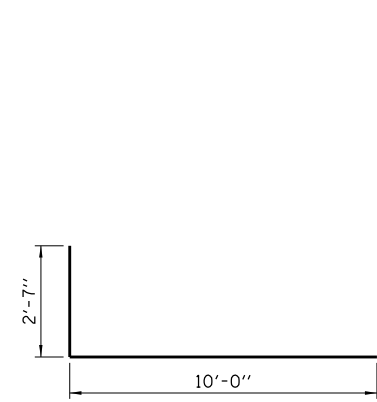
SECTION A-A
(TYPICAL FOR 4 SHAFTS)

BAR LIST - EACH FOUNDATION

| BAR | NUMBER | SIZE | LENGTH | SHAPE |
|-------|--------|------|--------------|-------|
| h(E) | 16 | #5 | 13'-8" | — |
| p(E) | 30 | #8 | 13'-8" | — |
| s(E) | 38 | #5 | 10'-3" | C |
| +1(E) | 15 | #5 | 6'-8" | — |
| +1(E) | 60 | #8 | 12'-7" | — |
| v(E) | 48 | #9 | B. ADD 2'-3" | — |

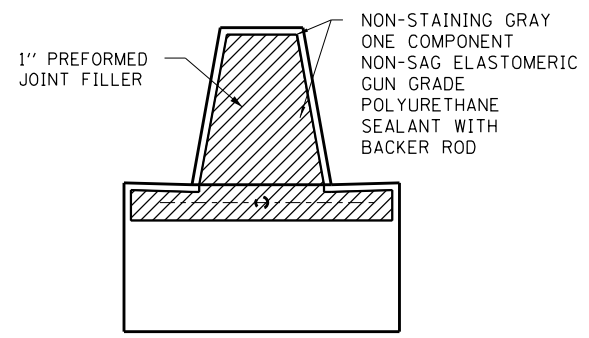
*4 BAR SPIRAL (E) - SEE SIDE ELEVATION

| DESIGN TABLE FOR DRILLED SHAFTS IN COHESIVE SOILS (QU > 1.25 TON/SQ. FT.) | | | | | | | |
|---------------------------------------------------------------------------|--------|------------|--------|------------------------|------------------------|-------------------|-------------------------|
| TRUSS No. | W | X | B | CLASS DS CONC. CU. YD. | CLASS SI CONC. CU. YD. | REINF. BARS POUND | PROTECTIVE COAT SQ. YD. |
| T-80 | 5'-9" | 2'-4 1/2" | 25'-0" | 43.3 | 8.0 | 9950 | 20.3 |
| T-85 | 6'-7" | 1'-11 1/2" | 25'-0" | 43.3 | 8.0 | 9950 | 20.3 |
| T-90 | 6'-7" | 1'-11 1/2" | 25'-0" | 43.3 | 8.0 | 9950 | 20.3 |
| T-95 | 6'-7" | 1'-11 1/2" | 25'-0" | 43.3 | 8.0 | 9950 | 20.3 |
| T-100 | 7'-5" | 1'-6 1/2" | 25'-0" | 43.3 | 8.0 | 9950 | 20.3 |
| T-105 | 7'-5" | 1'-6 1/2" | 30'-0" | 48.5 | 8.0 | 10980 | 20.3 |
| T-110 | 7'-5" | 1'-6 1/2" | 30'-0" | 48.5 | 8.0 | 10980 | 20.3 |
| T-115 | 10'-2" | 0'-2" | 30'-0" | 48.5 | 8.0 | 10980 | 20.3 |
| T-120 | 10'-2" | 0'-2" | 30'-0" | 48.5 | 8.0 | 10980 | 20.3 |
| T-130 | 10'-2" | 0'-2" | 30'-0" | 48.5 | 8.0 | 10980 | 20.3 |
| T-140 | 10'-2" | 0'-2" | 30'-0" | 48.5 | 8.0 | 10980 | 20.3 |
| T-150 | 10'-2" | 0'-2" | 30'-0" | 48.5 | 8.0 | 10980 | 20.3 |



BAR +1(E)

BAR s(E)



SECTION D-D



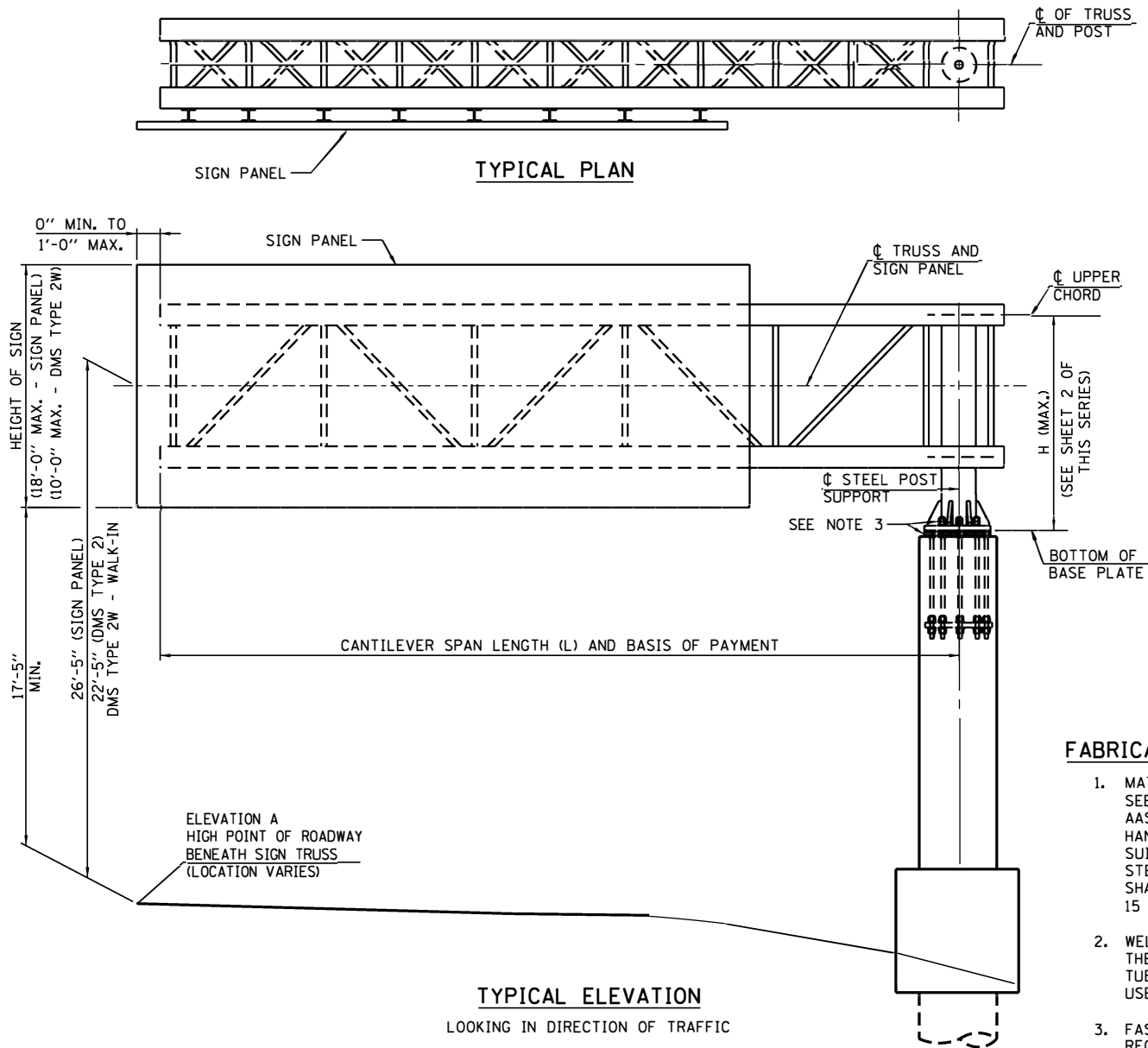
OVERHEAD SIGN STRUCTURE
SPAN TYPE
STRUCTURE DETAILS

STANDARD F1-10

NOTE:

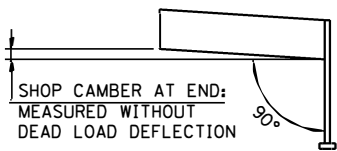
1. SEE NOTES ON SHEET 4 OF THIS SERIES.

APPROVED: *Paul Kovacs* DATE: 2-7-2012
CHIEF ENGINEERING OFFICER



SHOP CAMBER TABLE

| CANTILEVER LENGTH (L) | SHOP CAMBER AT END |
|-----------------------|--------------------|
| 20' | 1 1/2" |
| 25' | 1 1/2" |
| 30' | 2" |
| 35' | 2 1/2" |
| 40' | 2 1/2" |
| 45' | 3" |
| 50' | 3 1/2" |



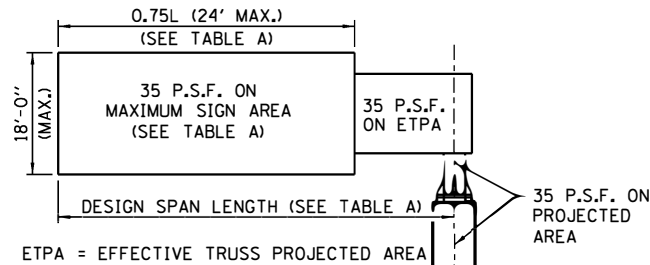
CAMBER DIAGRAM
(FOR FABRICATION ONLY)

TABLE B: MATERIAL SPECIFICATIONS
FOR STRUCTURAL STEEL AND FASTENERS

| ELEMENT OF STRUCTURE | SPECIFICATION | MINIMUM YIELD STRENGTH (K.S.I.) | MINIMUM ULTIMATE STRENGTH (K.S.I.) |
|--------------------------------|--------------------------------|---------------------------------|------------------------------------|
| STRUCTURAL STEEL TUBE | ASTM A500 GRADE B | 46 | 58 |
| STRUCTURAL STEEL POST AND PIPE | API 5L GRADE B OR X42 OR X52 | 35 | 52 |
| | ASTM A106 GRADE B | 35 | 60 |
| | ASTM A53, TYPE E OR S, GRADE B | 35 | 60 |
| STEEL BAR AND STEEL PLATES | ASTM A572 GRADE 50 | 50 | 65 |
| STAINLESS STEEL BOLTS | ASTM A193, CLASS 1, GRADE B8 | 30 | 75 |
| STRUCTURAL STEEL BOLTS | ASTM 325 TYPE 1 | -- | 105 |
| STAINLESS STEEL LOCKNUTS | ASTM A194 GRADE 8F | -- | -- |
| NUTS | ASTM A194 GRADE 2H | -- | -- |
| STEEL WASHERS | ASTM A563 GRADE DH | -- | -- |
| STAINLESS STEEL WASHERS | ASTM F436 | -- | -- |
| STEEL ANCHOR BOLTS | ASTM A240, TYPE 302 | -- | -- |
| | AASHTO M314 OR ASTM F1554 | 55 | 75 |

TABLE A: MAXIMUM LIMITS FOR SIGNS

| TRUSS TYPE | DESIGN SPAN LENGTH (FT.) | MAXIMUM SIGN AREA (SQ. FT.) | MAXIMUM SIGN LENGTH (FT.) |
|------------|--------------------------|-----------------------------|---------------------------|
| 20-D | 20 | 270 | 15 |
| 25-D | 25 | 338 | 18.75 |
| 30-D | 30 | 405 | 22.5 |
| 35-D | 35 | 432 | 24 |
| 40-D | 40 | 432 | 24 |
| 45-D | 45 | 432 | 24 |
| 50-D | 50 | 432 | 24 |



DESIGN WIND LOADING DIAGRAM

FABRICATION NOTES:

- MATERIALS: FOR MATERIAL SPECIFICATIONS FOR CANTILEVER SIGN STRUCTURES, SEE TABLE B. ALL STRUCTURAL STEEL PLATES AND SHAPES SHALL CONFORM TO AASHTO M270 GR. 50. STAINLESS STEEL FOR SHIMS, SLEEVES AND HANDHOLE COVERS SHALL BE ASTM A240, TYPE 302 OR 304 OR ANOTHER ALLOY SUITABLE FOR EXTERIOR EXPOSURE AND ACCEPTABLE TO THE ENGINEER. THE STEEL PIPE AND STIFFENING RIBS AT THE BASE PLATE FOR THE STEEL POST SHALL HAVE A MINIMUM LONGITUDINAL CHARTY V-NOTCH (CVN) ENERGY OF 15 LB.-FT. AT 40° F (ZONE 2) BEFORE GALVANIZING.
- WELDING: ALL MATERIALS, WELDING PROCEDURES AND INSPECTION USED FOR THE CANTILEVER OVERHEAD SIGN STRUCTURE SHALL CONFORM TO AWS D1.1-10 FOR TUBULAR, CYCLICALLY LOADED STRUCTURES. ADDITIONALLY, ALL WELDED MATERIALS USED SHALL BE PREQUALIFIED FOR USE WITH WPS AS PER AWS D1.1-10, TABLE 3.1.
- FASTENERS FOR STEEL TRUSSES: HIGH STRENGTH BOLTS SHALL SATISFY THE REQUIREMENTS OF AASHTO M164 (ASTM A325), OR APPROVED ALTERNATE, AND SHALL HAVE MATCHING LOCKNUTS. THREADED STUDS FOR SPLICES (IF MEMBERS INTERFERE) SHALL SATISFY THE REQUIREMENTS OF ASTM A449. ASTM A193 GRADE B7, OR APPROVED ALTERNATE, AND SHALL HAVE MATCHING LOCKNUTS. BOLTS AND LOCKNUTS NOT REQUIRED TO BE HIGH STRENGTH SHALL SATISFY THE REQUIREMENTS OF ASTM A307. ALL BOLTS AND LOCKNUTS SHALL BE HOT DIP GALVANIZED PER AASHTO M232, EXCEPT STAINLESS STEEL FASTENERS, NUTS AND WASHERS. THE LOCKNUTS SHALL HAVE NYLON OR STEEL INSERTS. A STAINLESS STEEL FLAT WASHER CONFORMING TO ASTM A240 TYPE 302 OR 304, IS REQUIRED UNDER BOTH HEAD AND NUT OR UNDER BOTH NUTS WHERE THREADED STUDS ARE USED. HIGH STRENGTH BOLT INSTALLATION SHALL CONFORM TO ARTICLE 505.04(f)(2)d OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. ROTATIONAL CAPACITY ("ROCAP") TESTING OF BOLTS WILL NOT BE REQUIRED.
- U-BOLTS: U-BOLTS SHALL BE PRODUCED FROM ASTM A276 TYPE 304, 304L, 316 OR 316L, CONDITION A, COLD FINISHED STAINLESS STEEL, OR AN EQUIVALENT MATERIAL ACCEPTABLE TO THE ENGINEER. ALL NUTS FOR U-BOLTS SHALL BE LOCKNUTS EQUIVALENT TO ASTM A307 WITH NYLON OR STEEL INSERTS AND HOT DIP GALVANIZED PER AASHTO M232. A STAINLESS STEEL FLAT WASHER CONFORMING TO ASTM A240, TYPE 302 OR 304, IS REQUIRED UNDER EACH U-BOLT LOCKNUT.
- GALVANIZING: ALL PLATES, SHAPES AND PIPE SHALL BE HOT DIP GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH AASHTO M111. PAINTING IS NOT PERMITTED. ALL FASTENERS SHALL BE HOT DIP GALVANIZED IN ACCORDANCE WITH AASHTO M111 OR M232 AS APPROPRIATE FOR THE PRODUCT (EXCEPT STAINLESS STEEL FASTENERS).

GENERAL NOTES:

- WORK THIS SHEET WITH OVERHEAD SIGN STRUCTURE CANTILEVER TYPE SUMMARY AND TOTAL BILL OF MATERIAL SHEET.
- AFTER ADJUSTMENTS TO LEVEL TRUSS AND ENSURE ADEQUATE VERTICAL CLEARANCE, ALL TOP AND LEVELING NUTS SHALL BE TIGHTENED AGAINST THE BASE PLATE WITH A MINIMUM TORQUE OF 200 LB.-FT. STAINLESS STEEL MESH SHALL THEN BE PLACED AROUND THE PERIMETER OF THE BASE PLATE. SECURE TO BASE PLATE WITH STAINLESS STEEL BANDING.
- SIGN SUPPORT STRUCTURES MAY BE SUBJECT TO DAMAGING VIBRATIONS AND OSCILLATIONS WHEN SIGN PANELS ARE NOT IN PLACE DURING ERECTION OR MAINTENANCE OF THE STRUCTURE. TO AVOID THESE, ATTACH TEMPORARY BLANK SIGN PANELS OR OTHER BRACING TO THE STRUCTURE UNTIL PERMANENT SIGNS ARE INSTALLED.
- TRUSSES SHALL BE SHIPPED INDIVIDUALLY WITH ADEQUATE PROVISION TO PREVENT DETRIMENTAL MOTION DURING TRANSPORT. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE CONFIGURATION AND PROTECTION OF THE TRUSSES.
- ALL WELDS SHALL BE CONTINUOUS UNLESS OTHERWISE SHOWN. ALL WELDING SHALL BE DONE IN ACCORDANCE WITH CURRENT AWS D1.1 STRUCTURE WELDING CODE AND THE STANDARD SPECIFICATIONS.
- ALL STEEL PLATES, SHAPES AND PIPE SHALL BE HOT DIP GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH AASHTO M111.
- PROVIDE RUBBED SURFACE FINISH FOLLOWED BY CONCRETE SEALER APPLICATION ON ENTIRE SURFACE OF CONCRETE COLUMN AND NORMAL SURFACE FINISH ON GRADE BEAM, EXCEPT BOTTOM SURFACE.
- REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
- DMS TYPE 2W - WALK-IN IS PERMITTED TO BE INSTALLED ON CANTILEVER TRUSS. DO NOT INSTALL SIGN PANEL IN CONJUNCTION WITH DMS TYPE 2W - WALK-IN. SEE SHEET 9 OF THIS SERIES FOR PERMISSIBLE SIGN SIZE AND WEIGHT.

CONSTRUCTION SPECIFICATIONS:

- ALL MATERIALS, EXCEPT AS SHOWN, FABRICATION, ERECTION AND CONSTRUCTION REQUIREMENTS SHALL BE IN ACCORDANCE WITH SECTION 733 OF THE LATEST ILLINOIS TOLLWAY SUPPLEMENTAL SPECIFICATIONS.

LOADING:

- ALL CANTILEVER TRUSSES ARE DESIGNED FOR AN 18'-0" DEEP SIGN PANEL OVER 75% OF THE ARM LENGTH, WITH A MAXIMUM PANEL WIDTH OF 24'-0".
- ALL CANTILEVER TRUSSES ARE DESIGNED FOR 35 PSF WIND PRESSURE ON TRUSS MEMBERS AND SIGN PANEL.
- THE AASHTO GROUP II AND III ALLOWABLE STRESS SHALL BE 133% (ALLOWABLE STRESS DESIGN).

DESIGN SPECIFICATIONS:

THESE STRUCTURES ARE DESIGNED TO SATISFY THE 2013 AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS, SIXTH EDITION.

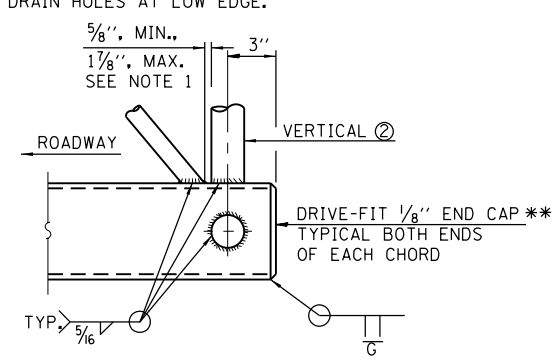
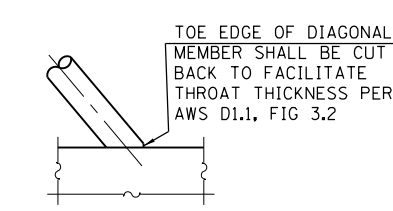
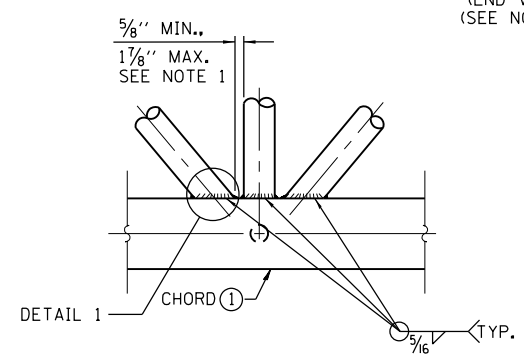
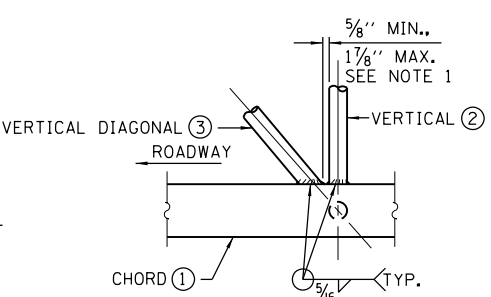
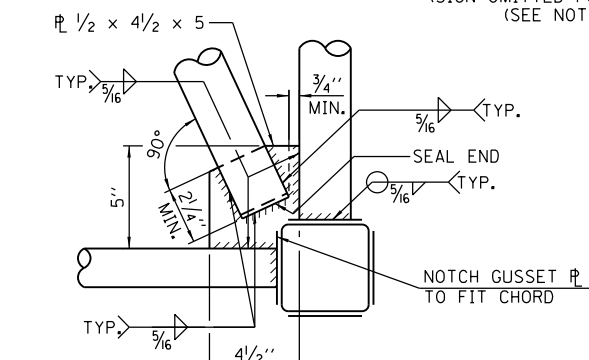
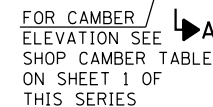
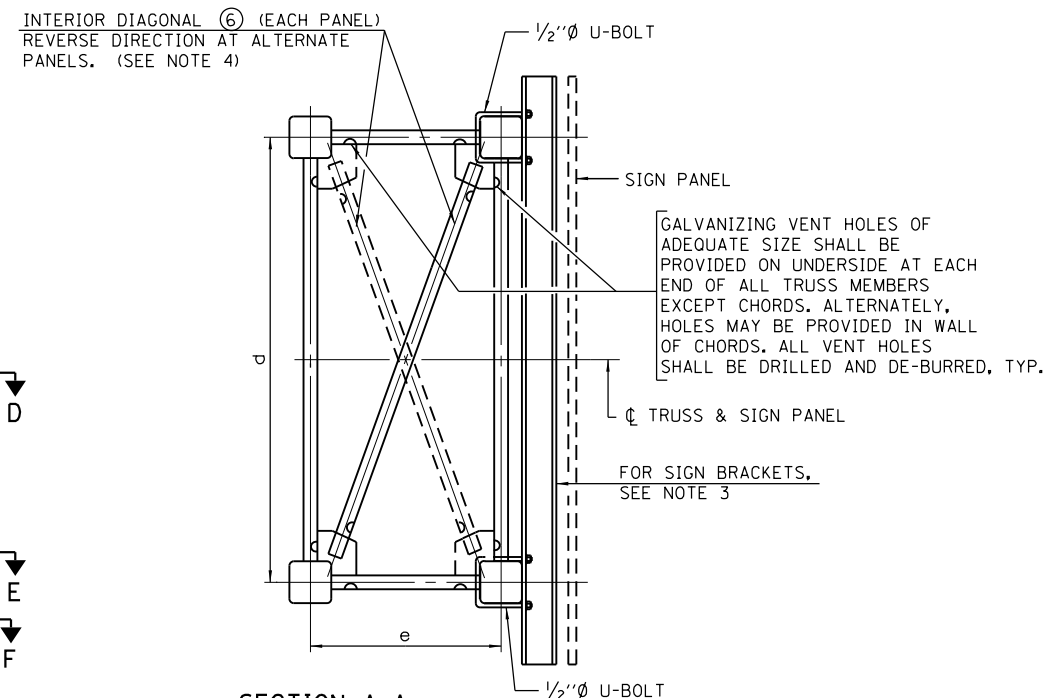
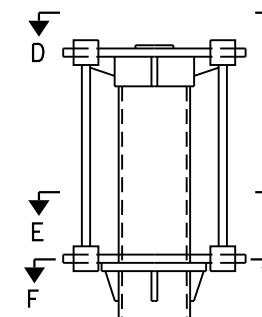
CONCRETE COLUMN, GRADE BEAM AND DRILLED SHAFT ARE DESIGNED IN ACCORDANCE WITH THE 2012 EDITION OF THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS (INCLUDING THE 2013 INTERIM REVISIONS).

DESIGN UNIT STRESSES FOR REINFORCED CONCRETE:

CLASS SI CONCRETE ----- f'c = 3,500 P.S.I.
CLASS DS CONCRETE ----- f'c = 4,000 P.S.I.
REINFORCING STEEL ----- fy = 60,000 P.S.I.



| DATE | REVISIONS |
|------------|-------------------------------------|
| 12-12-2013 | REVISED TABLES AND NOTES |
| 2-07-2014 | REVISED STEEL POST TO CONCRETE |
| 3-31-2014 | ADDED DMS TYPE II |
| 7-01-2014 | ADDED DIMENSIONS AND REVISED NOTES |
| 3-11-2015 | ADDED DIMENSIONS AND REVISED NOTES |
| 3-31-2016 | REVISED FOUNDATION NOTE |
| 3-31-2017 | ADDED WALKWAY GRATING DETAILS |
| 3-01-2018 | ADDED VERTICAL CLEARANCE |
| 3-01-2019 | UPDATED BARRIER SHAPE |
| 2-13-2020 | UPDATED TABLE C. ANCHOR BOLT DETAIL |



CANTILEVER END JOINT DETAIL

TABLE C: TRUSS AND POST DETAILS FOR 18'-0" (MAX.) SIGN HEIGHT

| DESIGN SPAN LENGTH (L) | TRUSS TYPE | TRUSS SIZE | | ACTUAL SPAN LENGTH | MAXIMUM SIGN LENGTH | STEEL SUPPORT POST (COLUMN) | | | | TRUSS MEMBERS AND DETAILS | | | | | | | | | | | | | |
|------------------------|------------|------------|-------|--------------------|---------------------|-----------------------------|---------------|------------------|----------|---------------------------|------------|--------|------------------|--------|--------------|--------|--------------------|--------|------------------|--------|--------|--------|-------|
| | | | | | | DIAMETER | WEIGHT | * WALL THICKNESS | H (MAX.) | TOP & BOTTOM CHORD ① | VERTICAL ② | | VERTICAL DIAG. ③ | | HORIZONTAL ④ | | HORIZONTAL DIAG. ⑤ | | INTERIOR DIAG. ⑥ | | PANELS | | |
| | | PIPE | WALL | | | | | | | | PIPE | WALL | PIPE | WALL | PIPE | WALL | PIPE | WALL | NO. | P | S | | |
| 20' | 20-D | 2'-6" | 5'-6" | 20'-1" | 15'-0" | 18" | 138.30 (#/FT) | ¾" | 12'-0" | HSS 5x5x¼ | 2½"Ø X.S | 0.276" | 3"Ø X.X.S | 0.600" | 1½"Ø X.S | 0.200" | 2½"Ø X.S | 0.276" | 1½"Ø X.S | 0.200" | 4 | 4'-7" | 1'-6" |
| 25' | 25-D | 3'-6" | 5'-6" | 24'-11" | 18'-9" | 18" | 181.73 (#/FT) | 1" | 12'-0" | HSS 5x5x¼ | 2½"Ø X.S | 0.276" | 3"Ø X.X.S | 0.600" | 2"Ø X.S | 0.218" | 2½"Ø X.S | 0.276" | 2"Ø X.S | 0.218" | 5 | 4'-7" | 1'-9" |
| 30' | 30-D | 3'-6" | 7'-0" | 30'-2" | 22'-6" | 18" | 181.73 (#/FT) | 1" | 12'-0" | HSS 6x6x¼ | 3"Ø X.S | 0.300" | 4"Ø X.X.S | 0.674" | 2"Ø X.S | 0.218" | 2½"Ø X.S | 0.276" | 2"Ø X.S | 0.218" | 5 | 5'-7" | 2'-0" |
| 35' | 35-D | 4'-0" | 7'-0" | 35'-0" | 24'-0" | 24" | 186.41 (#/FT) | ¾" | 12'-0" | HSS 6x6x¼ | 3"Ø X.S | 0.300" | 4"Ø X.X.S | 0.674" | 2"Ø X.S | 0.218" | 2½"Ø X.S | 0.276" | 2"Ø X.S | 0.218" | 5 | 6'-6" | 2'-3" |
| 40' | 40-D | 4'-0" | 7'-0" | 40'-0" | 24'-0" | 24" | 186.41 (#/FT) | ¾" | 12'-0" | HSS 6x6x¼ | 3"Ø X.S | 0.300" | 4"Ø X.X.S | 0.674" | 2"Ø X.S | 0.218" | 2½"Ø X.S | 0.276" | 2"Ø X.S | 0.218" | 6 | 6'-3" | 2'-3" |
| 45' | 45-D | 4'-6" | 7'-0" | 45'-0½" | 24'-0" | 24" | 245.87 (#/FT) | 1" | 12'-0" | HSS 6x6x¼ | 3"Ø X.S | 0.300" | 4"Ø X.X.S | 0.674" | 2"Ø X.S | 0.218" | 2½"Ø X.S | 0.276" | 2"Ø X.S | 0.218" | 7 | 6'-0½" | 2'-6" |
| 50' | 50-D | 4'-6" | 7'-0" | 50'-1" | 24'-0" | 24" | 245.87 (#/FT) | 1" | 12'-0" | HSS 6x6x¼ | 3"Ø X.S | 0.300" | 4"Ø X.X.S | 0.674" | 2"Ø X.S | 0.218" | 2½"Ø X.S | 0.276" | 2"Ø X.S | 0.218" | 8 | 5'-11" | 2'-6" |

* NOMINAL WALL THICKNESS SHOWN. THICKER WALL IS PERMITTED UPON ENGINEER'S APPROVAL.

NOTES:

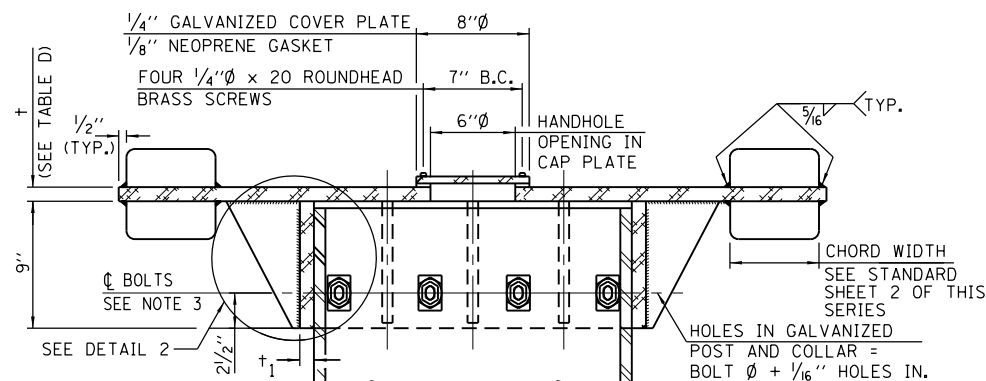
1. TRUSS MEMBERS SHALL BE SPACED A MINIMUM OF 3 TIMES THE WALL THICKNESS OF THE LARGEST CONNECTING MEMBERS TO ENSURE PROPER WELD SPACING.
2. FOR SECTIONS B-B, C-C, D-D, E-E AND F-F SEE SHEET 3 OF THIS SERIES.
3. FOR SIGN SUPPORT DETAILS, SEE ILLINOIS TOLLWAY STANDARD DRAWING F8, FOR DMS TYPE 2W - WALK-IN SIGN SUPPORT DETAILS, SEE SHEET 9 OF THIS SERIES.
4. DIRECTION OF INTERIOR DIAGONALS SHOWN IN SECTION A-A CORRECTLY DEPICTS TRUSSES HAVING AN ODD NUMBER OF PANELS. TRUSSES WITH AN EVEN NUMBER OF PANELS WILL HAVE DIAGONALS IN A REVERSED DIRECTION THAN AS SHOWN.
5. FOR ANY DESIGN SPAN LENGTH THAT FALLS BETWEEN TWO CONSECUTIVE SPANS, PROVIDED IN COLUMN 1 OF TABLE C, THE LARGER DESIGN SPAN LENGTH SHALL BE USED (I.E. FOR A 32' SPAN LENGTH FALLING BETWEEN 30' AND 35' DESIGN SPAN LENGTHS IN TABLE C, THE 35' DESIGN SPAN LENGTH TRUSS AND POST DETAILS SHALL BE USED).

APPROVED.....DATE 3-01-2020.....
CHIEF ENGINEERING OFFICER

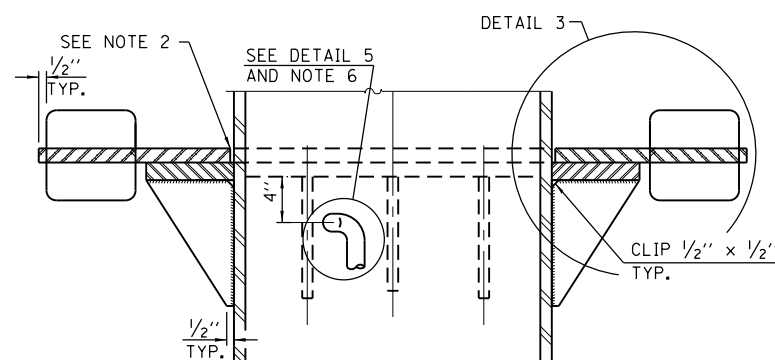


OVERHEAD SIGN STRUCTURE CANTILEVER TYPE STRUCTURE DETAILS

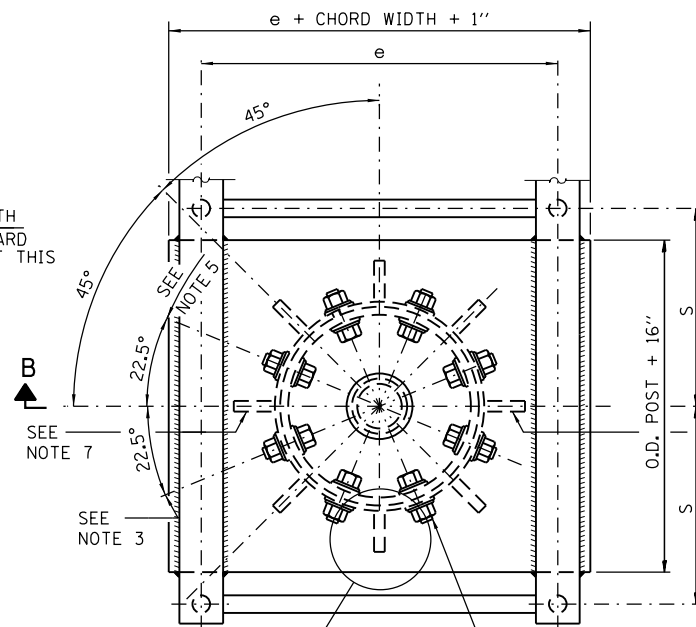
STANDARD F4-11



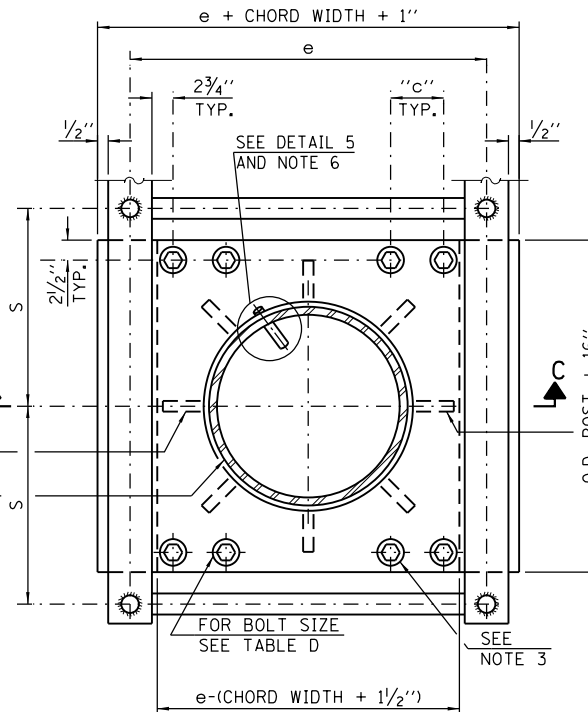
SECTION B-B



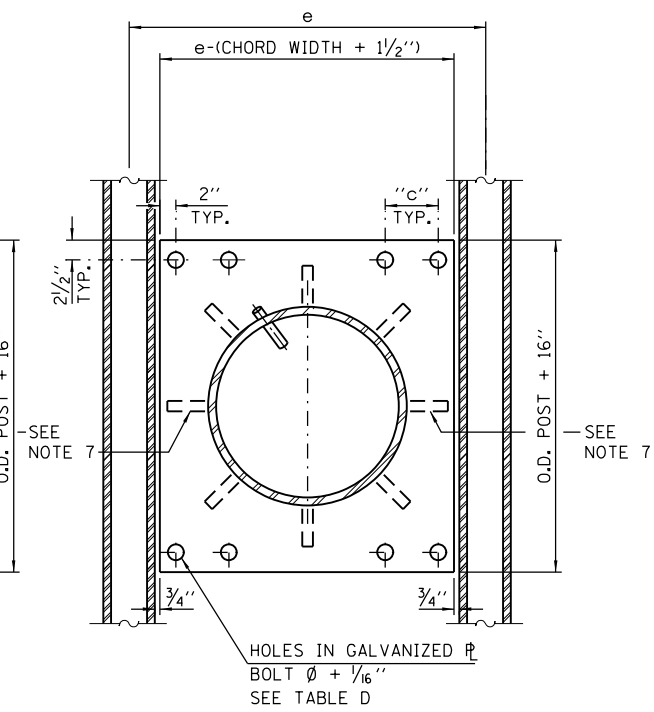
SECTION C-C



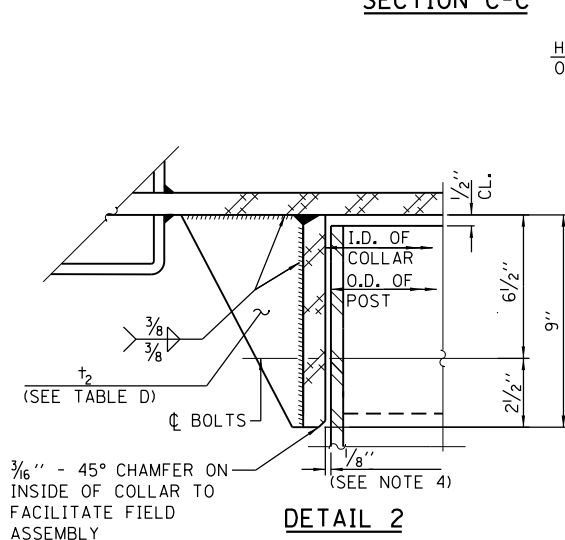
VIEW D-D
(CAP PLATE)



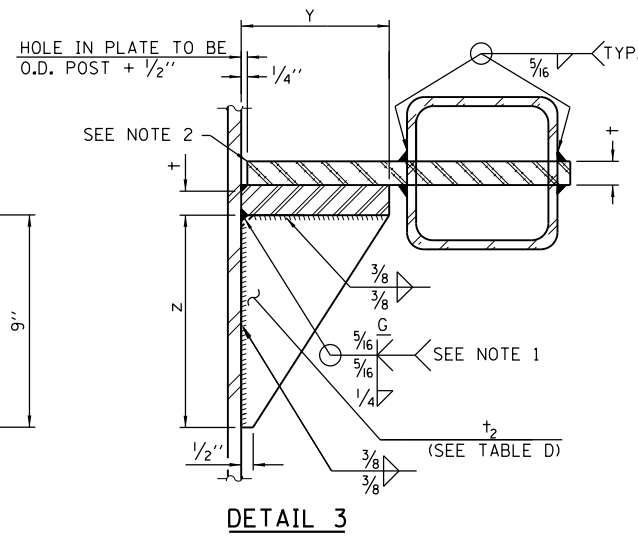
SECTION E-E
(JUNCTURE PLATE)



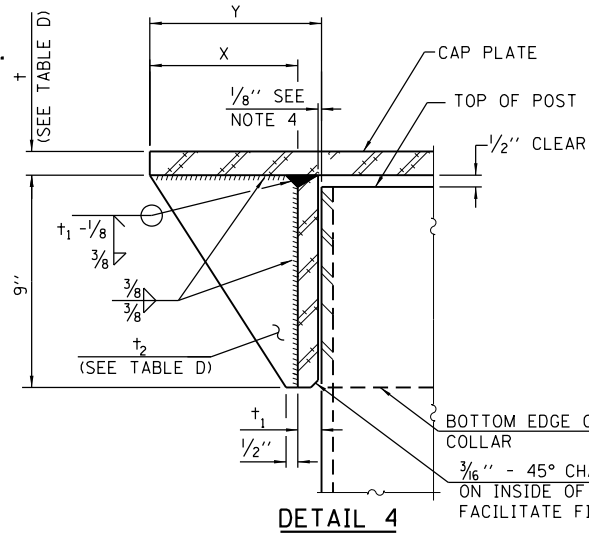
SECTION F-F
(SETTING PLATE)



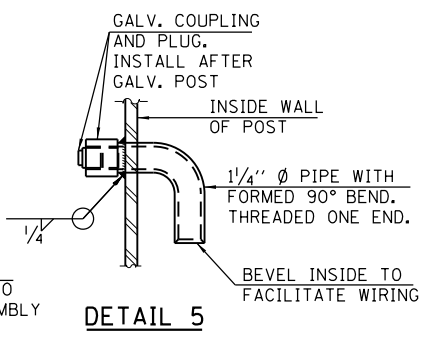
DETAIL 2



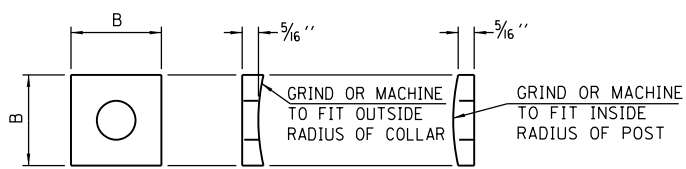
DETAIL 3



DETAIL 4



DETAIL 5



| BOLT SIZE | CONTOURED WASHERS | |
|------------|-------------------|------------|
| | HOLE DIA. | B |
| 1 1/8 inch | 1 1/4 inch | 2 1/4 inch |
| 1 1/4 inch | 1 3/8 inch | 2 1/4 inch |
| 1 1/2 inch | 1 5/8 inch | 2 1/4 inch |

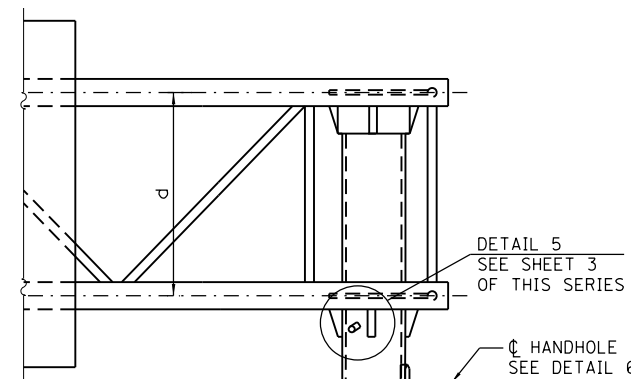
CONTOURED WASHERS
(ASTM A240, TYPE 304)

NOTES:

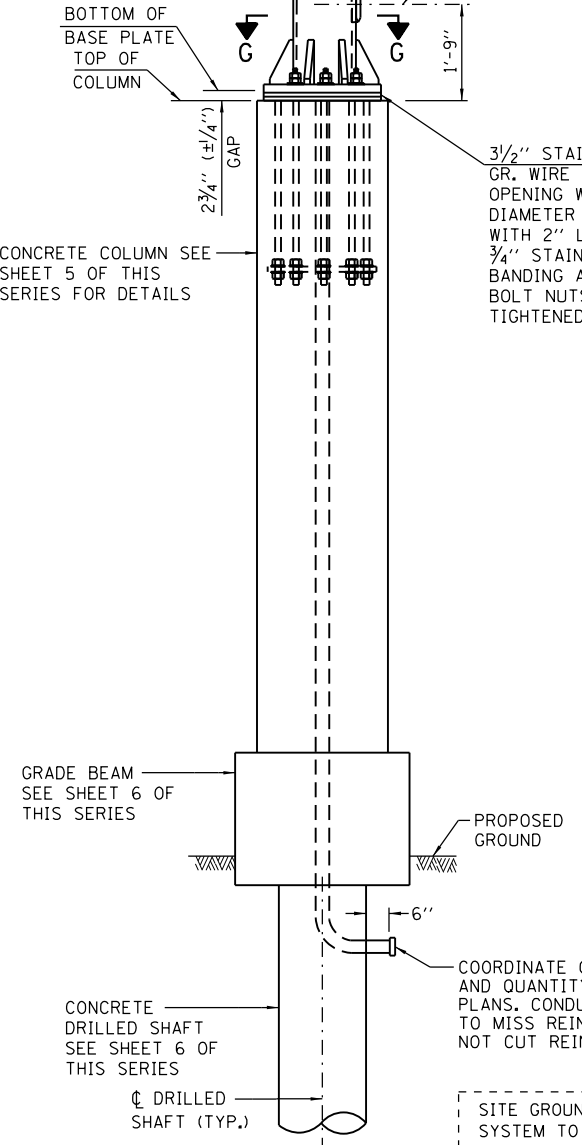
- GRIND TOP IF REQUIRED TO FULLY SEAT PLATE. REPAIR DAMAGED GALVANIZING BEFORE ASSEMBLY.
- AFTER TIGHTENING LOWER CONNECTION BOLTS, FILL GAP WITH NON-HARDENING SILICONE CAULK SUITABLE FOR EXTERIOR EXPOSURE AND ACCEPTABLE TO THE ENGINEER.
- CONNECTION BOLTS IN COLLAR AND BOLTS AT LOWER CHORD CONNECTION SHALL BE HIGH STRENGTH WITH MATCHING LOCKNUTS. LOWER CONNECTION BOLTS SHALL HAVE 2 FLAT WASHERS EACH.
- AFTER GALVANIZING, COLLAR I.D. SHALL EQUAL O.D. OF GALVANIZED POST PLUS 1/8 inch (± 1/16 inch) MAXIMUM GAP BETWEEN POST AND COLLAR AT ANY LOCATION SHALL BE 1/8 inch BEFORE TIGHTENING BOLTS.
- OPTIONAL FULL PENETRATION WELD IN COLLAR. (TWO LOCATIONS MAXIMUM (180° APART) X-RAY OR UT 100%) ALL BOLTS SHOWN ARE HIGH STRENGTH.
- ORIENT PIPE TOWARD SIGN PANEL SIDE. HOLE IN POST = O.D. PIPE + 1/8 inch.
- OMIT INDICATED STIFFENER IN TRUSS TYPE 20-D.

TABLE D: BOLT SCHEDULE

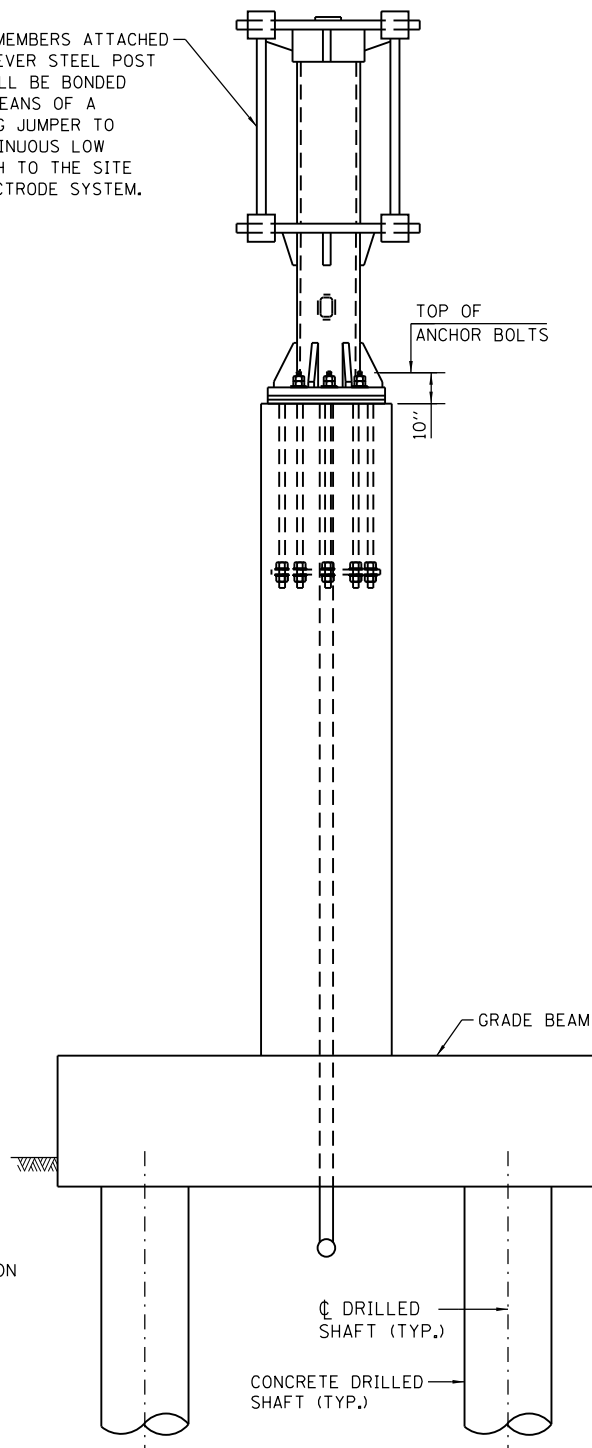
| SPAN LENGTH | POST OUTSIDE DIAMETER | JUNCTURE & COLLAR CONNECTION BOLT DIAMETER | LOWER JUNCTURE BOLT SPACING DIMENSION "c" | PLATE THICKNESS | | STIFFENER THICKNESS (t2) | NO. OF STIFFENERS | STIFFENERS | | |
|-------------|-----------------------|--------------------------------------------|-------------------------------------------|-----------------|------|--------------------------|-------------------|------------|----|---------|
| | | | | (t) | (t1) | | | x | y | z |
| < = 20' | 18" | 1 1/8" | 3 1/8" | 1" | 3/4" | 1/2" | 6 | 5" | 6" | 8" |
| 21'-30' | 18" | 1 1/2" | 3 3/4" | 1 1/8" | 7/8" | 3/4" | 8 | 5" | 6" | 8" |
| 31'-40' | 24" | 1 1/2" | 4 1/2" | 1 1/4" | 1" | 3/4" | 8 | 7" | 8" | 10 1/2" |
| 41'-50' | 24" | 1 1/2" | 4 1/2" | 1 1/4" | 1" | 3/4" | 8 | 7" | 8" | 10 1/2" |



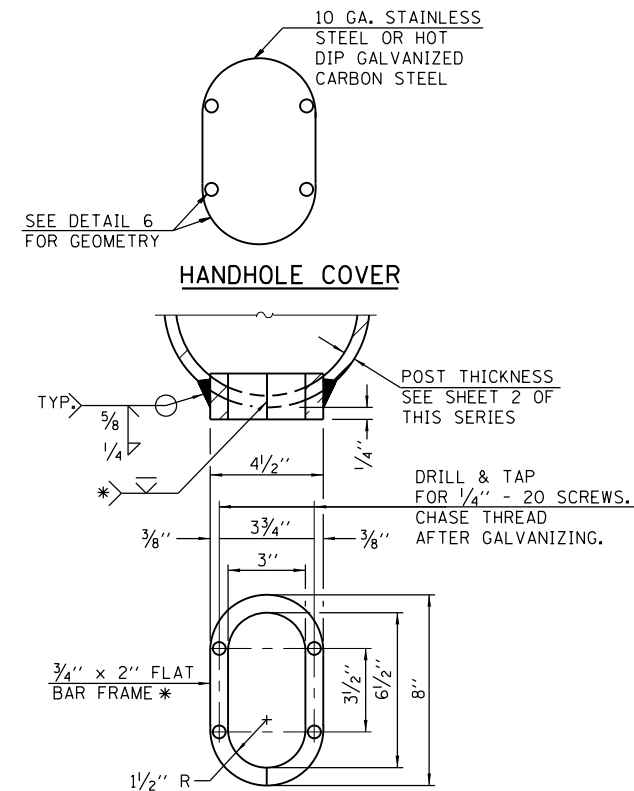
ALL METALLIC MEMBERS ATTACHED TO THE CANTILEVER STEEL POST STRUCTURE SHALL BE BONDED TOGETHER BY MEANS OF A COPPER BONDING JUMPER TO CREATE A CONTINUOUS LOW IMPEDANCE PATH TO THE SITE GROUNDING ELECTRODE SYSTEM.



FRONT ELEVATION



SIDE ELEVATION



PROVIDE 8" x 4 1/2" COVER. OUTSIDE CORNERS = 2 1/4" RADIUS. PROVIDE 4-5/16" Ø HOLES IN COVER FOR 1/4" - 20 ROUND HEAD HOT DIP GALVANIZED OR STAINLESS STEEL MACHINE SCREWS. (SEE COVER DETAILS.)

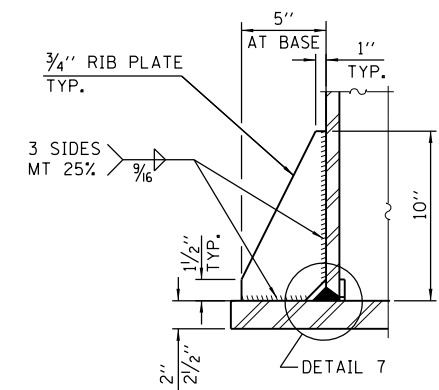
DETAIL 6

* BENT BARS MAY BE BUTT WELDED TOP AND BOTTOM OR BOTTOM ONLY. IN LIEU OF FABRICATED HANDHOLE FRAME AS SHOWN, MAY CUT FROM 2" PLATE (ROLLING DIRECTION VERTICAL). ALL CUT FACES TO BE GROUND TO ANSI ROUGHNESS OF 500 µIN OR LESS.

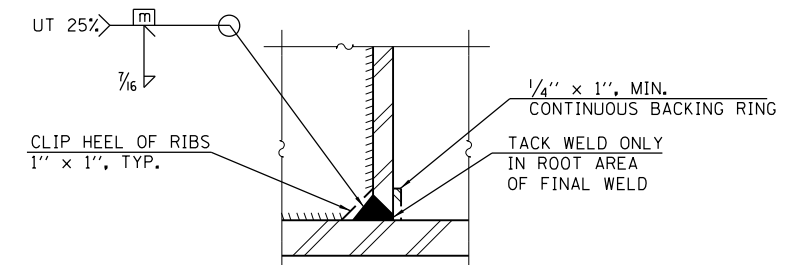
* * 18" IS MINIMUM TO BE GALVANIZED. ENTIRE BOLT MAY BE GALVANIZED AT CONTRACTOR'S OPTION.

TABLE E: BASE PLATE DETAIL

| SPAN LENGTH (L) | POST OUTSIDE DIAMETER | BASE PLATE | | BOLT CIRCLE | BOLT DIA. |
|-----------------|-----------------------|------------|-----------|-------------|-----------|
| | | DIAMETER | THICKNESS | | |
| < = 20' | 18" | 30" | 2" | 24" | 1 3/4" |
| 21'-30' | 18" | 30" | 2" | 24" | 1 3/4" |
| 31'-40' | 24" | 36" | 2 1/2" | 30" | 2" |
| 41'-50' | 24" | 36" | 2 1/2" | 30" | 2 1/4" |



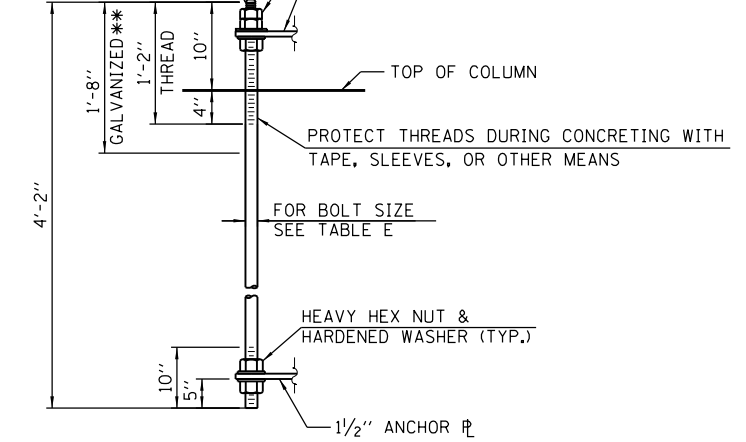
SECTION H-H



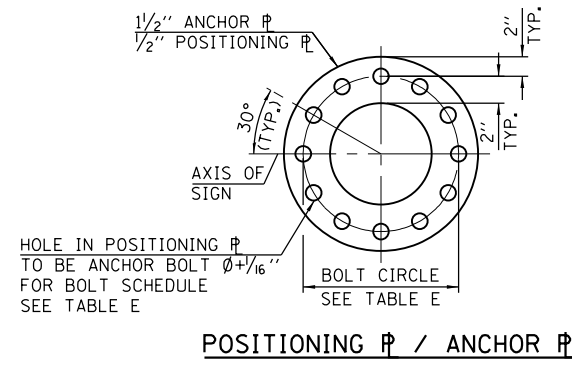
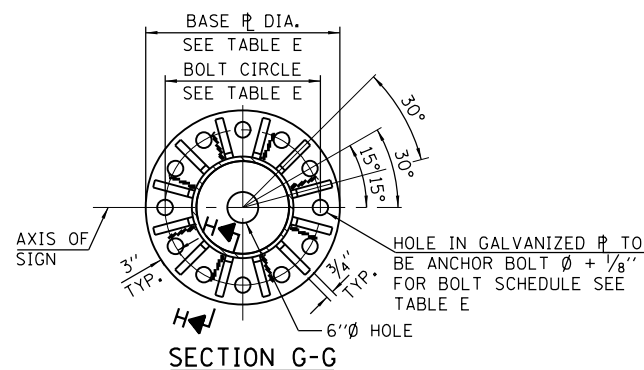
DETAIL 7
(TYPICAL RIB)

UTILIZE 1/2" POSITIONING PLATE AND TEMPORARY NUTS WITH LEVELING NUTS OR OTHER ENGINEER APPROVED METHODS TO MAINTAIN ANCHOR BOLTS ALIGNMENT DURING CONCRETE PLACEMENT. PLATE, EXTRA NUTS AND OTHER POSITIONING AIDS BECOME CONTRACTOR'S PROPERTY.

FOR UT, GRIND TOP OF BOLT SQUARE AND SMOOTH BEFORE GALVANIZING.



ANCHOR BOLT DETAIL



NOTE:

ANCHOR BOLTS SHALL CONFORM TO AASHTO M314 OR ASTM F1554 AND MEET CHARPY V-NOTCH (CVN) ENERGY OF 15 LB.-FT. AT 10° F. BEFORE GALVANIZING. GALVANIZE THE UPPER 18" (MINIMUM ***) AND ASSOCIATED M291, GRADE A, C OR DH HEAVY HEX NUTS, HEAVY HEX LOCK NUTS AND HARDENED WASHERS PER AASHTO M293. NO WELDING SHALL BE PERMITTED ON BOLTS. PROVIDE AN UNFINISHED NUT AT BOTTOM, A HEXAGON LOCKNUT, HEXAGON NUT AND WASHER ABOVE BASE PLATE AND A LEVELING NUT AND WASHER BELOW BASE PLATE. NUTS SHALL EACH BE TIGHTENED WITH 200 LB.-FT. MINIMUM TORQUE AGAINST BASE PLATE. BEFORE OR AFTER THREADING, BUT BEFORE GALVANIZING, EACH ANCHOR BOLT SHALL BE ULTRASONICALLY TESTED (UT) BY A LEVEL II OR III INSPECTOR, QUALIFIED IN ACCORDANCE WITH ANSI GUIDELINES, USING A STRAIGHT BEAM, 1/2" Ø 3.5 MHZ. TRANSDUCER, TO ENSURE NO REJECTABLE FLAWS EXIST IN THE UPPER 18" (TENSION CRITERIA).



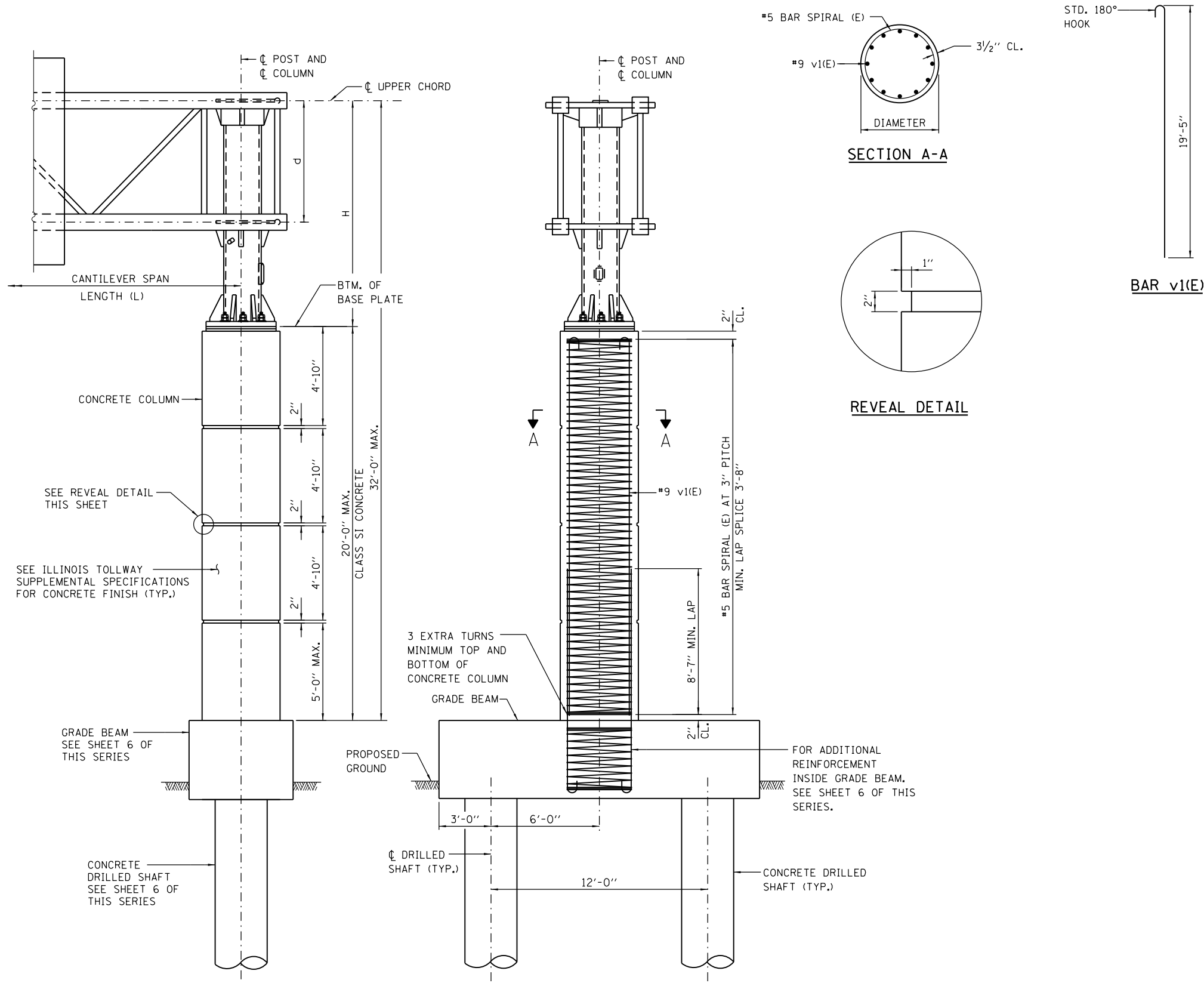


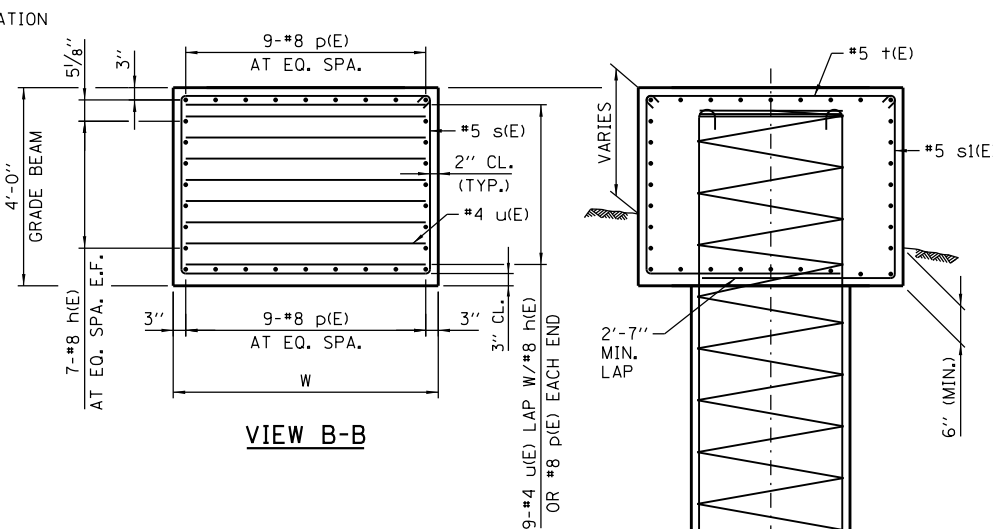
TABLE F: CONCRETE COLUMN DESIGN TABLE

| SPAN LENGTH (L) | STEEL POST DIAMETER | CONCRETE COLUMN | | | |
|-----------------|---------------------|-----------------|--------------------|-------------------------|---------------------|
| | | DIAMETER | VERTICAL BAR v1(E) | CLASS SI CONC. CU. YD.* | REINF. BARS POUND * |
| < = 20' | 18" | 3'-6" | 16-#9 | 7.1 | 1,910 |
| 21'-30' | 18" | 3'-6" | 16-#9 | 7.1 | 1,910 |
| 31'-40' | 24" | 4'-0" | 20-#9 | 9.2 | 2,330 |
| 41'-50' | 24" | 4'-0" | 20-#9 | 9.2 | 2,330 |

* CONCRETE VOLUME AND REBAR WEIGHT ARE DETERMINED FOR 20'-0" CONCRETE COLUMN HEIGHT. ADJUST CONCRETE VOLUME AND REBAR WEIGHT ACCORDINGLY IF CONCRETE COLUMN HEIGHT IS LESS THAN 20'-0".

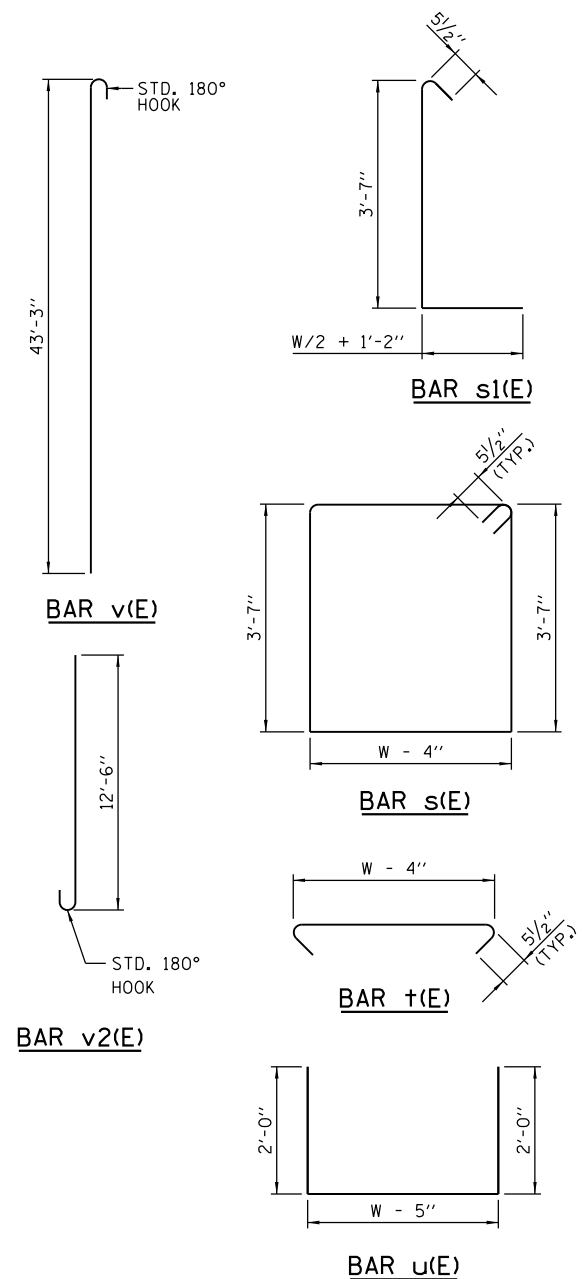
(2 SHAFT AND 1 GRADE BEAM)

| |
|----------------------------------------|
| #4 BAR SPIRAL (E) - SEE SIDE ELEVATION |
| #5 BAR SPIRAL (E) - SEE SIDE ELEVATION |

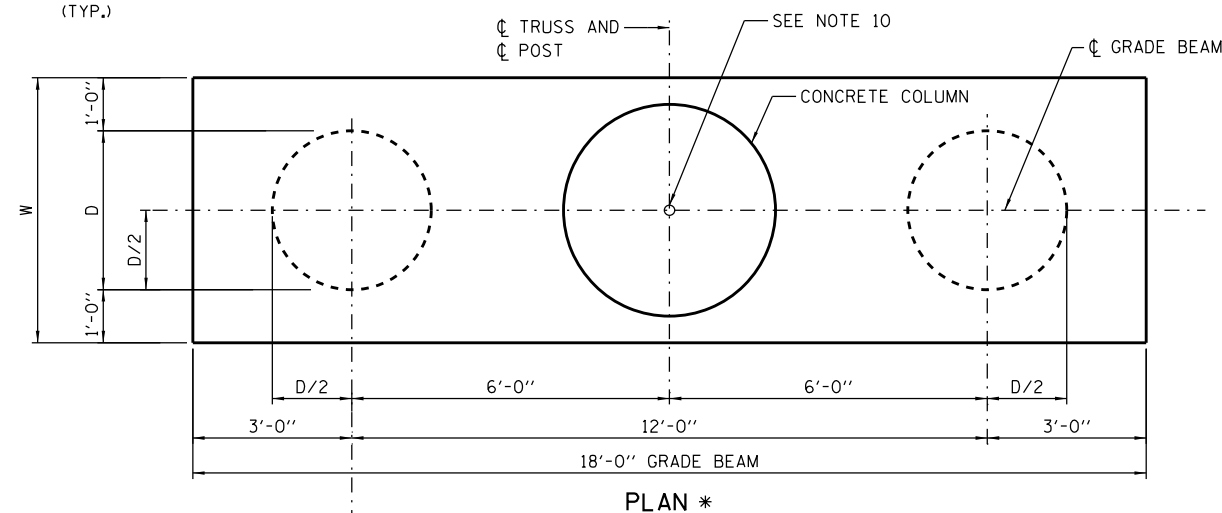


VIEW B-B

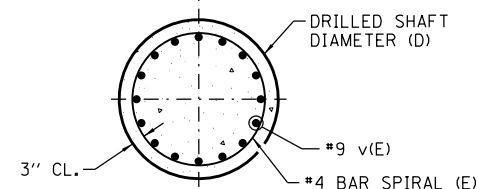
SECTION C-C



1. THE FOUNDATION DETAILS SHOWN ARE BASED ON THE PRESENCE OF MOSTLY COHESIVE SOIL CONDITIONS (SILTY OR SANDY CLAY), WITH AN AVERAGE UNCONFINED COMPRESSIVE STRENGTH (QU) > 1.25 TON/SQ. FT. WHICH SHALL BE DETERMINED BY PREVIOUS SOIL INVESTIGATIONS AT THE JOBSITE. WHEN OTHER CONDITIONS ARE INDICATED, THE BORING DATA SHALL BE INCLUDED IN THE PLANS AND THE FOUNDATION DIMENSIONS SHOWN SHALL BE THE RESULT OF SITE SPECIFIC DESIGNS. IF CONDITIONS ENCOUNTERED IN THE FIELD ARE DIFFERENT THAN THOSE INDICATED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER TO DETERMINE IF THE FOUNDATION DIMENSIONS NEED TO BE MODIFIED.
2. ALL MATERIAL, FABRICATION, AND CONSTRUCTION REQUIREMENTS SHALL BE IN ACCORDANCE WITH SECTION 734 OF THE ILLINOIS TOLLWAY SUPPLEMENTAL SPECIFICATIONS.
3. CONCRETE SHALL BE PLACED MONOLITHICALLY, WITHOUT CONSTRUCTION JOINTS UNLESS NOTED OTHERWISE.
4. BACKFILL SHALL BE PLACED PER SECTION 502 OF THE STANDARD SPECIFICATION AND PRIOR TO ERECTION OF CONCRETE COLUMN.
5. PROVIDE RUBBED SURFACE FINISH FOLLOWED BY CONCRETE SEALER APPLICATION ON ENTIRE SURFACE OF CONCRETE COLUMN AND NORMAL SURFACE FINISH ON GRADE BEAM, EXCEPT BOTTOM SURFACE. COST IS INCLUDED IN THE COST OF "FOUNDATION FOR OVERHEAD SIGN STRUCTURE, CANTILEVER TYPE".
6. ALL REBAR DESIGNATED (E) SHALL BE EPOXY COATED. REBAR SHALL BE POSITIONED SO THAT THERE WILL BE NO INTERFERENCE BETWEEN VERTICAL REINFORCEMENT AND STIRRUPS.
7. NO SONOTUBES OR DECOMPOSABLE FORMS SHALL BE USED 6" BELOW THE FINISHED GROUND LINE. PERMANENT METAL FORMS OR OTHER SHIELDING SHALL NOT BE LEFT IN PLACE BELOW THE ELEVATION WITHOUT THE ENGINEER'S WRITTEN PERMISSION. EXCAVATIONS SHALL BE DEWATERED BEFORE CONCRETE PLACEMENT IF DIRECTED BY THE ENGINEER AT NO ADDITIONAL COST.
8. FOR SIZE AND NUMBER OF PVC COATED STEEL CONDUITS, SEE ELECTRICAL CONSTRUCTION DRAWINGS.
9. TYPICAL SIGN STRUCTURE FOUNDATION IS SHOWN ON THIS SHEET. SEE SHEET 7 OF THIS SERIES FOR FOUNDATION LOCATED IN ROADWAY MEDIAN.
10. COORDINATE CONDUIT SIZE, LOCATION AND QUANTITY WITH ELECTRICAL PLANS. CONDUITS SHALL BE PLACED TO MISS REINFORCEMENT BARS. DO NOT CUT REINFORCEMENT BARS.



PLAN *



SECTION A-A
(TYPICAL BOTH SHAFTS)

* REINFORCEMENT IN GRADE BEAM
NOT SHOWN FOR CLARITY.

** FOR GRADE BEAM ONLY.

| BAR SPIRAL LAP SPLICE | |
|-----------------------|----------|
| BAR | MIN. LAP |
| #4 | 2'-11" |
| #5 | 3'-8" |

| SPAN LENGTH (L) | W | D | B | F | VERTICAL BAR | | | CLASS DS CONC. CU. YD.** | CLASS DS CONC. CU. YD. | REINF. BA POUND |
|-----------------|-------|-------|-----|-----|-----------------|-----------------|-------|-----------------------------|---------------------------|--------------------|
| | | | | | v(E) SHAFT 1 | v(E) SHAFT 2 | v2(E) | | | |
| < = 20' | 5'-0" | 3'-0" | 40' | 44' | 12-#9 | 12-#9 | 16-#9 | 13.4 | 21 | 7,700 |
| 21'-30' | 5'-0" | 3'-0" | 40' | 44' | 12-#9 | 12-#9 | 16-#9 | 13.4 | 21 | 7,700 |
| 31'-40' | 6'-0" | 4'-0" | 40' | 44' | 20-#9 | 20-#9 | 20-#9 | 16 | 37.3 | 10,800 |
| 41'-50' | 6'-0" | 4'-0" | 40' | 44' | 20-#9 | 20-#9 | 20-#9 | 16 | 37.3 | 10,800 |






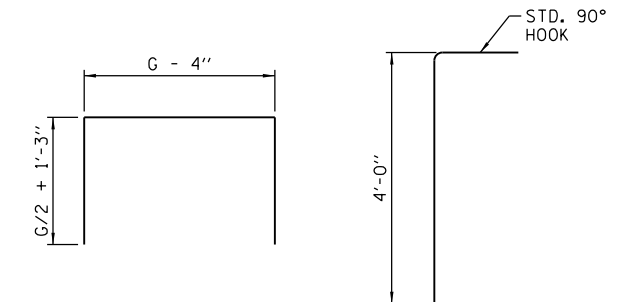
OVERHEAD SIGN STRUCTURE CANTILEVER TYPE STRUCTURE DETAILS

STANDARD F4-11


 APPROVED..... DATE 3-31-2014
 CHIEF ENGINEERING OFFICER

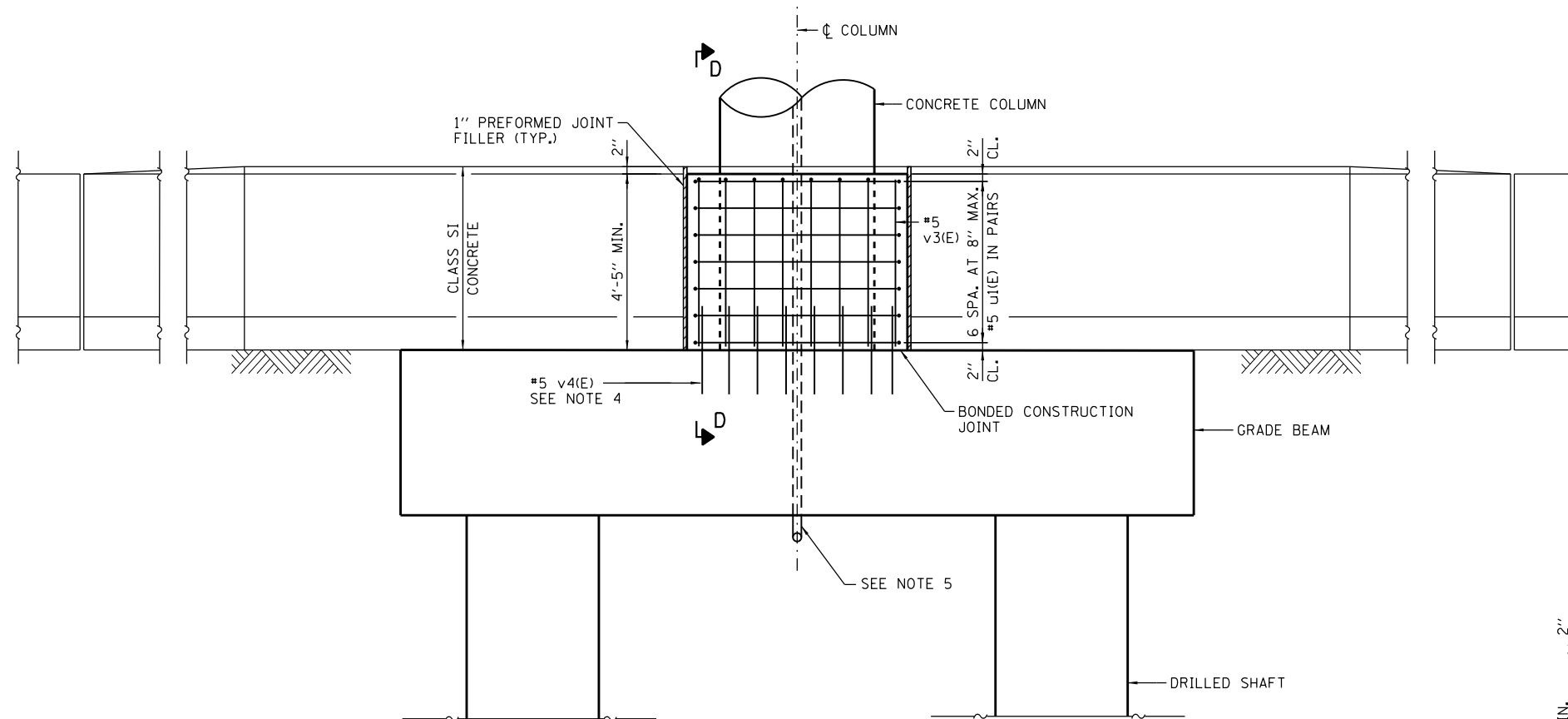
BAR LIST - CRASHWALL

| BAR | SIZE | G = 4'-6" | | G = 5'-0" | | SHAPE |
|-------|------|-----------|--------|-----------|--------|-------------------------------------------------------------------------------------|
| | | NUMBER | LENGTH | NUMBER | LENGTH | |
| u1(E) | #5 | 14 | 11'-2" | 14 | 12'-2" |  |
| v3(E) | #5 | 24 | 4'-10" | 28 | 4'-10" |  |
| v4(E) | #5 | 24 | 2'-0" | 28 | 2'-0" |  |

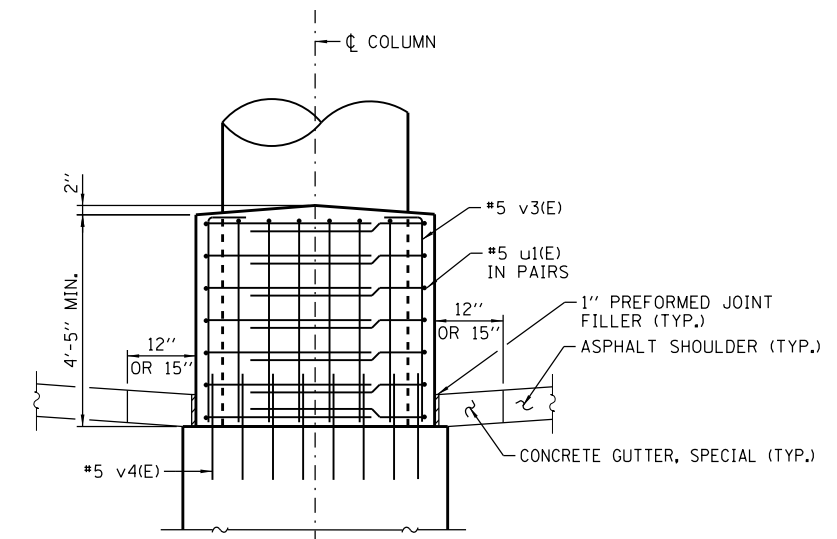


BAR u1(E)

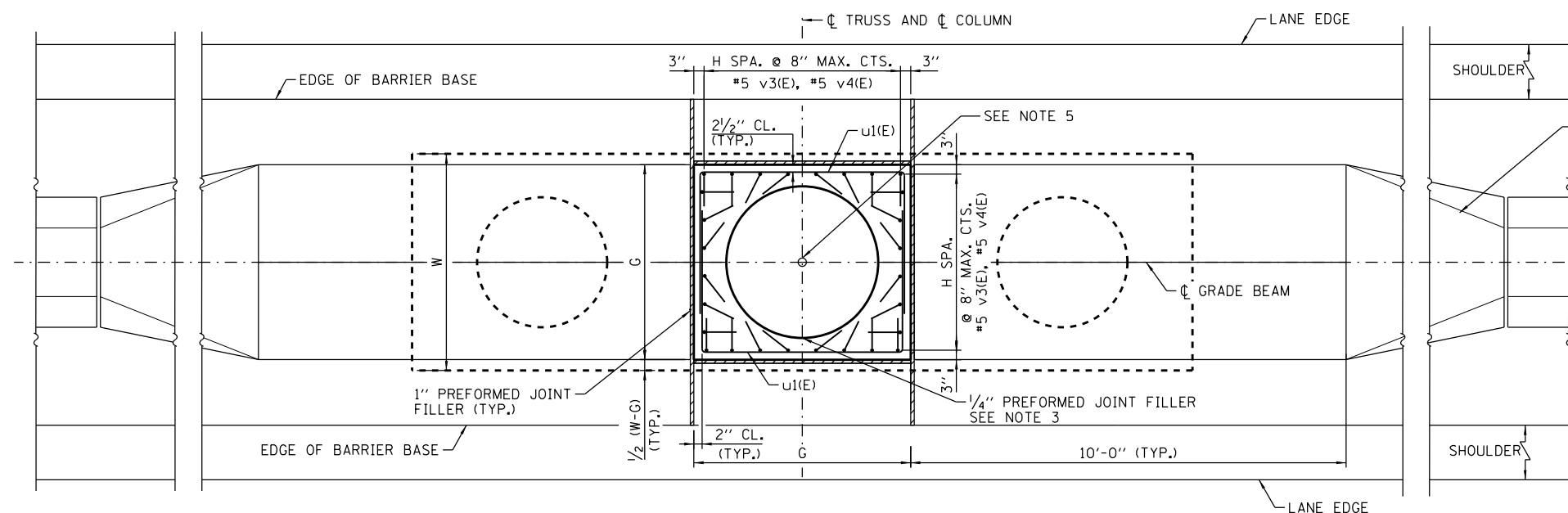
BAR v3(E)



SIDE ELEVATION



SECTION D-D



PLAN

NOTES:

- SEE SHEET 6 OF THIS SERIES FOR ADDITIONAL NOTES.
- GRADE BEAM AND DRILLED SHAFT DIMENSIONS, DETAILS, QUANTITIES AND BAR LIST ARE SHOWN ON SHEET 6 OF THIS SERIES.
- SEAL EXPOSED SURFACE OF 1/4" PREFORMED JOINT FILLER WITH BACKER ROD AND SILICONE SEALER (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE).
- #5 DRILLED ANCHOR BARS WILL BE EPOXY GROUTED AASHTO M31, GRADE 60 REBAR. PROVIDE 12" MINIMUM EMBEDMENT. INSTALL ANCHORS ACCORDING TO STANDARD SPECIFICATIONS SECTION 584. LOCATE GRADE BEAM REBAR PRIOR TO DRILLING. DO NOT DAMAGE GRADE BEAM REBAR DURING INSTALLATION.
- COORDINATE CONDUIT SIZE, LOCATION AND QUANTITY WITH ELECTRICAL PLANS. CONDUITS SHALL BE PLACED TO MISS REINFORCEMENT BARS. DO NOT CUT REINFORCEMENT BARS.
- PROTECTIVE COAT SHALL BE APPLIED TO TRAFFIC AND TOP FACES OF CRASHWALL.

TABLE H: DESIGN TABLE FOR CRASHWALL

| SPAN LENGTH (L) | W | G | H | CLASS SI CONCRETE CU. YD. | REINF. BARS POUND | PROTECTIVE COAT SQ. YD. |
|-----------------|-------|-------|---|---------------------------|-------------------|-------------------------|
| < = 20' | 5'-0" | 4'-6" | 6 | 1.7 | 340 | 6.0 |
| 21'-30' | 5'-0" | 4'-6" | 6 | 1.7 | 340 | 6.0 |
| 31'-40' | 6'-0" | 5'-0" | 7 | 2.0 | 380 | 7.0 |
| 41'-50' | 6'-0" | 5'-0" | 7 | 2.0 | 380 | 7.0 |

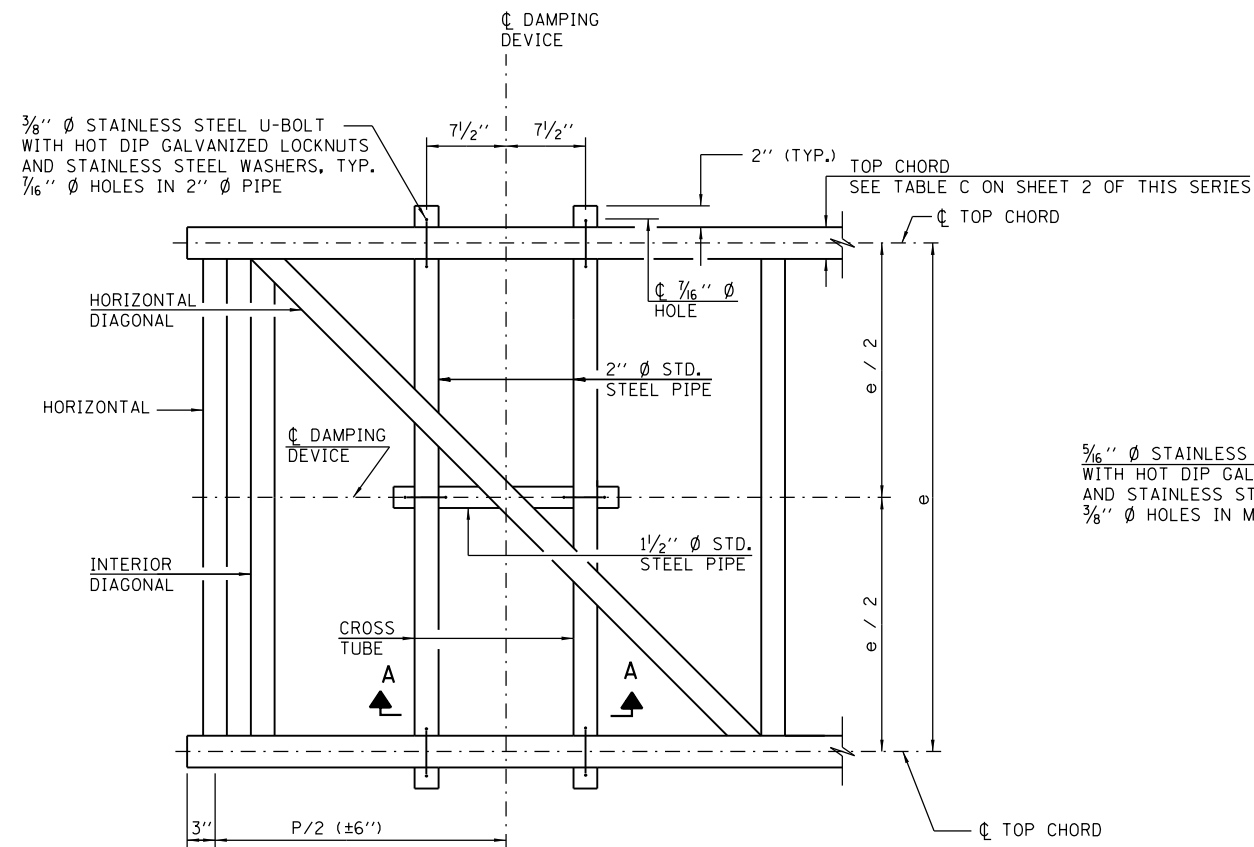
SHEET 7 OF 12



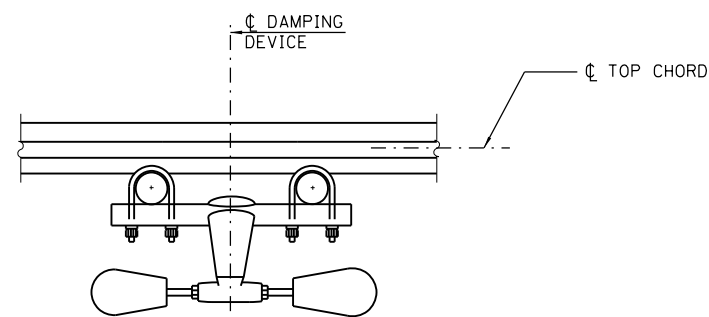
OVERHEAD SIGN STRUCTURE
CANTILEVER TYPE
STRUCTURE DETAILS

STANDARD F4-11

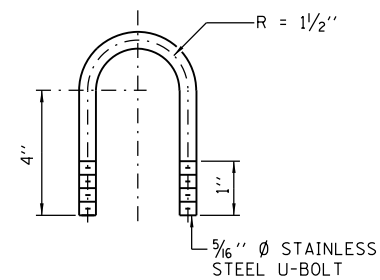
APPROVED: *Paul Kovacs* DATE 3-31-2014
CHIEF ENGINEERING OFFICER



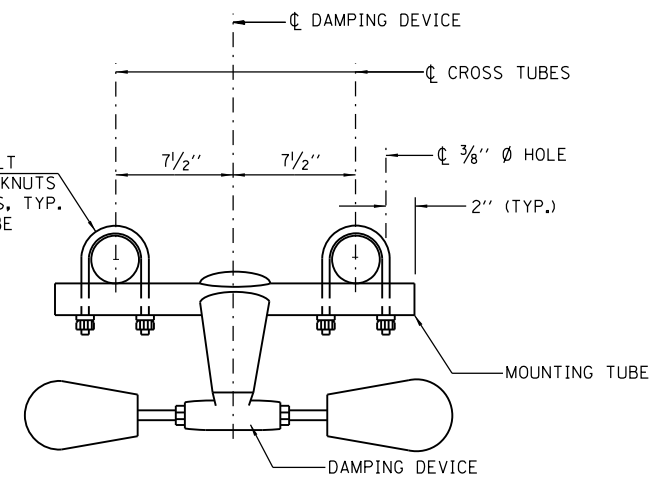
PLAN DETAIL



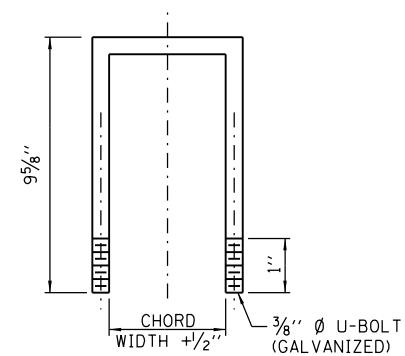
SECTION A-A



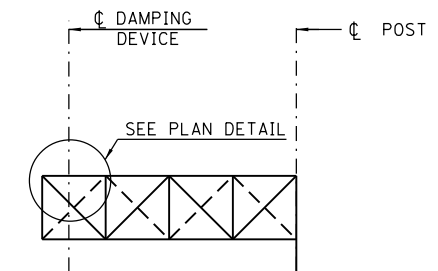
DAMPING DEVICE MOUNTING
TUBE U-BOLT DETAIL
(TYPICAL)



TRUSS DAMPING
DEVICE CONNECTION DETAIL



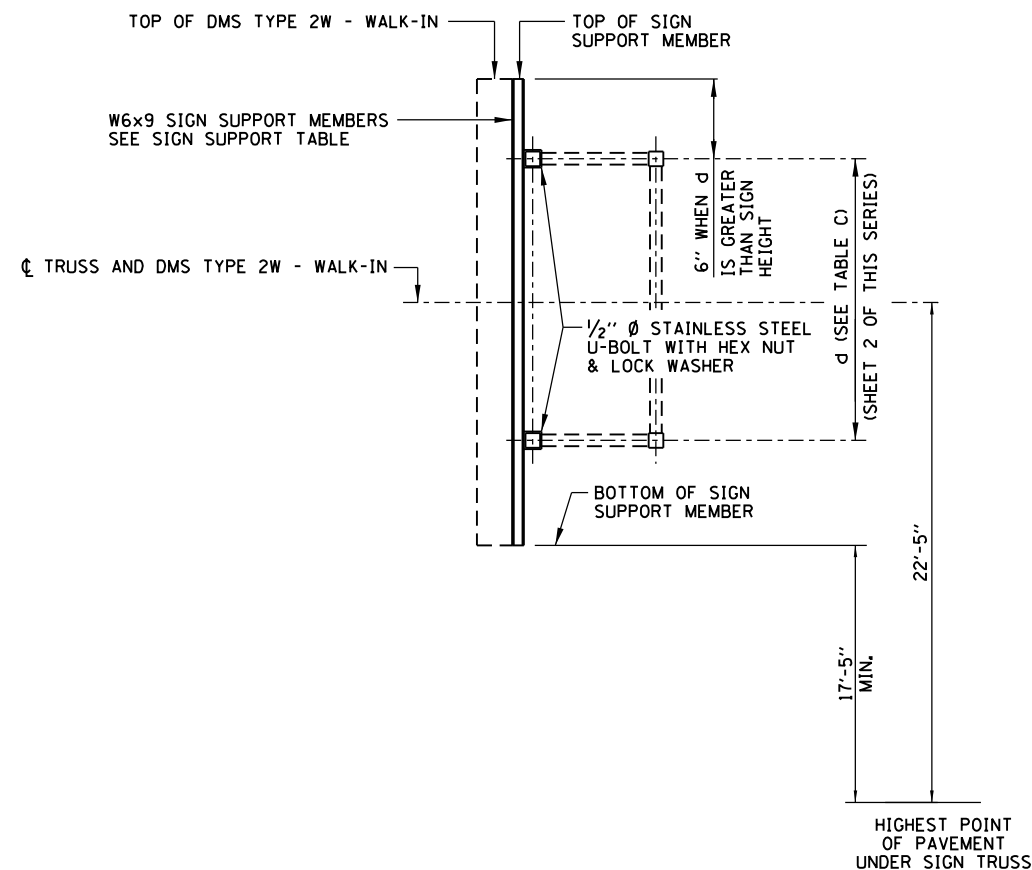
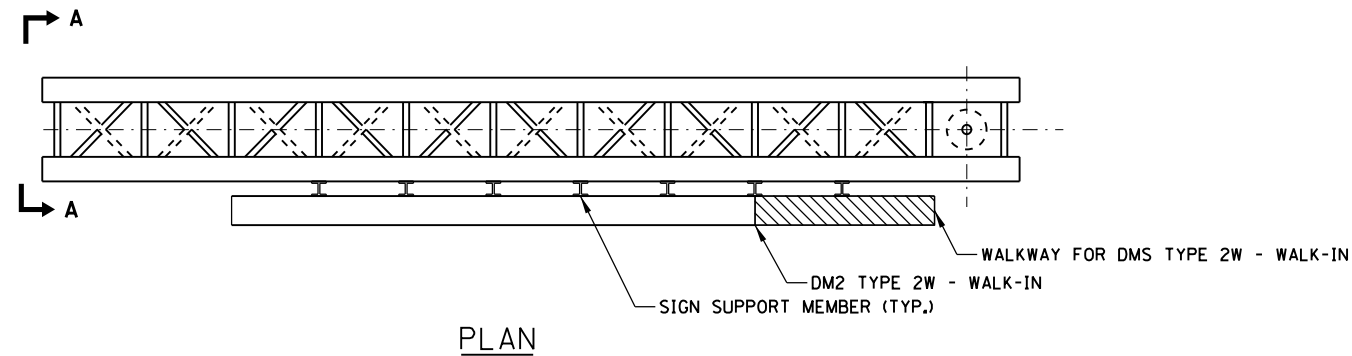
TOP CHORD TO CROSS TUBE
U-BOLT DETAIL
(TYPICAL)



ELEVATION

NOTE:

DAMPER: ONE DAMPER PER TRUSS. (31 LBS. STOCKBRIDGE-TYPE 29" MINIMUM BETWEEN ENDS OF WEIGHTS.)



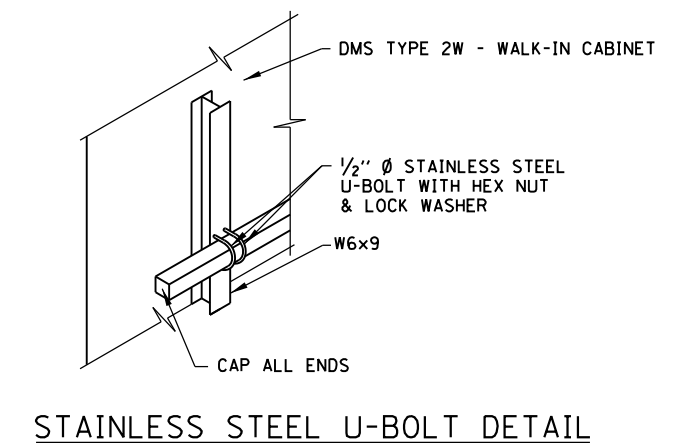
DMS TYPE 2W - WALK-IN SUPPORT DETAIL

TABLE I: SIGN SUPPORT TABLE

| W6x9 | | |
|--------------|--------------------------|----------------------------------------|
| SIGN WIDTH | | NUMBER OF SIGN SUPPORTS REQUIRED |
| GREATER THAN | LESS THAN OR EQUAL TO | |
| | 8'-0" | 2 |
| 8'-0" | 14'-0" | 3 |
| 14'-0" | 20'-0" | 4 |
| 20'-0" | 26'-0" | 5 |
| 26'-0" | 32'-0" | 6 |


TABLE J: DMS TYPE
2W - WALK-IN TABLE

| MAXIMUM TRUSS LENGTH | | | | MAXIMUM WEIGHT |
|----------------------------|--------|--------|-----------|-------------------|
| | HEIGHT | WIDTH | DEPTH | |
| 40 FEET | 8'-0" | 26'-6" | 3'-4 1/2" | 4200 LBS. |



NOTES:

1. DMS TYPE 2W - WALK-IN SHALL BE ATTACHED TO TRUSS AS CLOSE TO PANEL JOINTS AS POSSIBLE.
2. VERIFY SIGN SUPPORT MEMBER LENGTH PRIOR TO FABRICATION.
3. DMS TYPE 2W - WALK-IN MANUFACTURER SHALL DESIGN, PROVIDE AND INSTALL HORIZONTAL MOUNTING MEMBERS. VERTICAL SPACING OF HORIZONTAL MEMBERS SHALL BE DESIGNED BY DMS TYPE 2W - WALK-IN MANUFACTURER. VERIFY VERTICAL SPACING WITH HOLES FOR STAINLESS STEEL U-BOLT.

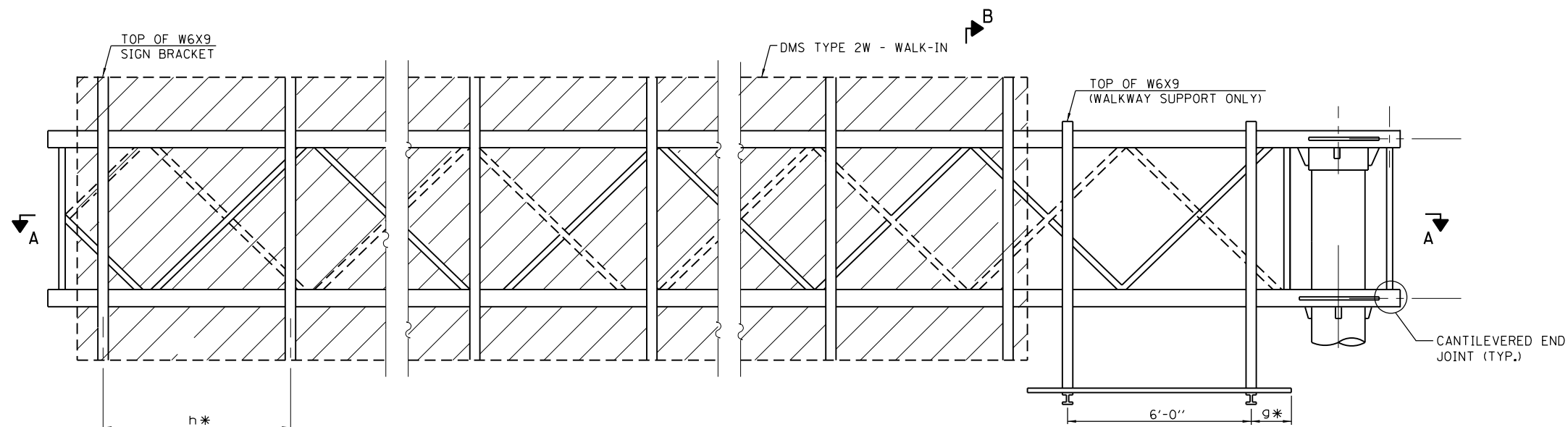

 APPROVED..... DATE 3-31-2014
 CHIEF ENGINEERING OFFICER

SHEET 9 OF 12



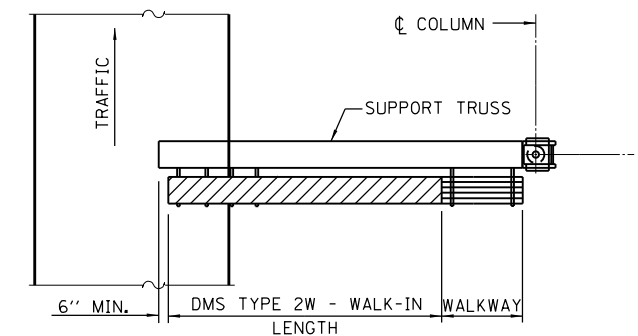
OVERHEAD SIGN STRUCTURE
CANTILEVER TYPE
STRUCTURE DETAILS

STANDARD F4-11



* BRACKET AND GRATING DIMENSIONS ARE NOMINAL AND WILL VARY BASED ON ACTUAL DMS TYPE 2W - WALK-IN DIMENSIONS PLUS MANUFACTURER'S MOUNTING DEVICES.

TYPICAL FRONT ELEVATION
WITH HANDRAIL OMITTED FOR CLARITY.
FOR SECTION B-B, SEE SHEET 11 OF THIS SERIES.



PLAN
WALKWAY AND HANDRAIL SKETCH
(ROAD PLAN BENEATH TRUSS VARIES)
WALKWAY MAY BE LOCATED AT RIGHT OR LEFT END OF TRUSS.

NOTES:

SPACE WALKWAY BRACKETS AND SIGN BRACKETS W6X9 FOR EFFICIENCY AND WITHIN LIMITS SHOWN:

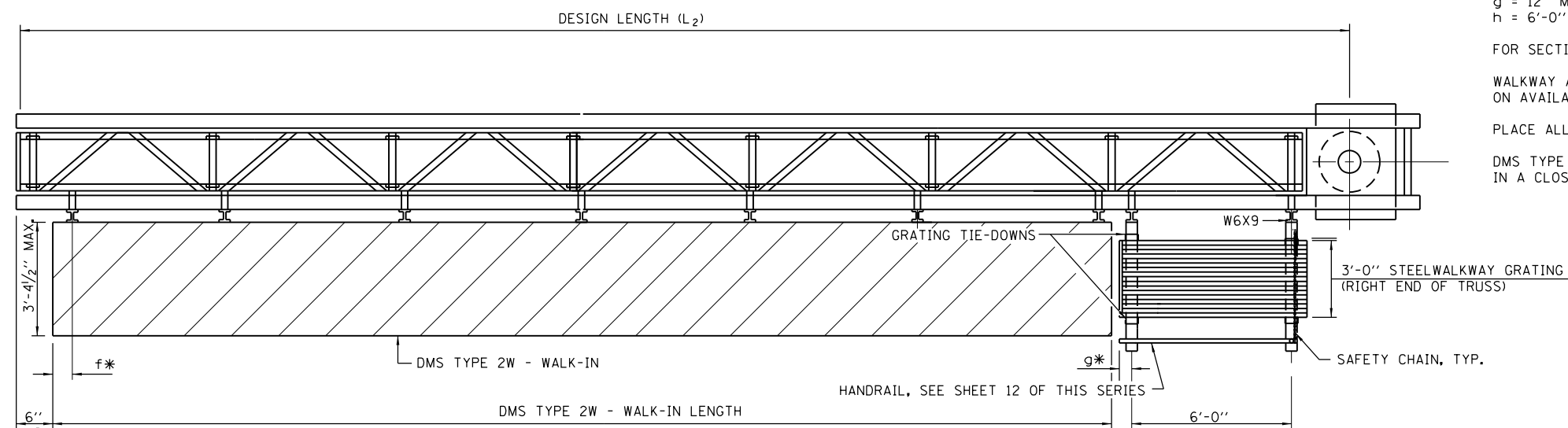
f = 12" MAXIMUM, 4" MINIMUM (END OF SIGN TO ϕ OF NEAREST BRACKET)
g = 12" MAXIMUM, 4" MINIMUM (END OF WALKWAY GRATING TO ϕ OF NEAREST SUPPORT BRACKET)
h = 6'-0" MAXIMUM (ϕ TO ϕ SIGN AND/OR WALKWAY SUPPORT BRACKETS, W6X9)

FOR SECTION B-B, SEE SHEET 11 OF THIS SERIES.

WALKWAY AND TRUSS GRATING WIDTH DIMENSIONS ARE NOMINAL AND MAY VARY $\pm 1/2$ " BASED ON AVAILABLE STANDARD WIDTH.

PLACE ALL SIGN AND WALKWAY BRACKETS AS CLOSE TO PANEL POINTS AS PRACTICAL.

DMS TYPE 2W - WALK-IN SHALL HAVE THE DOOR AT THE END, OPPOSITE THE WALKWAY SECURED IN A CLOSED POSITION.



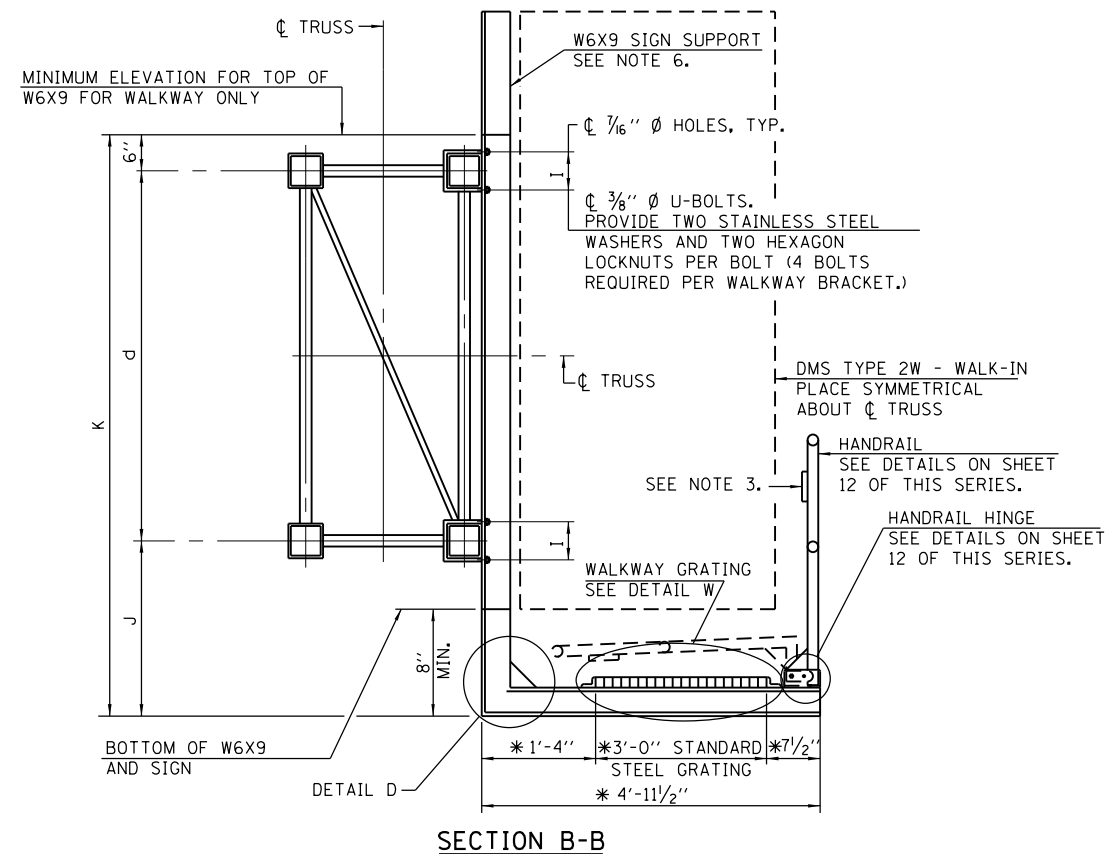
SECTION A-A

PLACE ALL SIGN AND WALKWAY BRACKETS AS CLOSE TO PANEL POINTS AS PRACTICAL.

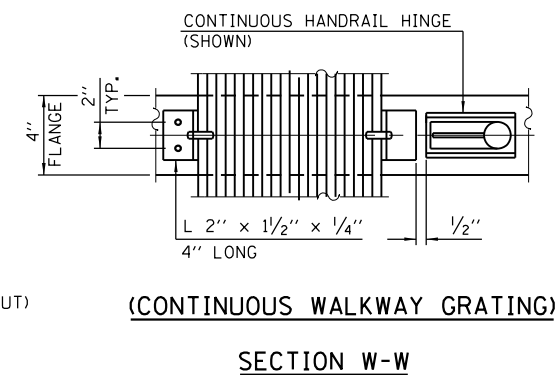
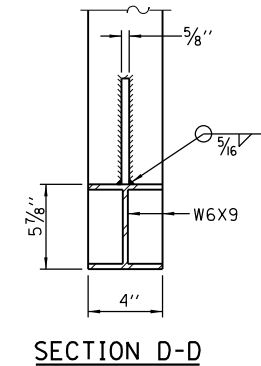
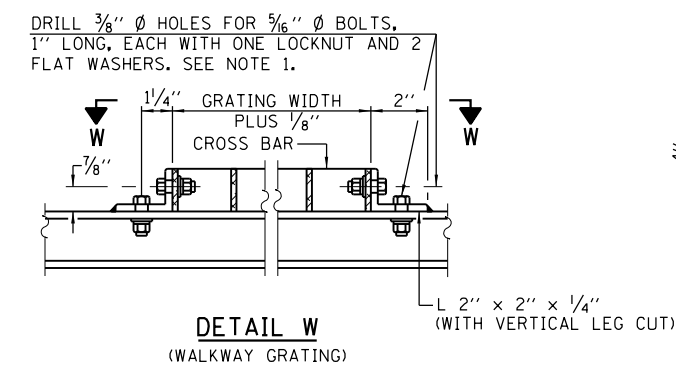
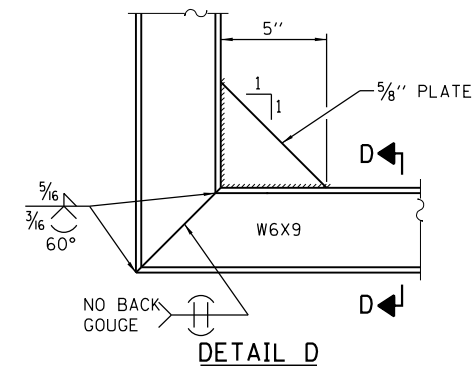
BRACKET TABLE

| W6X9 | | |
|--------------|-----------------------|-----------------------------|
| SIGN WIDTH | | NUMBER OF BRACKETS REQUIRED |
| GREATER THAN | LESS THAN OR EQUAL TO | |
| | 8'-0" | 2 |
| 8'-0" | 14'-0" | 3 |
| 14'-0" | 20'-0" | 4 |
| 20'-0" | 26'-0" | 5 |
| 26'-0" | 32'-0" | 6 |





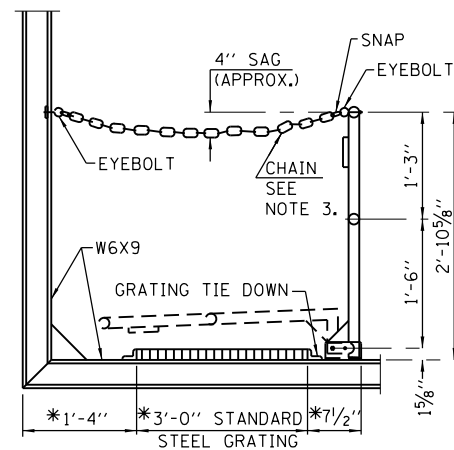
*BRACKET AND GRATING DIMENSIONS ARE NOMINAL AND WILL VARY BASED ON ACTUAL DMS TYPE 2W - WALK-IN DIMENSIONS PLUS MANUFACTURERS MOUNTING DEVICE.



NOTES:

1. DRILLING HOLES IN GRATING MAY BE DONE IN SHOP OR FIELD, BASED ON CONTRACTOR'S PREFERENCE AND SUBJECT TO ACCURATE ALIGNMENT.
2. IF HANDRAIL JOINT PRESENT, WELD ANGLE TO W6X9 AND 1/4" EXTENSION BARS. SEE SHEET 12 OF THIS SERIES.
3. $\# 1/8" \times 1/2" \times 2"$ WELDED TO HANDRAIL POSTS TO PROTECT LOCATIONS THAT CONTACT GRATING.
4. DMS TYPE 2W - WALK-IN MANUFACTURER SHALL DESIGN AND SUPPLY HARDWARE FOR CONNECTION TO W6X9. BOLTS SHALL BE STAINLESS STEEL OR HOT DIP GALVANIZED HIGH STRENGTH PER IDOT SPECIFICATIONS.

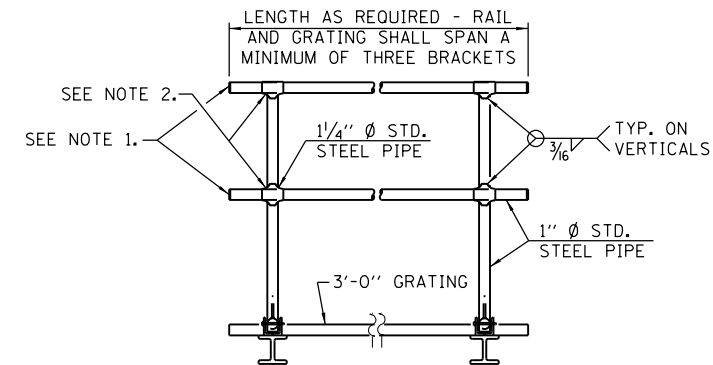
APPROVED.....
 DATE 3-31-2014
 CHIEF ENGINEERING OFFICER



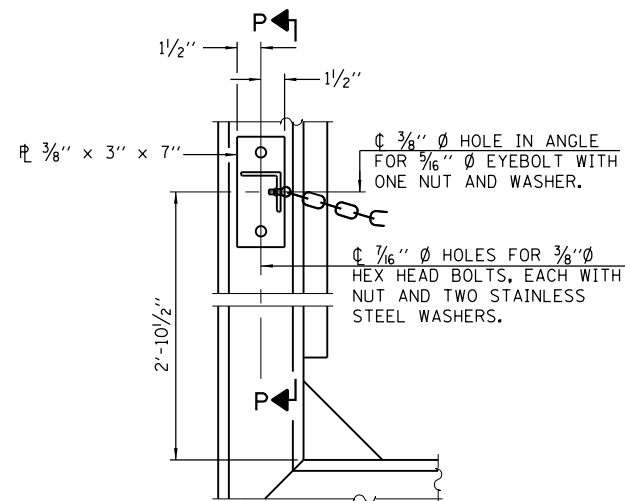
SIDE ELEVATION
(SHOWING SAFETY CHAIN W/O SIGN)

* BRACKET AND GRATING DIMENSIONS ARE NOMINAL AND WILL VARY BASED ON ACTUAL DMS TYPE 2W - WALK-IN DIMENSIONS PLUS MANUFACTURERS MOUNTING DEVICE.

HANDRAIL DETAILS

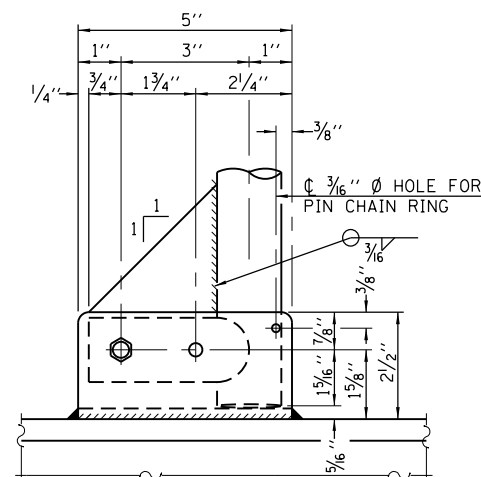


FRONT ELEVATION

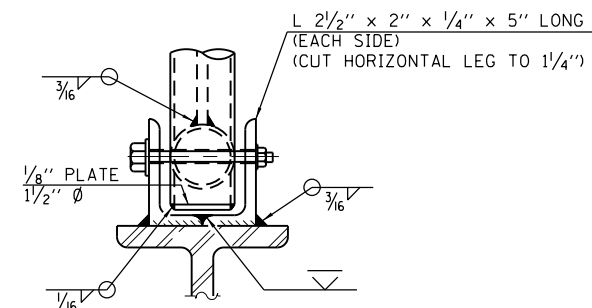


ALTERNATE SAFETY CHAIN ATTACHMENT

ITEMS NOT SHOWN SAME AS "SIDE ELEVATION" OF "HANDRAIL DETAILS"

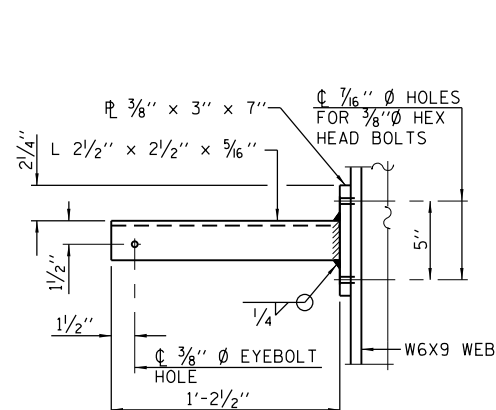


SIDE ELEVATION

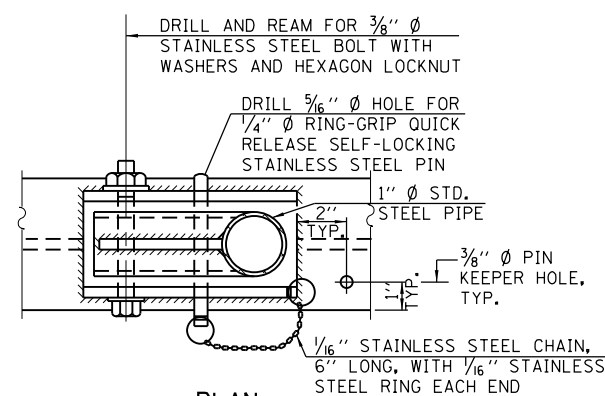


FRONT ELEVATION

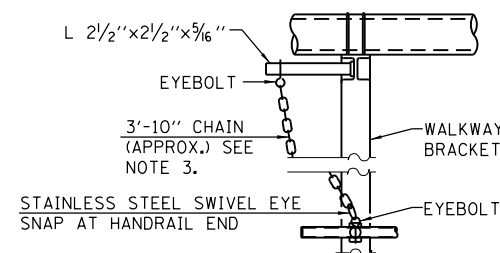
DETAILS NOT SHOWN SAME AS "ELEVATION" AT RIGHT.



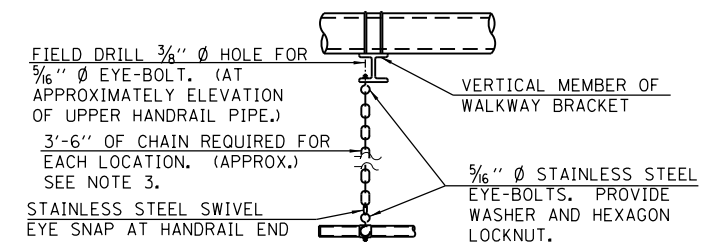
SECTION P-P



**PLAN
DETAIL E HANDRAIL HINGE**



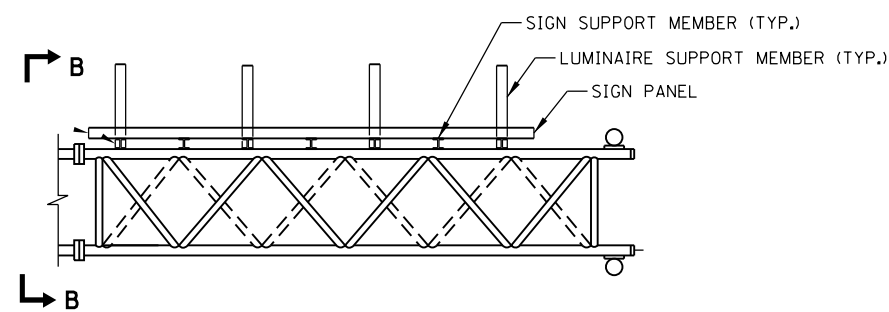
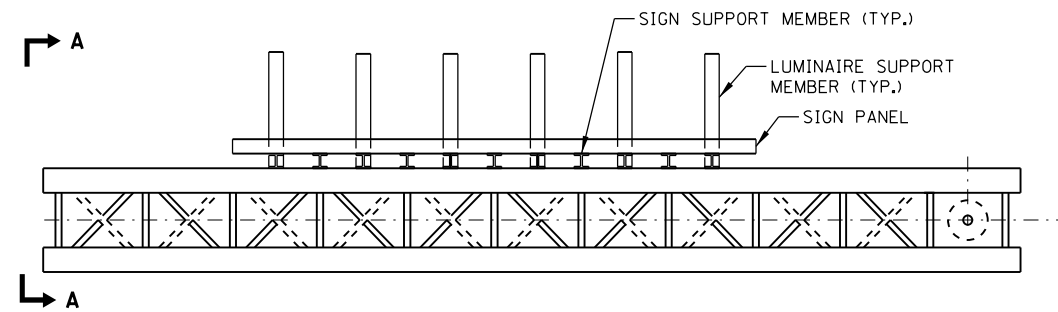
ALTERNATE SAFETY CHAIN ATTACHMENT
DETAILS NOT SHOWN SIMILAR TO "SAFETY CHAIN" DETAILS (WALKWAY OMITTED FOR CLARITY)



SAFETY CHAIN
ONE REQUIRED FOR EACH END OF WALKWAY.

NOTES:

1. INSTALL STANDARD FORCE-FIT END CAPS OR WELD 1/8" END PLATES WITH 1/8" C.F.W. AND GRIND SMOOTH. (ALL RAIL ENDS)
2. HORIZONTAL HANDRAIL MEMBER SHALL BE CONTINUOUS THRU 1 1/4" Ø PIPE. PROVIDE 3/16" Ø HOLE IN 1 1/4" Ø PIPE FOR 3/8" Ø BOLT. FIELD DRILL 3/16" Ø HOLE IN HORIZONTAL RAIL MEMBER. PROVIDE LOCKNUT AND TWO STAINLESS STEEL WASHERS FOR BOLT. (USE 3/16" EYEBOLTS IN 3/16" Ø HOLES ON TOP RAIL AT ENDS ONLY.)
3. 3/16" TYPE 304L STAINLESS STEEL CHAIN, APPROXIMATELY 12 LINKS PER FOOT.



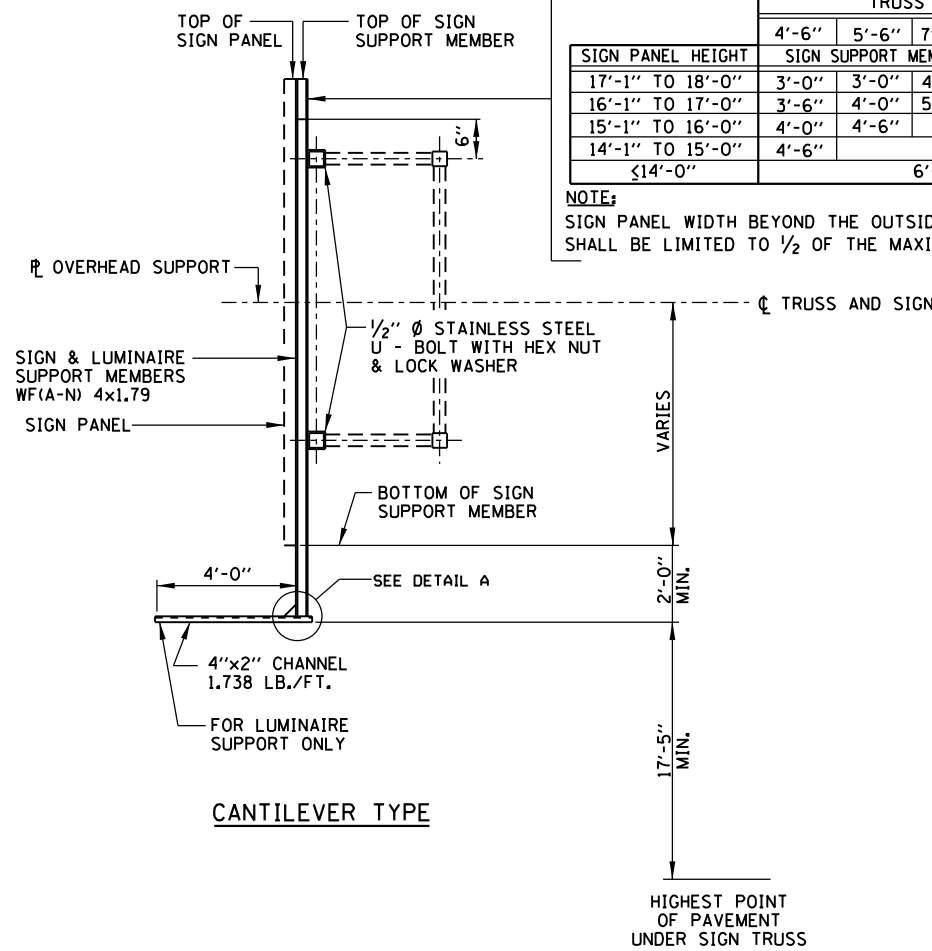
PLAN

PLAN

SIGN SUPPORT MEMBERS-WF(A-N) 4x1.79

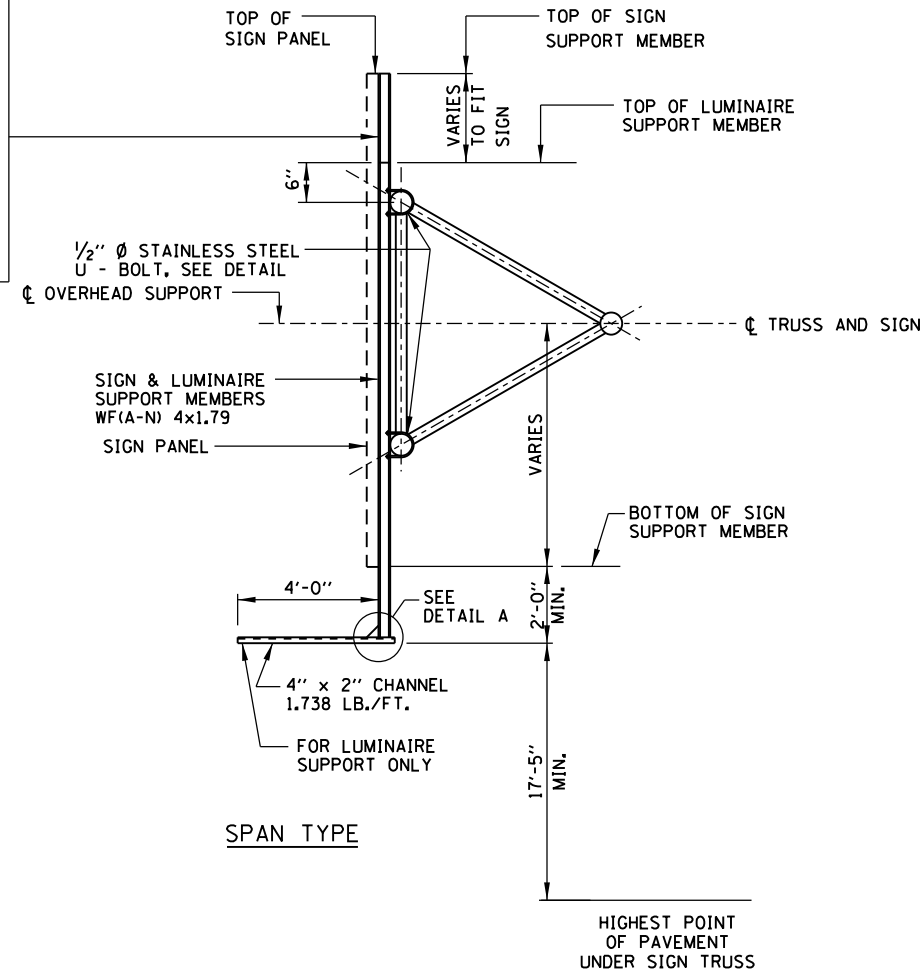
| SIGN PANEL HEIGHT | TRUSS DEPTH | | | | |
|-------------------|-------------|-------|-------|-------|--------|
| | 4'-6" | 5'-6" | 7'-0" | 8'-2" | 8'-10" |
| 17'-1" TO 18'-0" | 3'-0" | 3'-0" | 4'-6" | 5'-6" | 6'-0" |
| 16'-1" TO 17'-0" | 3'-6" | 4'-0" | 5'-0" | 6'-0" | |
| 15'-1" TO 16'-0" | 4'-0" | 4'-6" | 6'-0" | | |
| 14'-1" TO 15'-0" | 4'-6" | 6'-0" | | | |
| ≤14'-0" | 6'-0" | | | | |

NOTE:
SIGN PANEL WIDTH BEYOND THE OUTSIDE VERTICAL MEMBER SHALL BE LIMITED TO 1/2 OF THE MAXIMUM SPACING



CANTILEVER TYPE

SECTION A-A



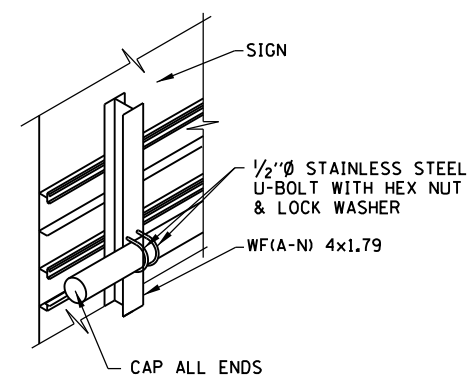
SPAN TYPE

SECTION B-B

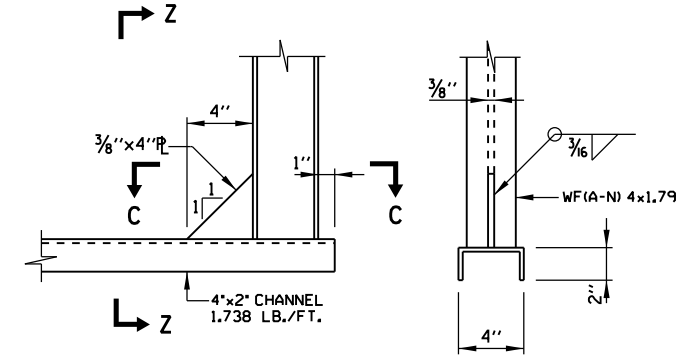
SIGN AND LUMINAIRE SUPPORT DETAIL

NOTES:

1. SIGN PANEL SHALL BE ATTACHED TO TRUSS AS CLOSE TO PANEL JOINTS AS POSSIBLE.
2. LUMINAIRE SUPPORT MEMBERS TO BE INSTALLED ONLY WHEN SIGN STRUCTURE IS TO BE ILLUMINATED.

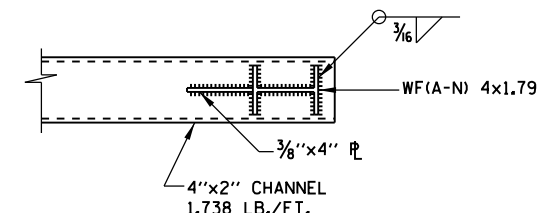


STAINLESS STEEL U-BOLT DETAIL



DETAIL A

SECTION Z-Z



SECTION C-C

NOTES:

ALL MATERIAL IS ALUMINUM (UNLESS OTHERWISE NOTED).

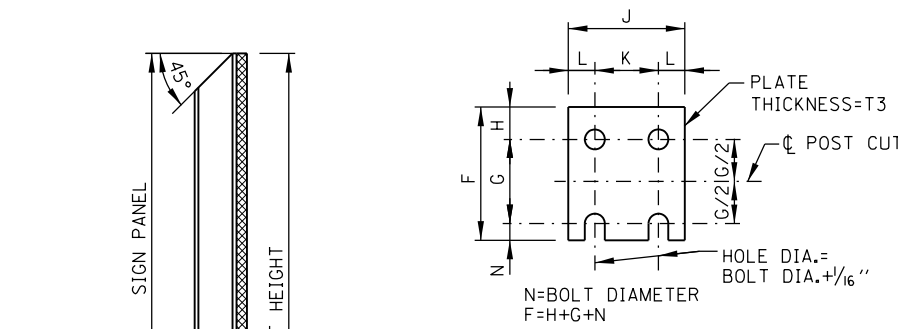
APPROVED, *Paul Kovacs* DATE 2-7-2012...
CHIEF ENGINEERING OFFICER

| DATE | REVISIONS |
|-----------|---------------------------------------------------------|
| 1-1-2009 | ADDED PLAN VIEWS FOR SIGN STRUCTURES |
| 2-7-2012 | REVISED OVERHEAD SIGN STRUCTURE CANTILEVER DIAGONALS |
| 2-1-2013 | REMOVED VERTICAL CLEARANCE. |
| 3-31-2014 | REVISED SIGN SUPPORT MEMBERS |
| 3-11-2015 | REVISED VERTICAL CL. AND SIGN SUPPORT |
| 3-01-2018 | ADDED VERTICAL CLEARANCE |
| 3-01-2019 | REVISED NOTE 2 |



OVERHEAD SIGN STRUCTURE
SIGN AND LUMINAIRE
SUPPORTS

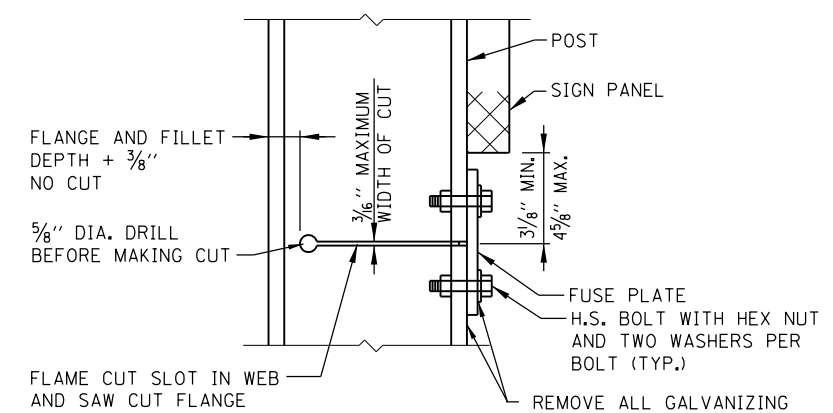
STANDARD F8-07



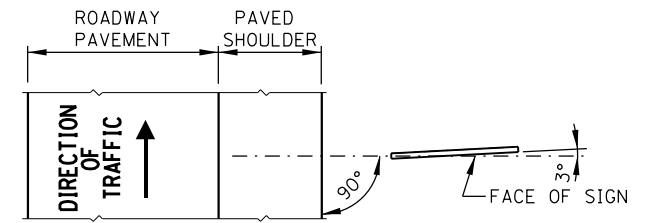
FUSE PLATE DETAIL
INSTALL WITH NOTCHES TOWARDS BASE

| G & H DIM. TABLE | | |
|------------------|--------|--------|
| BOLT DIA. | G | H |
| 1/2" | 2" | 1 1/8" |
| 5/8" | 2 1/4" | 1 1/4" |
| 3/4" | 2 1/2" | 1 3/8" |
| 7/8" | 2 3/4" | 1 1/2" |
| 1" | 3" | 1 5/8" |
| 1 1/8" | 3 1/4" | 1 3/4" |
| 1 1/4" | 3 1/2" | 1 7/8" |

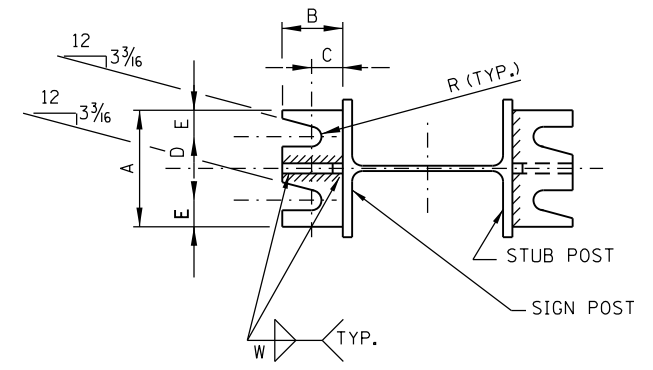
FABRICATORS NOTES
THE SLOT AND THE 5/8" DIA. HOLE IN THE WEB AND THE FUSE PLATE BOLT HOLES IN THE FLANGE SHALL BE MADE BEFORE GALVANIZING. POST FLANGE SHALL BE SAW CUT AFTER GALVANIZING AND BARE METAL SURFACES SHALL BE COATED WITH AN APPROVED ZINC SOLDER OR ZINC-RICH PAINT. THESE SURFACES SHALL NOT BE COATED UNTIL THE FUSE PLATE IS INSTALLED AND BOLTS FULLY TIGHTENED.



HINGE JOINT DETAIL A



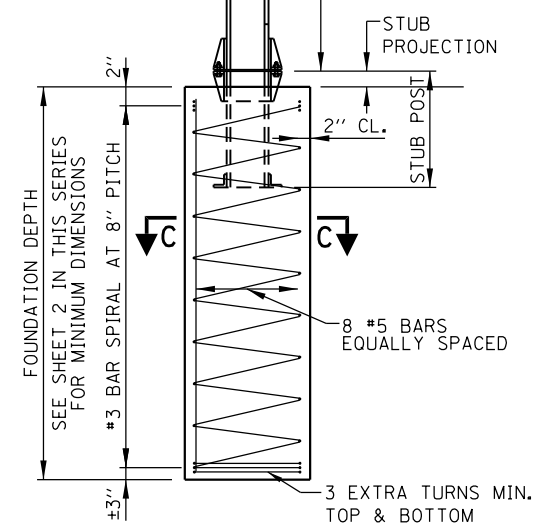
LOCATION SKETCH



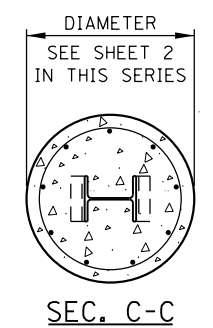
SEC. A-A

GENERAL NOTES

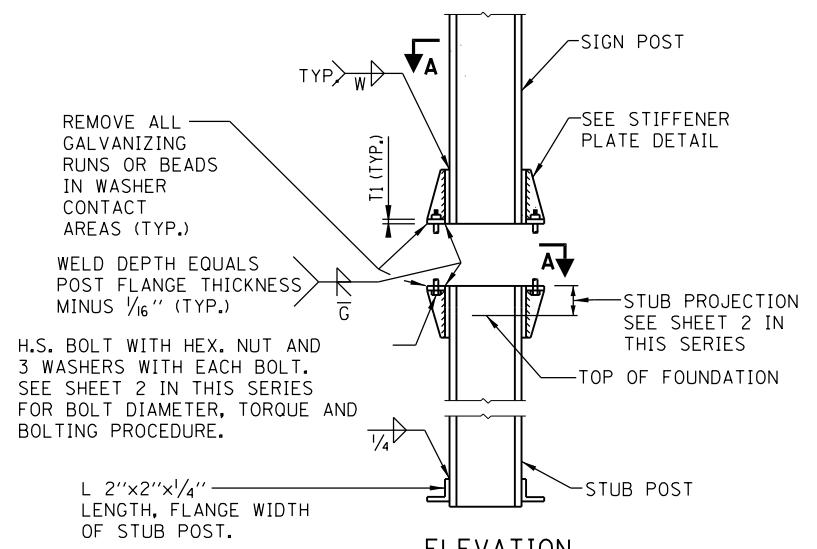
- DESIGN:** THE LATEST EDITION OF THE "AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRE AND TRAFFIC SIGNALS".
- CONSTRUCTION:** STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS.
- LOADING:** FOR 80 MPH WIND VELOCITY PLUS 30% GUST FACTOR NORMAL TO SIGN.
- DESIGN STRESSES:**
STRUCTURAL STEEL - PER AASHTO 20,000 P.S.I.
REINFORCING STEEL - 24,000 P.S.I.
CLASS SI CONCRETE - 1,400 P.S.I.
- FOUNDATION:** MINIMUM UNCONFINED COMPRESSIVE STRENGTH, Q_u FOR ALL LAYERS FOR COHESIVE SOILS (CLAYS) SHALL BE 1.25 TON/SQ.FT.
- WELDING:** ALL WELDING TO BE CONTINUOUS UNLESS OTHERWISE SHOWN. ALL WELDING TO BE DONE IN ACCORDANCE WITH CURRENT AWS SPECIFICATIONS, AND STANDARD SPECIFICATIONS.
- MATERIALS:** ALL STRUCTURAL STEEL SHALL CONFORM TO ASTM A36 AND LRFD SPECIFICATIONS.
- ALL HIGH STRENGTH STEEL BOLTS, NUTS AND WASHERS SHALL CONFORM TO STANDARD SPECIFICATIONS.
- HIGH STRENGTH STEEL BOLTS, NUTS AND HARDENED WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO M232.
- HIGH STRENGTH BOLTS IN BASE PLATES SHALL BE TIGHTENED TO THE TORQUE SHOWN ON SHEET 2 IN THIS SERIES.
- AFTER FABRICATION, THE POST, FUSE PLATE, BASE PLATE AND UPPER 6" OF STUB POST SHALL BE HOT-DIP GALVANIZED ACCORDING TO ASTM M111, EXCEPT AS NOTED UNDER FABRICATOR NOTES.



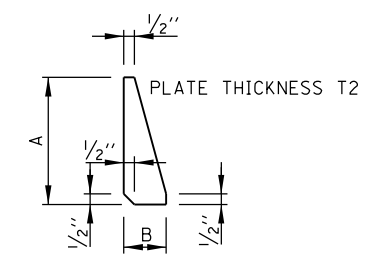
SIDE ELEVATION POST & FOUNDATION



SEC. C-C

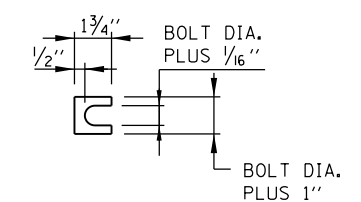


ELEVATION SIGN POST & STUB POST



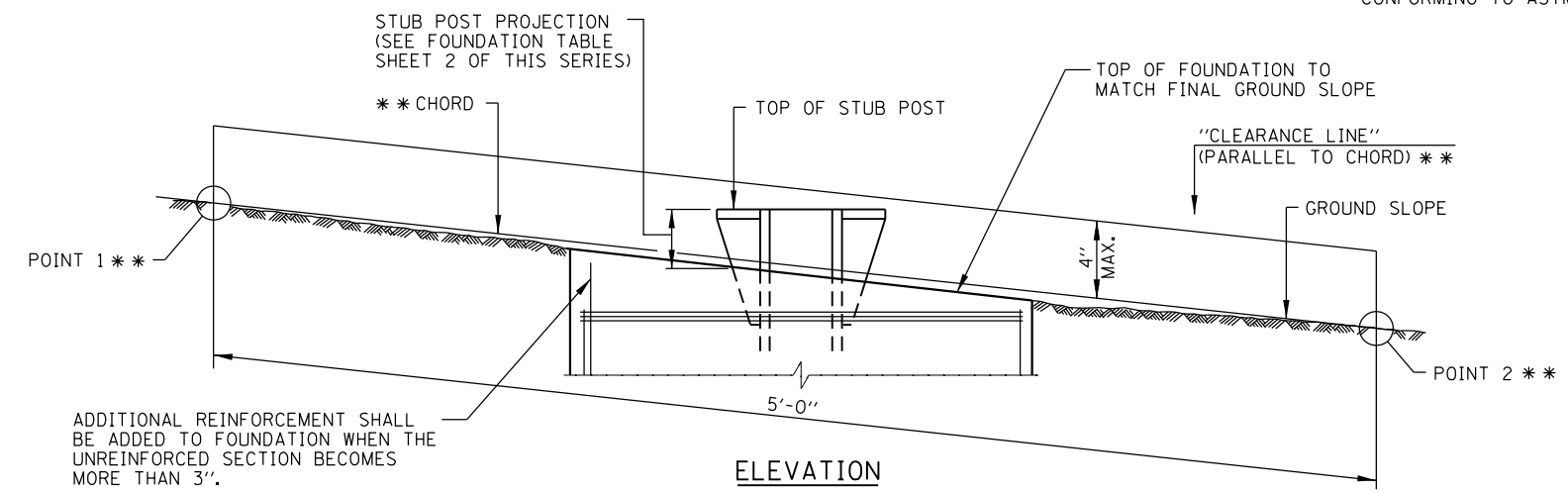
STIFFENER PLATE DETAIL

SEE SHEET 2 IN THIS SERIES FOR DIMENSIONS



SHIM DETAIL


FURNISH 2-.012" THICK AND 2-.032" THICK SHIMS PER POST. SHIMS SHALL BE FABRICATED FROM BRASS SHIM STOCK CONFORMING TO ASTM B36.



ELEVATION GROUND LINE & STUB POST

** FOR ALL "POINT 1" AND "POINT 2" LOCATIONS, "CLEARANCE LINE" MUST BE AT OR ABOVE TOP OF STUB POST.

| DATE | REVISIONS |
|-----------|--------------------------------------------------------------------------------------|
| 2-7-2012 | ADDED STUB POST CLEARANCE DIMENSIONS, REVISED SIGN INSTALLATION CLEARANCE DIMENSIONS |
| 11-1-2012 | REVISED NOTES, MODIFIED SLOPE REQUIREMENTS FOR BREAKAWAY SUPPORTS |
| 3-01-2019 | CLARIFIED DESIGN STRESS FOR SOIL PRESSURE |



BREAKAWAY SIGN SUPPORT DETAILS

STANDARD F9-05

| POST | FOUNDATION TABLE | | | | | | | | | | | | BASE CONNECTION DATA TABLE | | | | | | | | | | |
|--------|------------------|------------|------------|---------------|------|-------|-------------|------|-------|-----------|------------|-----------------|----------------------------|--------------------------------------|-----|-----|-----|-----|-----|-----|----|---------|---------|
| | FOUNDATION | | | REINFORCEMENT | | | | | | STUB POST | | | BOLT SIZE AND TORQUE | A | B | C | D | E | T1 | T2 | W | R | |
| | | | | VERTICAL BARS | | | BAR SPIRALS | | | | STUB LGTH. | STUB PROJECTION | | | | | | | | | | | LBS.*** |
| | DIA. | MIN. DEPTH | CY.* CONC. | NO. | SIZE | LGTH. | SIZE | O.D. | LGTH. | LBS.** | | | | | | | | | | | | | |
| W6x9 | 2'-0" | 6'-0" | .70 | 8 | #5 | 5'-9" | #3 | 20½" | 79' | 78 | 2'-3" | 3" | 44 | 5/8" Ø x 3/4" LG. TORQUE = 450" # | 6" | 2¼" | 1¼" | 3½" | 1¼" | ¾" | ½" | ¼" | 11/32 " |
| W6x15 | 2'-0" | 6'-0" | .70 | 8 | #5 | 5'-9" | #3 | 20½" | 79' | 78 | 2'-6" | 3" | 71 | | | | | | | | | | |
| W8x18 | 2'-0" | 6'-0" | .70 | 8 | #5 | 5'-9" | #3 | 20½" | 79' | 78 | 2'-6" | 3" | 85 | | | | | | | | | | |
| W10x22 | 2'-6" | 6'-6" | 1.18 | 8 | #5 | 6'-3" | #3 | 26½" | 105' | 92 | 3'-0" | 2½" | 110 | ¾" Ø x 3¾" LG. TORQUE = 750" # | 6" | 2½" | 1¾" | 3¼" | 1¾" | 1" | ½" | 5/16 " | 13/32 " |
| W10x26 | 2'-6" | 7'-0" | 1.27 | 8 | #5 | 6'-9" | #3 | 26½" | 112' | 98 | 3'-0" | 2½" | 137 | | | | | | | | | | |
| W12x26 | 2'-6" | 7'-9" | 1.41 | 8 | #5 | 7'-6" | #3 | 26½" | 119' | 107 | 3'-0" | 2½" | 140 | | | | | | | | | | |
| W14x30 | 3'-0" | 7'-3" | 1.90 | 8 | #5 | 7'-0" | #3 | 32½" | 145' | 113 | 3'-0" | 2½" | 150 | 7/8" Ø x 4" LG. TORQUE = 950" # | 7" | 2¾" | 1½" | 4" | 1½" | 1" | ¾" | ¾" | 15/32 " |
| W14x38 | 3'-0" | 8'-0" | 2.09 | 8 | #5 | 7'-9" | #3 | 32½" | 153' | 122 | 3'-6" | 2½" | 208 | | | | | | | | | | |
| W16x45 | 3'-0" | 8'-6" | 2.23 | 8 | #5 | 8'-3" | #3 | 32½" | 162' | 130 | 3'-6" | 2½" | 233 | | | | | | | | | | |
| | | | | | | | | | | | | | | 1" Ø x 4½" LG. TORQUE = 1100" # | 7½" | 3" | 1¾" | 4" | 1¾" | 1¼" | ¾" | 17/32 " | |

* QUANTITY OF CLASS SI CONCRETE CONSISTS OF ALL CONCRETE NECESSARY FOR ONE FOUNDATION. (CUBIC YARDS)

** THIS INCLUDES REINFORCEMENT BARS AND SPIRAL HOOPING REQUIRED FOR ONE FOUNDATION.

*** INCLUDES WEIGHT OF STUB POST WITH ANGLES, GUSSETS, BASE PLATES, BOLTS, NUTS, WASHERS, PLUS BASE PLATES AND GUSSETS ON MAIN POST, PLUS FUSE PLATE (IF ANY) WITH BOLTS, NUTS AND WASHERS. (ONE POST)

EQUIVALENT TORQUE VALUES

450" # = 37.5' #
750" # = 62.5' #
950" # = 79.2' #
1100" # = 91.7' #

| POST | FUSE PLATE DATA TABLE | | | | FUSE PLATE BOLT SIZE TABLE | | | | | | | | | | |
|--------|-----------------------|-----|-------|------|----------------------------|-----------|----------|-----------|-----------|------------|-----------|-----------|-----------|-----------|-----------|
| | | | | | SIGN DEPTH | | | | | | | | | | |
| | J | K | L | T3 | 4' | 5' | 6' | 7' | 8' | 9' | 10' | 11' | 12' | 13' | 14' |
| W6x9 | 4" | 2¼" | 7⁄8" | ¼" | ½"Øx1½" | ½"Øx1½" | ½"Øx1½" | 5⁄8"Øx1¾" | 5⁄8"Øx1¾" | 5⁄8"Øx1¾" | --- | --- | --- | --- | --- |
| W6x15 | 6" | 3½" | 1¼" | ¾" | ½"Øx1¾" | ½"Øx1¾" | 5⁄8"Øx2" | 5⁄8"Øx2" | ¾"Øx2" | ¾"Øx2" | ¾"Øx2" | ¾"Øx2" | 7⁄8"Øx2" | 7⁄8"Øx2" | --- |
| W8x18 | 5¼" | 2¾" | 1¼" | ¾" | ½"Øx1¾" | ½"Øx1¾" | ½"Øx1¾" | 5⁄8"Øx2" | 5⁄8"Øx2" | ¾"Øx2" | ¾"Øx2" | 7⁄8"Øx2¼" | 7⁄8"Øx2¼" | 7⁄8"Øx2¼" | 7⁄8"Øx2¼" |
| W10x22 | 5¾" | 2¾" | 1½" | ½" | ½"Øx2" | ½"Øx2" | ½"Øx2" | 5⁄8"Øx2" | 5⁄8"Øx2" | ¾"Øx2¼" | ¾"Øx2¼" | 7⁄8"Øx2¼" | 7⁄8"Øx2¼" | 7⁄8"Øx2½" | 1"Øx2½" |
| W10x26 | 5¾" | 2¾" | 1½" | 5⁄8" | ½"Øx2" | ½"Øx2" | ½"Øx2" | 5⁄8"Øx2¼" | 5⁄8"Øx2¼" | ¾"Øx2½" | ¾"Øx2½" | 7⁄8"Øx2½" | 7⁄8"Øx2½" | 1"Øx2¾" | 1"Øx2¾" |
| W12x26 | 6½" | 3½" | 1½" | 5⁄8" | --- | --- | --- | --- | --- | 5⁄8"Øx 2¼" | --- | --- | 7⁄8"Øx2½" | 7⁄8"Øx2½" | 1"Øx2½" |
| W14x30 | 6¾" | 3½" | 15⁄8" | ½" | ½"Øx2" | ½"Øx2" | ½"Øx2" | ½"Øx2" | ½"Øx2" | 5⁄8"Øx2" | 5⁄8"Øx2¼" | ¾"Øx2¼" | ¾"Øx2¼" | 7⁄8"Øx2½" | 1"Øx2½" |
| W14x38 | 6¾" | 3½" | 15⁄8" | ½" | --- | ½"Øx2" | ½"Øx2" | ½"Øx2" | ½"Øx2" | 5⁄8"Øx2¼" | 5⁄8"Øx2¼" | ¾"Øx2½" | ¾"Øx2½" | 7⁄8"Øx2½" | 7⁄8"Øx2½" |
| W16x45 | 7" | 3½" | 1¾" | ½" | --- | --- | --- | ½"Øx2" | ½"Øx2" | 5⁄8"Øx2¼" | 5⁄8"Øx2¼" | 5⁄8"Øx2¼" | ¾"Øx2½" | ¾"Øx2½" | 7⁄8"Øx2½" |
| POST | FUSE PLATE DATA TABLE | | | | FUSE PLATE BOLT SIZE TABLE | | | | | | | | | | |
| | | | | | SIGN DEPTH | | | | | | | | | | |
| | J | K | L | T3 | 15' | 16' | 17' | 18' | 19' | 20' | 21' | 22' | 23' | 24' | --- |
| W6x9 | 4" | 2¼" | 7⁄8" | ¼" | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| W6x15 | 6" | 3½" | 1¼" | ¾" | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| W8x18 | 5¼" | 2¾" | 1¼" | ¾" | 7⁄8"Øx2¼" | 7⁄8"Øx2¼" | --- | --- | --- | --- | --- | --- | --- | --- | --- |
| W10x22 | 5¾" | 2¾" | 1½" | ½" | 1"Øx2¾" | 1"Øx2¾" | 1"Øx2¾" | 1"Øx2¾" | 1"Øx2¾" | 1"Øx2¾" | --- | --- | --- | --- | --- |
| W10x26 | 5¾" | 2¾" | 1½" | 5⁄8" | 1"Øx2¾" | 1⁄8"Øx3" | 1⁄8"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | --- |
| W12x26 | 6½" | 3½" | 1½" | 5⁄8" | 1"Øx2¾" | 1"Øx2¾" | 1⁄8"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | --- |
| W14x30 | 6¾" | 3½" | 15⁄8" | ½" | 1"Øx2¾" | 1"Øx2¾" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | --- |
| W14x38 | 6¾" | 3½" | 15⁄8" | ½" | 1"Øx2½" | 1"Øx2¾" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | --- |
| W16x45 | 7" | 3½" | 1¾" | ½" | 7⁄8"Øx2½" | 1"Øx2¾" | 1"Øx2¾" | 1⁄8"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | 1¼"Øx3" | --- |

PROCEDURE FOR ASSEMBLY OF BASE CONNECTION:

1. ASSEMBLE POST TO STUB WITH H.S. BOLTS AND ONE OF THE THREE FLAT WASHERS ON EACH BOLT BETWEEN PLATES AS SHOWN.
2. SHIMS MAY BE USED BETWEEN PLATES TO LEVEL POST.
3. TIGHTEN BOLTS IN BASE PLATE IN A SYSTEMATIC ORDER TO THE REQUIRED TORQUE.
4. LOOSEN EACH BOLT AND RETIGHTEN TO THE REQUIRED TORQUE IN SAME ORDER AS INITIAL TIGHTENING.
5. BURR OR CENTER PUNCH THREADS AT JUNCTURE OF BOLT AND NUT TO PREVENT NUT FROM LOOSENING.

PROCEDURE FOR FUSE PLATE BOLT TIGHTENING:

ALL FRICTION FUSE BOLTS SHALL BE TIGHTENED IN THE SHOP AS APPROVED BY THE ENGINEER ACCORDING TO ONE OF THE FOLLOWING METHODS:

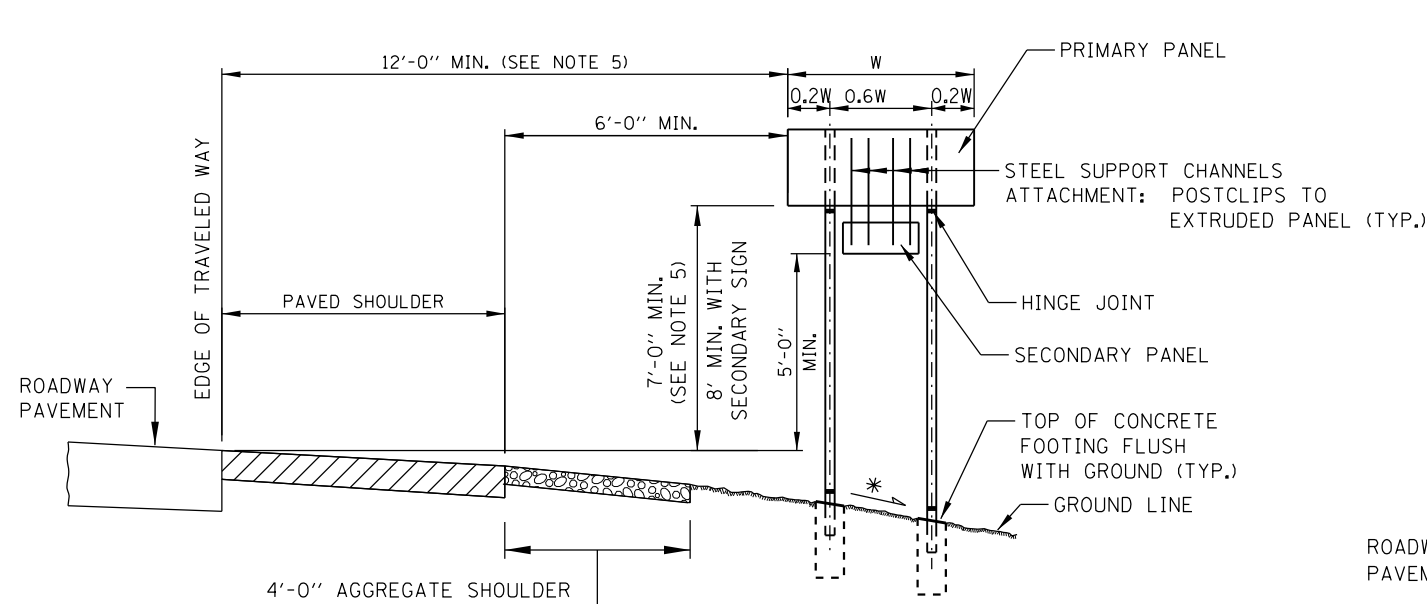
1. TURN-OF-NUT TIGHTENING.
2. TIGHTENING BY USE OF A DIRECT TENSION INDICATOR.

THE ABOVE METHODS OF INSTALLATION AND TIGHTENING SHALL CONFORM TO THE LATEST ISSUE OF THE SPECIFICATION FOR STRUCTURAL JOINTS USING ASTM A-325 OR A-490 BOLTS, FOR SLIP-CRITICAL CONNECTIONS AS ISSUED BY THE RESEARCH COUNCIL ON RIVETED AND BOLTED STRUCTURAL JOINTS OF THE ENGINEERING FOUNDATION.

TIGHTENING SHALL BE TO SUCH A DEGREE AS TO OBTAIN THE FOLLOWING MINIMUM RESIDUAL TENSION IN EACH BOLT.

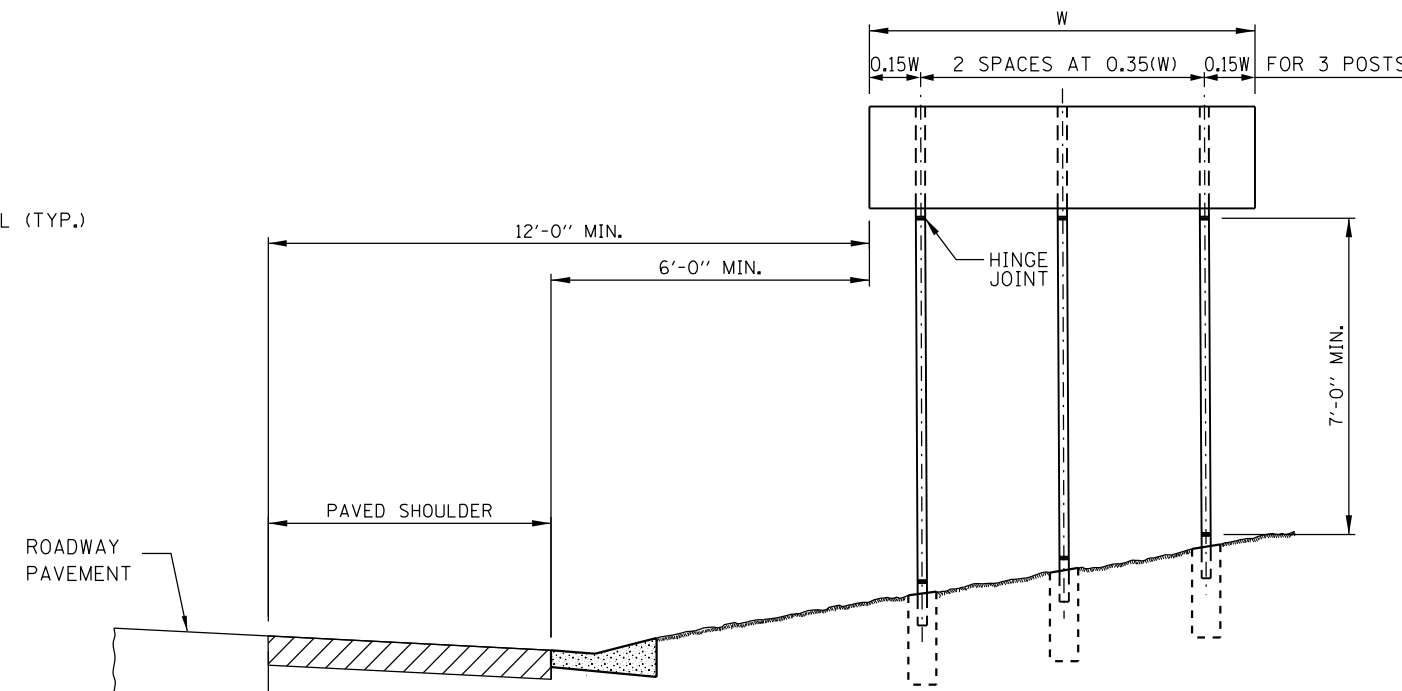
| BOLT DIA. | MIN. RESIDUAL BOLT TENSION | BOLT DIA. | MIN. RESIDUAL BOLT TENSION | BOLT DIA. | MIN. RESIDUAL BOLT TENSION |
|-----------|-------------------------------|-----------|-------------------------------|-----------|-------------------------------|
| ½" | 12,050 | 7⁄8" | 39,250 | 1¼" | 71,700 |
| 5⁄8" | 19,200 | 1" | 51,500 | | |
| ¾" | 28,400 | 1⅛" | 56,450 | | |





CONDITION 1 - SIGN INSTALLATION

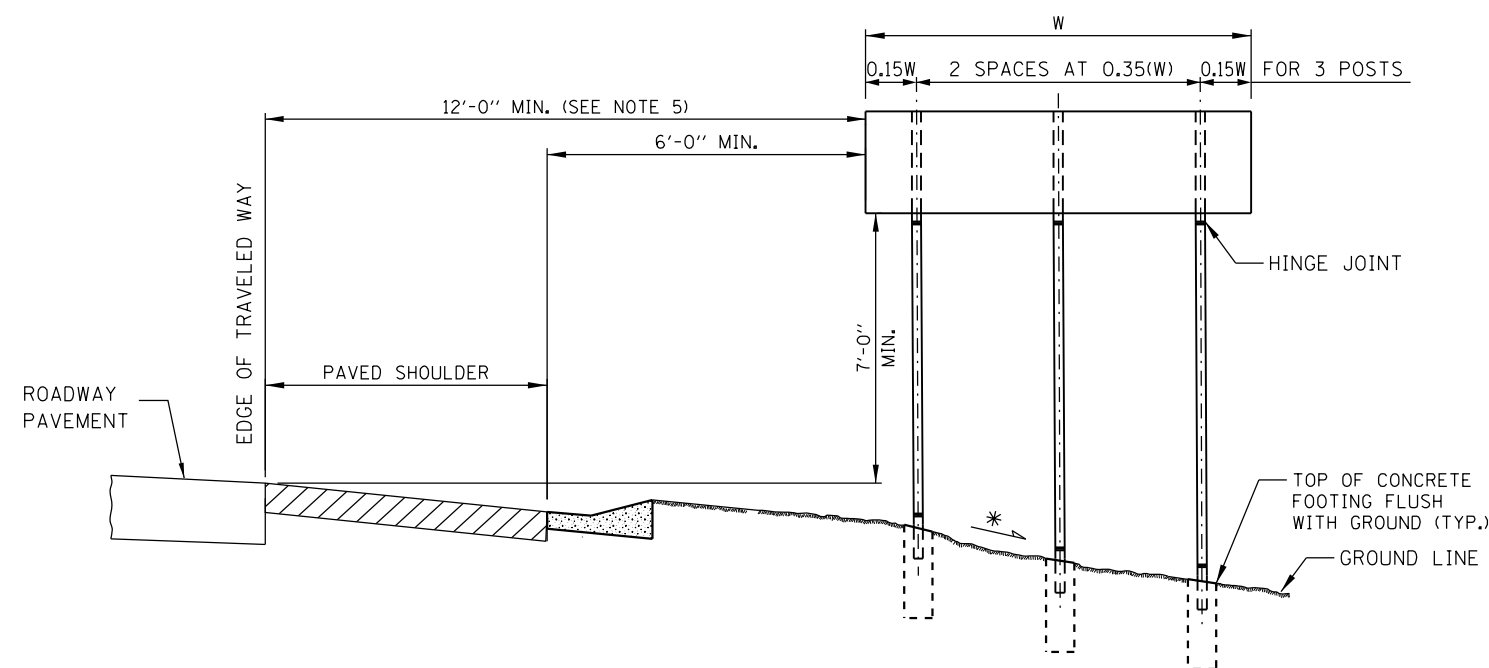
(*) FORESLOPE 1:6 (V:H) OR FLATTER



CONDITION 3 - SIGN INSTALLATION

NOTES:

- SEE SIGN INSTALLATION SCHEDULE IN CONTRACT PLANS FOR DIMENSIONS.
- THE DIMENSIONS OF ALL POSTS FOR GROUND MOUNTED SIGNS ARE BASED ON DESIGN CROSS SECTIONS. THE CONTRACTOR SHALL VERIFY REQUIRED POST LENGTHS IN THE FIELD, PRIOR TO SUBMITTING SHOP DRAWINGS AND POST FABRICATION TO MAINTAIN THE CLEARANCES SHOWN.
- SIGN FOUNDATION ELEVATIONS TO BE BASED ON FINISHED SLOPES.
- ANY ADDITIONAL SIGN TO BE ADDED LATER MUST BE SUPPORTED BY THE EXISTING SIGN PANEL AND NOT THE SIGN POST. MINIMUM CLEARANCES SHALL BE MAINTAINED.
- SIGNS THAT ARE PLACED WELL OUTSIDE THE CLEAR ZONE MAY BE INSTALLED WITH A MINIMUM HEIGHT OF 5 FEET, MEASURED VERTICALLY FROM THE BOTTOM OF THE SIGN TO THE HORIZONTAL ELEVATION OF THE NEAR EDGE OF TRAVELED ROADWAY.
- MINIMUM HEIGHT OF LOWEST POST SHALL BE 7'-0" MEASURED BETWEEN STUB PROJECTION AND HINGE JOINT.
- FOR TWO POSTS SPACED LESS THAN 7 FEET APART, EACH POST SHALL HAVE A MASS LESS THAN 18 lb/ft.
- WHEN THE TOTAL COMBINED WEIGHT OF THE TWO POSTS LOCATED WITHIN 7 FEET OF EACH OTHER EXCEEDS 600 lbs., THE SIGN SHALL BE PLACED WELL OUTSIDE THE CLEAR ZONE OR BE SHIELDED FROM VEHICULAR IMPACT.



CONDITION 2 - SIGN INSTALLATION

(*) FORESLOPE 1:6 (V:H) OR FLATTER

UNSHIELDED SLOPE

SHEET 3 OF 4



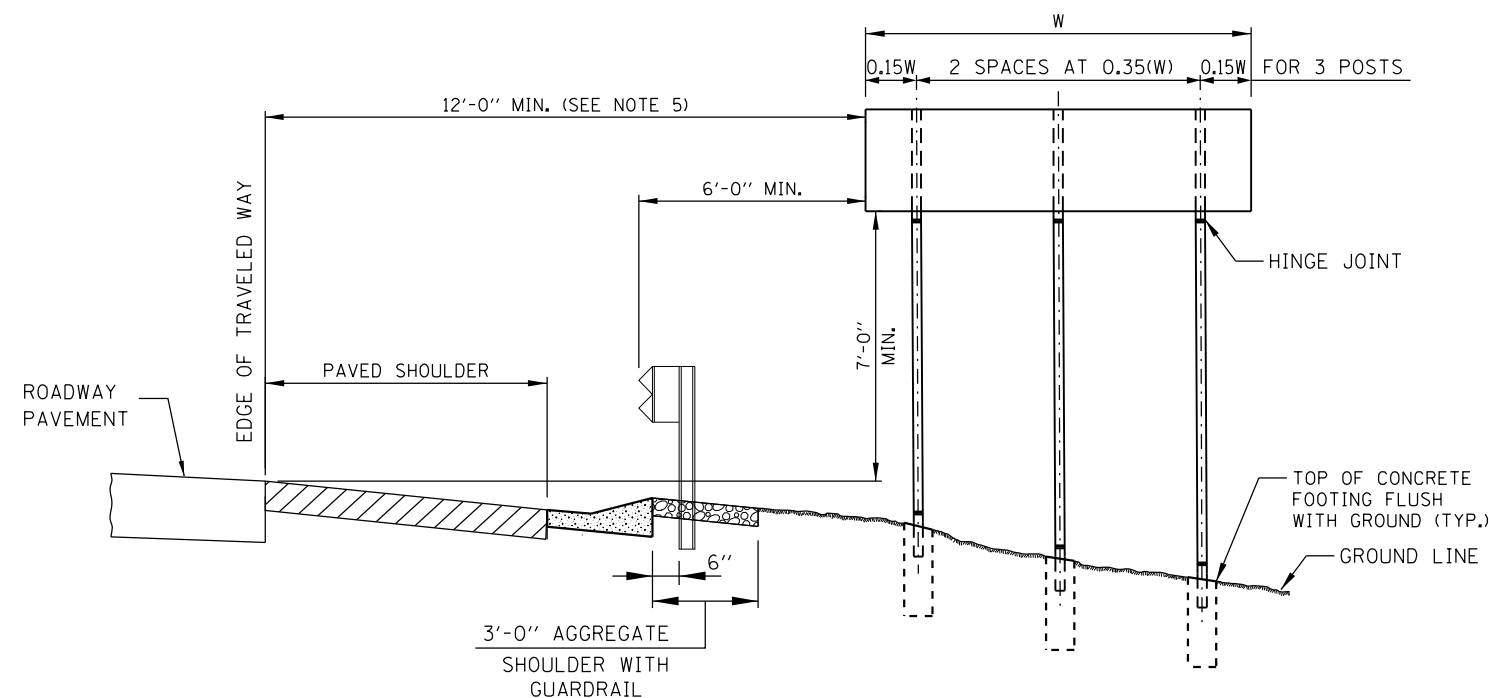
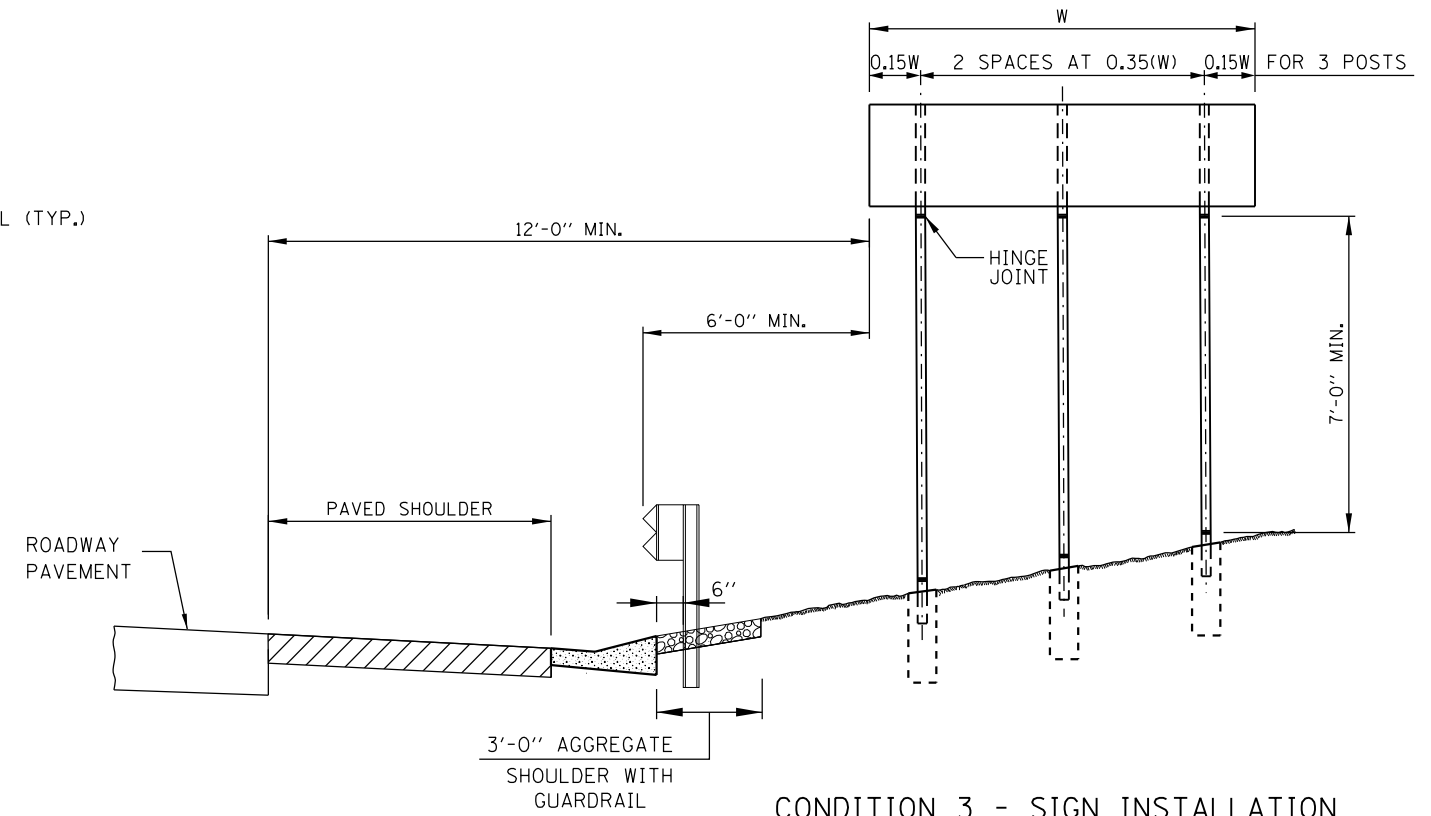
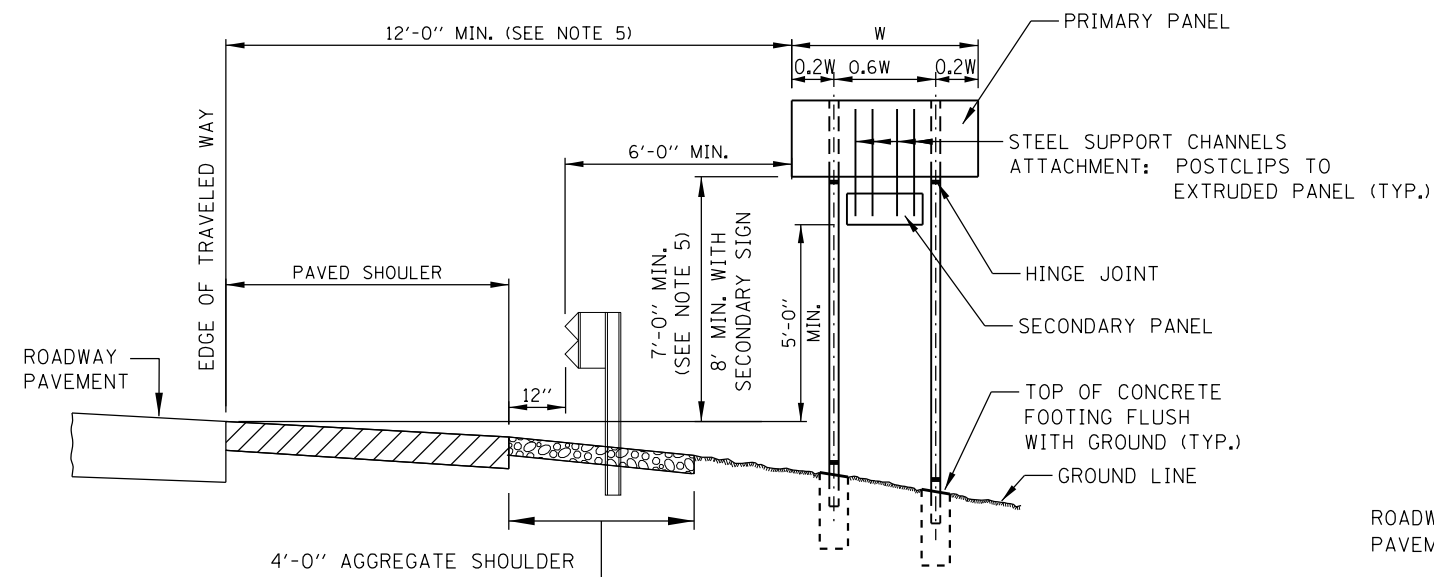
BREAKAWAY SIGN SUPPORT
DETAILS

STANDARD F9-05

APPROVED.....
CHIEF ENGINEER

Paul Kovacs

DATE 1-1-2010.....



NOTES:

1. SEE SIGN INSTALLATION SCHEDULE IN CONTRACT PLANS FOR DIMENSIONS.
2. THE DIMENSIONS OF ALL POSTS FOR GROUND MOUNTED SIGNS ARE BASED ON DESIGN CROSS SECTIONS. THE CONTRACTOR SHALL VERIFY REQUIRED POST LENGTHS IN THE FIELD, PRIOR TO SUBMITTING SHOP DRAWINGS AND POST FABRICATION TO MAINTAIN THE CLEARANCES SHOWN.
3. SIGN FOUNDATION ELEVATIONS TO BE BASED ON FINISHED SLOPES.
4. ANY ADDITIONAL SIGN TO BE ADDED LATER MUST BE SUPPORTED BY THE EXISTING SIGN PANEL AND NOT THE SIGN POST. MINIMUM CLEARANCES SHALL BE MAINTAINED.
5. SIGNS THAT ARE PLACED WELL OUTSIDE THE CLEAR ZONE MAY BE INSTALLED WITH A MINIMUM HEIGHT OF 5 FEET, MEASURED VERTICALLY FROM THE BOTTOM OF THE SIGN TO THE HORIZONTAL ELEVATION OF THE NEAR EDGE OF TRAVELED ROADWAY.
6. MINIMUM HEIGHT OF LOWEST POST SHALL BE 7'-0" MEASURED BETWEEN STUB PROJECTION AND HINGE JOINT.
7. FOR TWO POSTS SPACED LESS THAN 7 FEET APART, EACH POST SHALL HAVE A MASS LESS THAN 18 lb/ft.
8. WHEN THE TOTAL COMBINED WEIGHT OF THE TWO POSTS LOCATED WITHIN 7 FEET OF EACH OTHER EXCEEDS 600 lbs., THE SIGN SHALL BE PLACED WELL OUTSIDE THE CLEAR ZONE OR BE SHIELDED FROM VEHICULAR IMPACT.

SHEET 4 OF 4



BREAKAWAY SIGN SUPPORT DETAILS

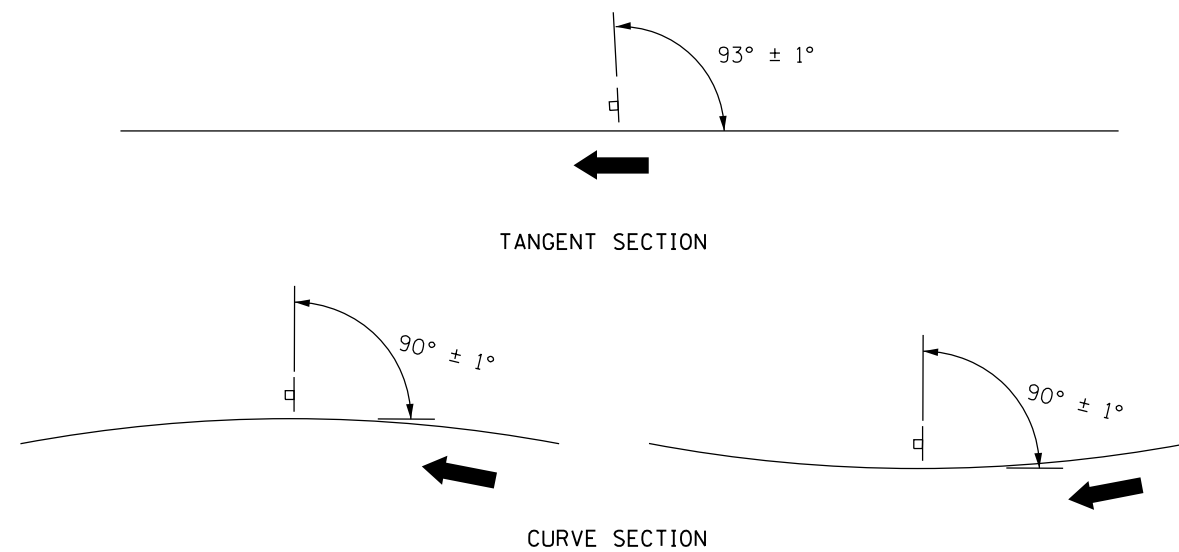
STANDARD F9-05

SHIELDED SLOPE

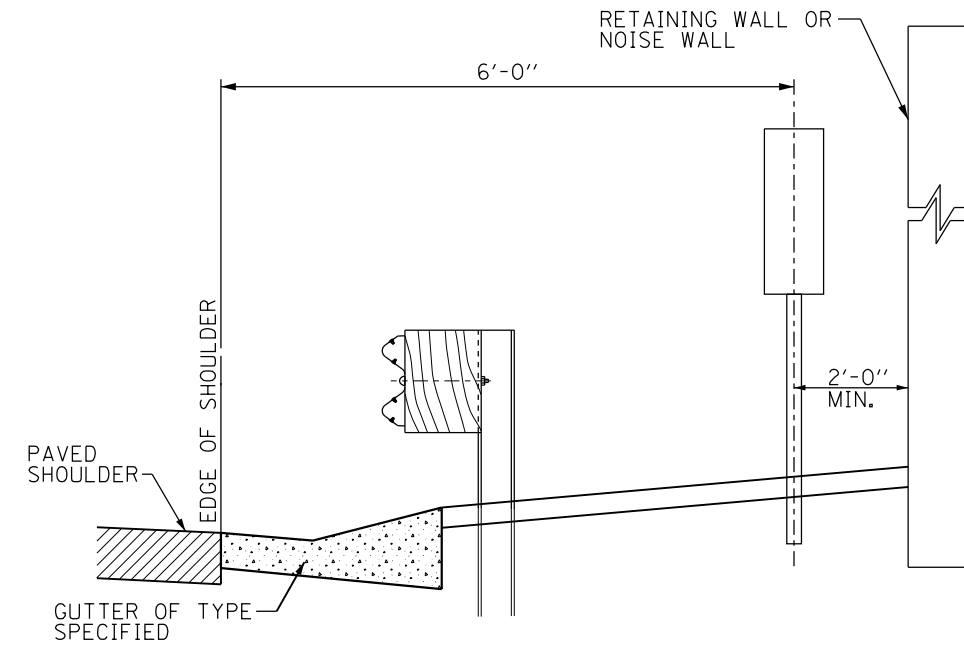
APPROVED.....
CHIEF ENGINEER

DATE 1-1-2010.....

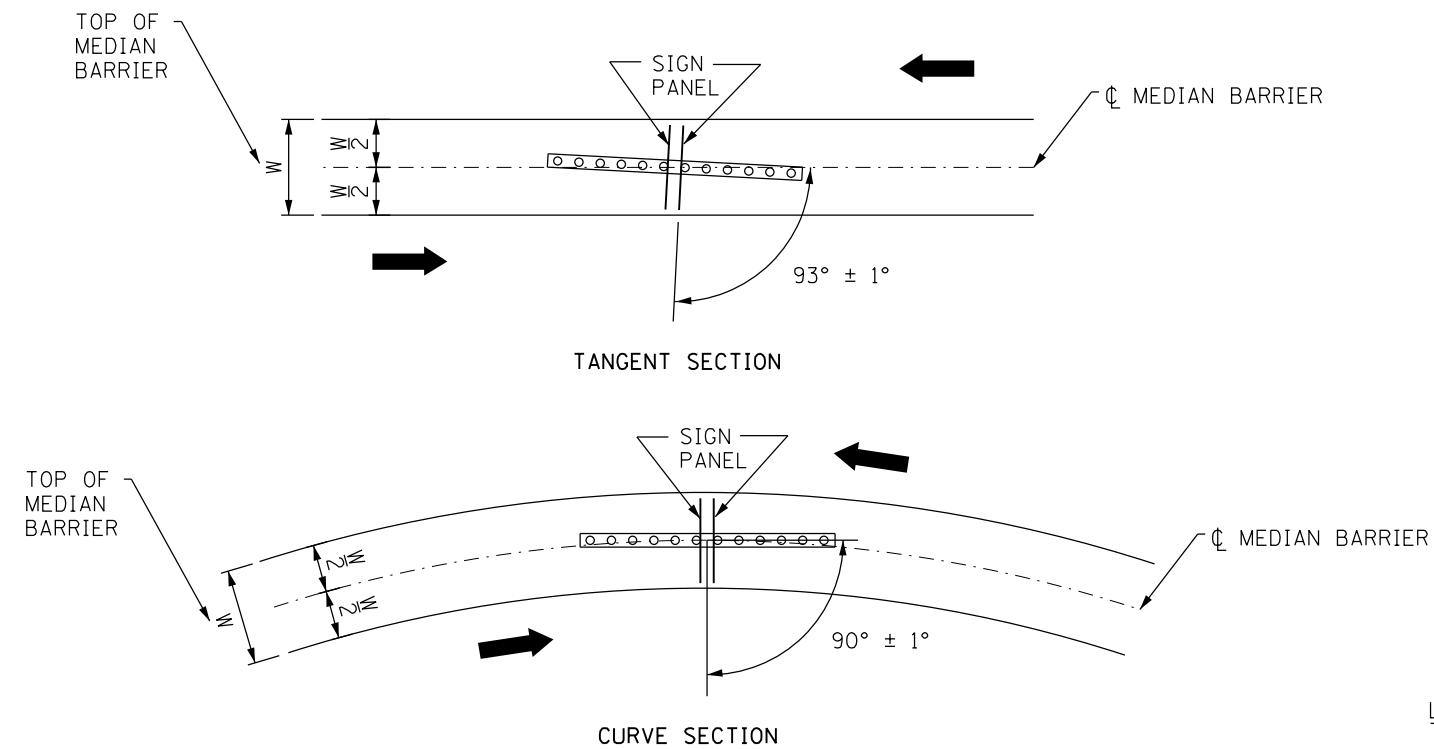
Paul Kovacs



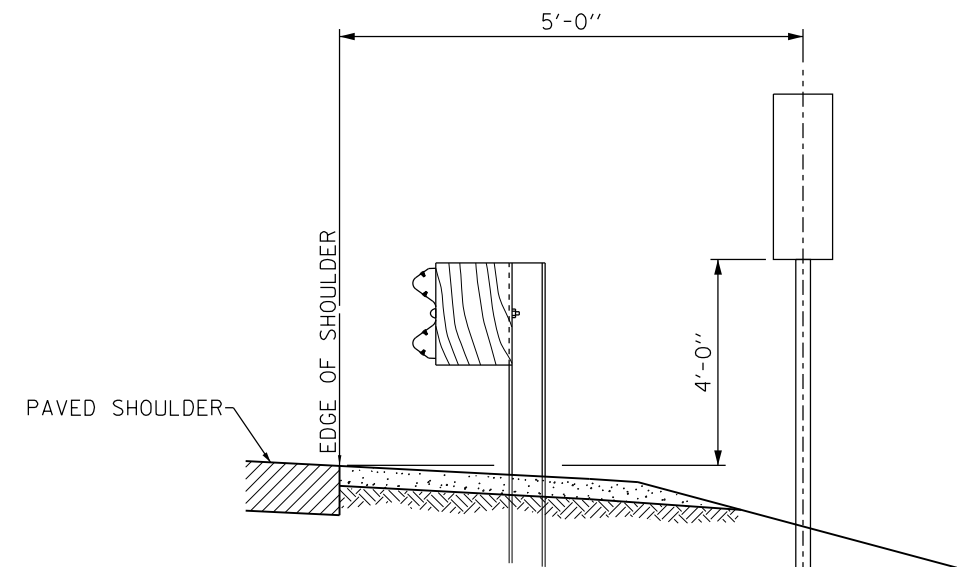
GROUND MOUNT SIGN POSITIONING
NOT TO SCALE



SECTION WITH GUTTER
NOT TO SCALE



MEDIAN BARRIER SIGN POSITIONING
NOT TO SCALE



SECTION WITHOUT GUTTER
NOT TO SCALE

LEGEND:

← DIRECTION OF TRAFFIC

SHEET 1 OF 2

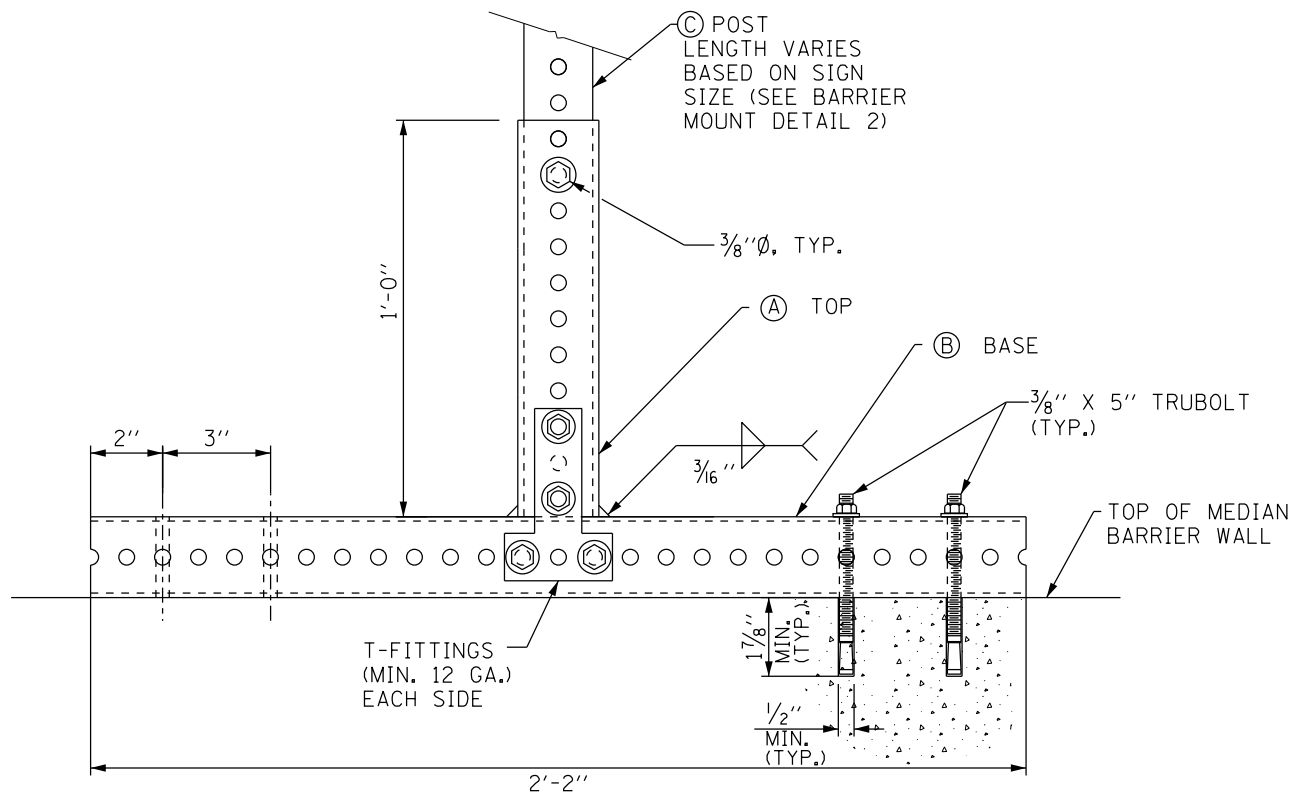
APPROVED: *Paul Kovacs* DATE 4-6-2009
CHIEF ENGINEER

| DATE | REVISIONS |
|------------|--------------------------------------------------|
| 5-8-2009 | POSITIONING DETAILS |
| 8-1-2009 | REVISED BARRIER WALL MOUNT |
| 3-1-2013 | REMOVED MILE POST SIGNS |
| 3-31-2016 | REVISED BOLT NOTE |
| 3-01-2019 | REMOVED "LIGHT POLE/SIGN STRUCTURE MOUNT DETAIL" |
| 12-31-2019 | REVISED BARRIER MOUNT DETAIL AND GENERAL NOTES |

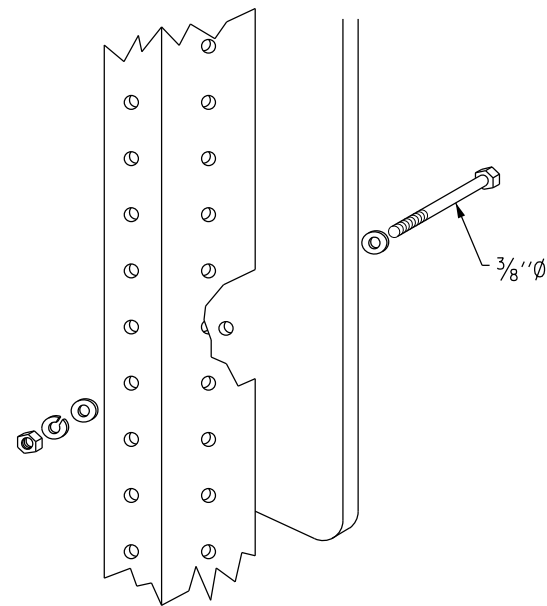


MILEPOST MARKER

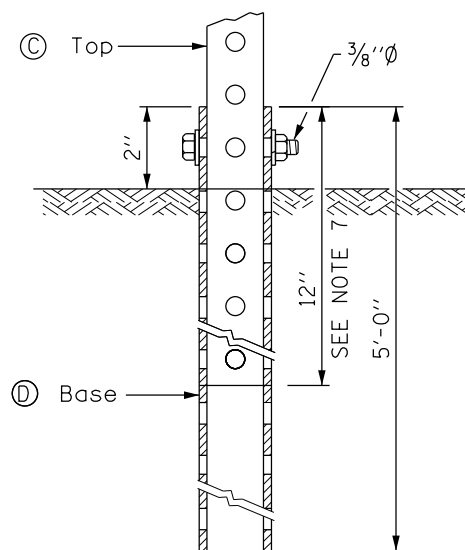
STANDARD F11-06



BARRIER MOUNT DETAIL
NOT TO SCALE

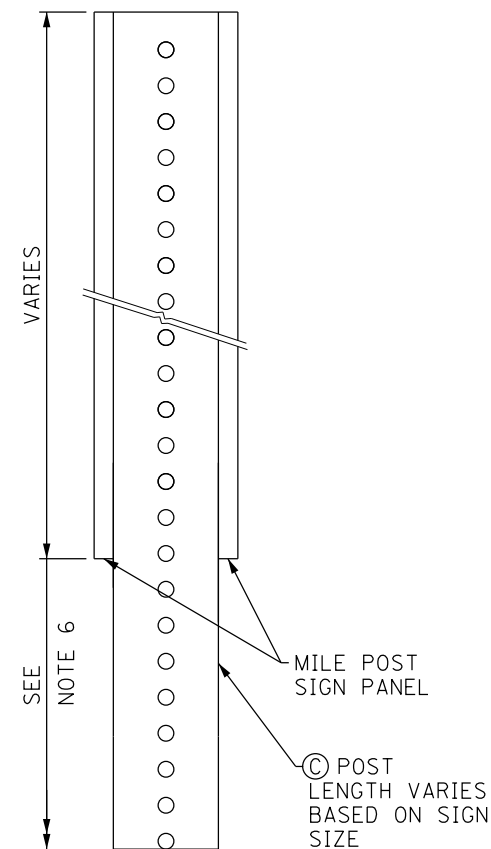


TELESCOPING STEEL POSTS
NOT TO SCALE

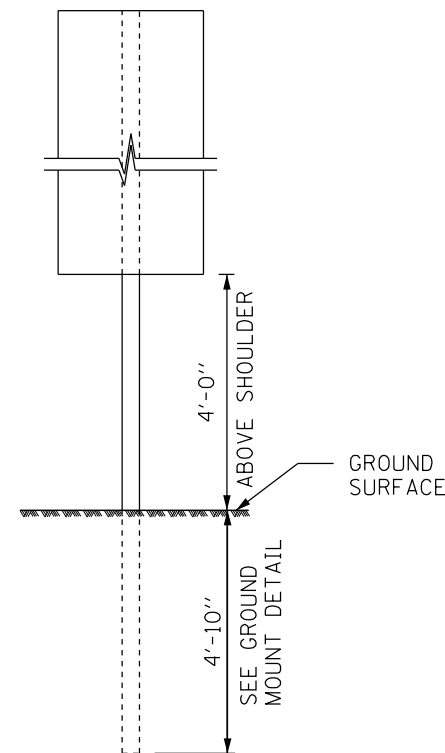


GROUND MOUNT DETAIL
NOT TO SCALE

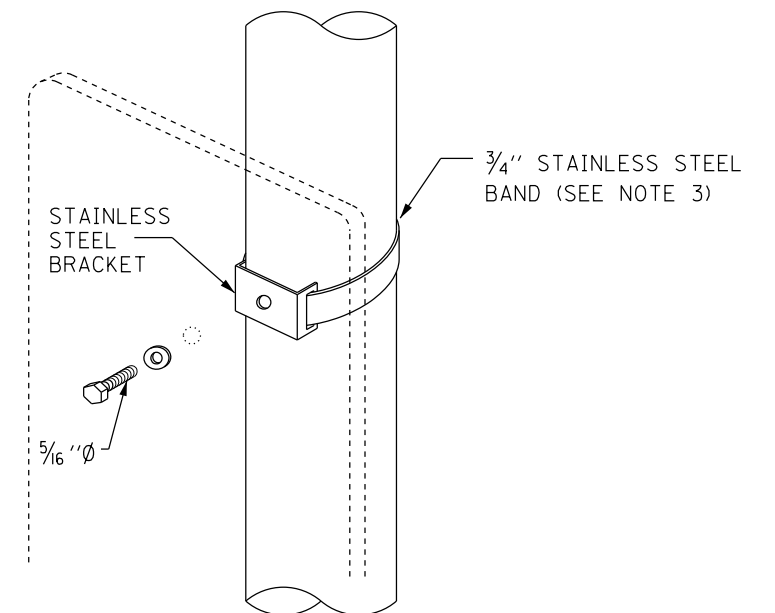
| | |
|---|----------------------------------|
| Ⓐ | 2 1/4" x 2 1/4" x 1'-0" (12 GA.) |
| Ⓑ | 2 1/4" x 2 1/4" x 2'-2" (12 GA.) |
| Ⓒ | 2" x 2" x VARIES (12 GA.) |
| Ⓓ | 2 1/4" x 2 1/4" x 5'-0" (12 GA.) |



BARRIER MOUNT DETAIL 2
NOT TO SCALE



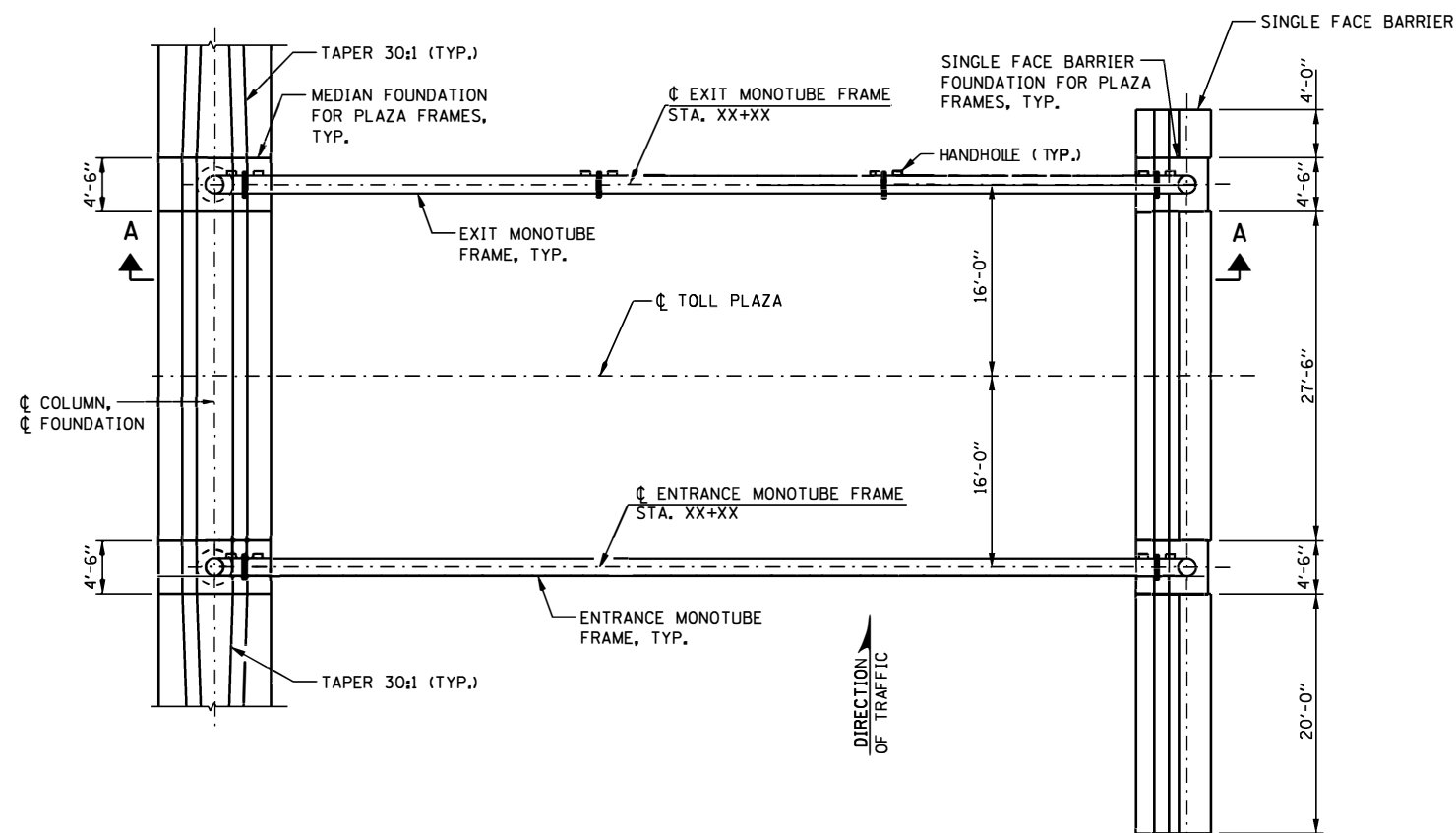
ONE POST INSTALLATION
NOT TO SCALE



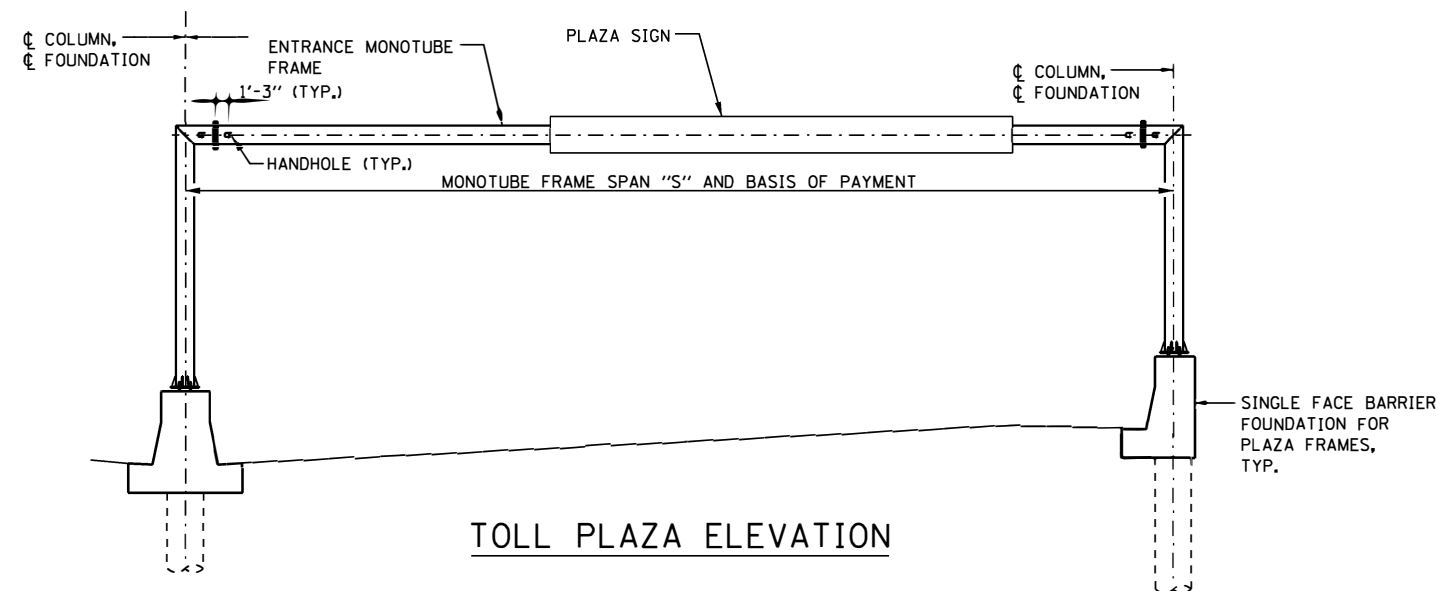
LIGHT POLE/SIGN STRUCTURE MOUNT DETAIL
NOT TO SCALE

GENERAL NOTES:

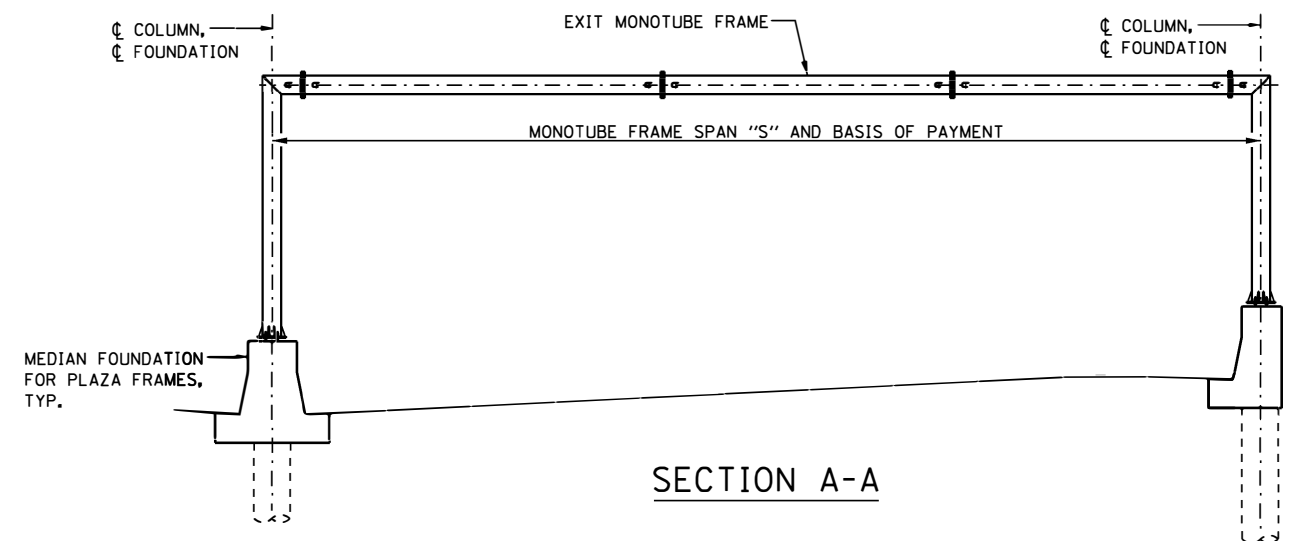
- ALL ANCHOR BOLTS FOR MEDIAN BARRIER MOUNT DETAIL SHALL BE 3/8" DIA. RED HEAD "TRUBOLT" OR APPROVED EQUAL.
- ALL DIMENSIONS ARE IN INCHES UNLESS SHOWN OTHERWISE.
- FOLLOWING ARE THE STEPS FOR FASTENING THE MILEPOST MARKER SIGN PANEL. ALL MOUNTING DETAILS SHOWN ON THIS SHEET APPLY:
 - CENTER ALL FASTENERS ON THE SIGN PANEL.
 - START AND FINISH THE FASTENER SPACING USING A MINIMUM OF 3" TO A MAXIMUM OF 6" FROM THE TOP AND BOTTOM EDGE OF THE SIGN PANEL.
 - THE DISTANCE BETWEEN SUCCESSIVE FASTENERS SHALL NOT EXCEED 2'-0".
- CENTER THE 5/16" DIA. BOLT IN THE MIDDLE OF THE SIGN.
- USE THE SAME ATTACHMENT FOR BACK TO BACK MILEPOST MARKER SIGN.
- DISTANCE FROM THE EDGE OF SHOULDER TO THE BOTTOM OF THE MILEPOST MARKER SIGN SHALL BE A MINIMUM OF 4'-0" REGARDLESS OF BARRIER TYPE.
- THE TOP SECTION SHALL BE TELESKOPEDED INTO THE BASE SECTION 12 INCHES AND FASTENED TOGETHER.
- FOR ATTACHMENT TO BRIDGE PARAPET USE BARRIER WALL MOUNT DETAIL. ONLY ONE PANEL REQUIRED WHEN ATTACHED TO PARAPET ALONG OUTSIDE SHOULDER.
- BASE AND POST ASSEMBLY SHALL BE HOT DIP GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH AASTHO M111 OR AS SPECIFIED IN THE SPECIAL PROVISION.



TOLL PLAZA PLAN



TOLL PLAZA ELEVATION



SECTION A-A

NOTES:

1. SEE PLANS FOR SIGN SIZE AND LOCATION.
2. MAXIMUM PLAZA SIGN AREA IS 108 SQ. FT.
MAXIMUM PLAZA SIGN LENGTH IS 36 FT.

SHEET 1 OF 8

| DATE | REVISIONS |
|-----------|---------------------------------------------------------------------------|
| 7-01-2014 | ADDED GROUNDING DETAILS. |
| 3-11-2015 | ADDED MEDIAN AND NOTES. |
| 3-31-2016 | REVISED FOUNDATION NOTE. |
| 3-01-2018 | REVISED SIGN STRUCTURE |
| 3-01-2019 | UPDATED CONSTANT SLOPE BARRIER, REINFORCING |
| | DETAILS AND QUANTITIES |
| 5-24-2019 | UPDATED SHOULDER BARRIER DETAILS AND QUANTITIES |
| | FOR 3'-8" |
| 2-13-2020 | ADDED HANDHOLES, INSTALLATION & INSPECTION NOTES OF ANCHOR BOLT & SPLICES |



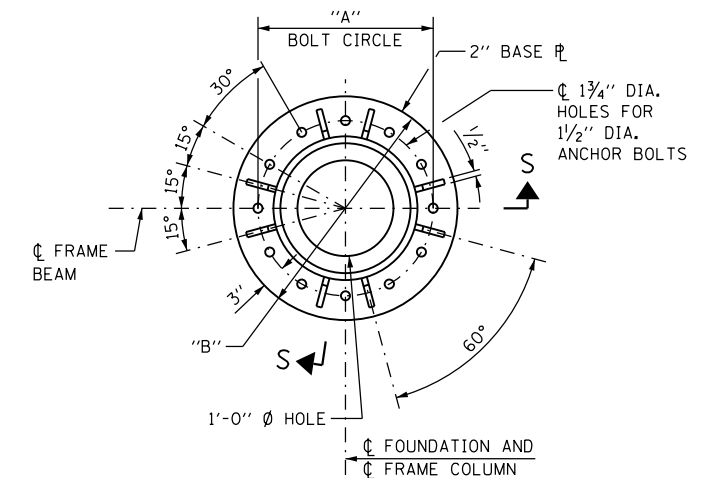
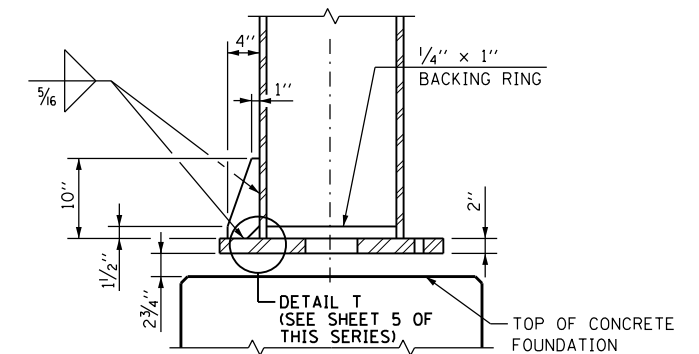
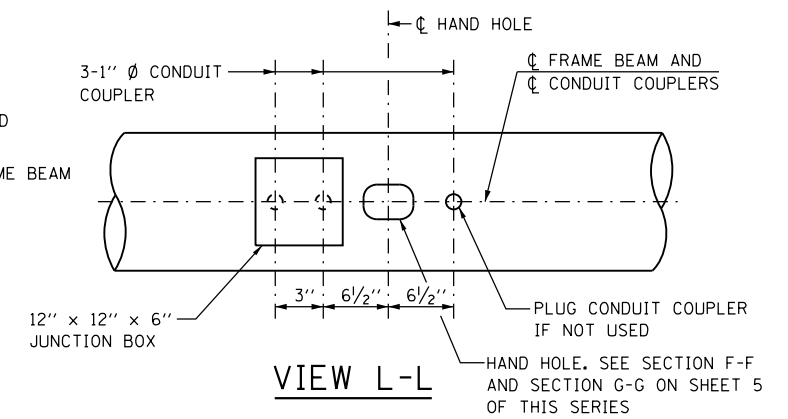
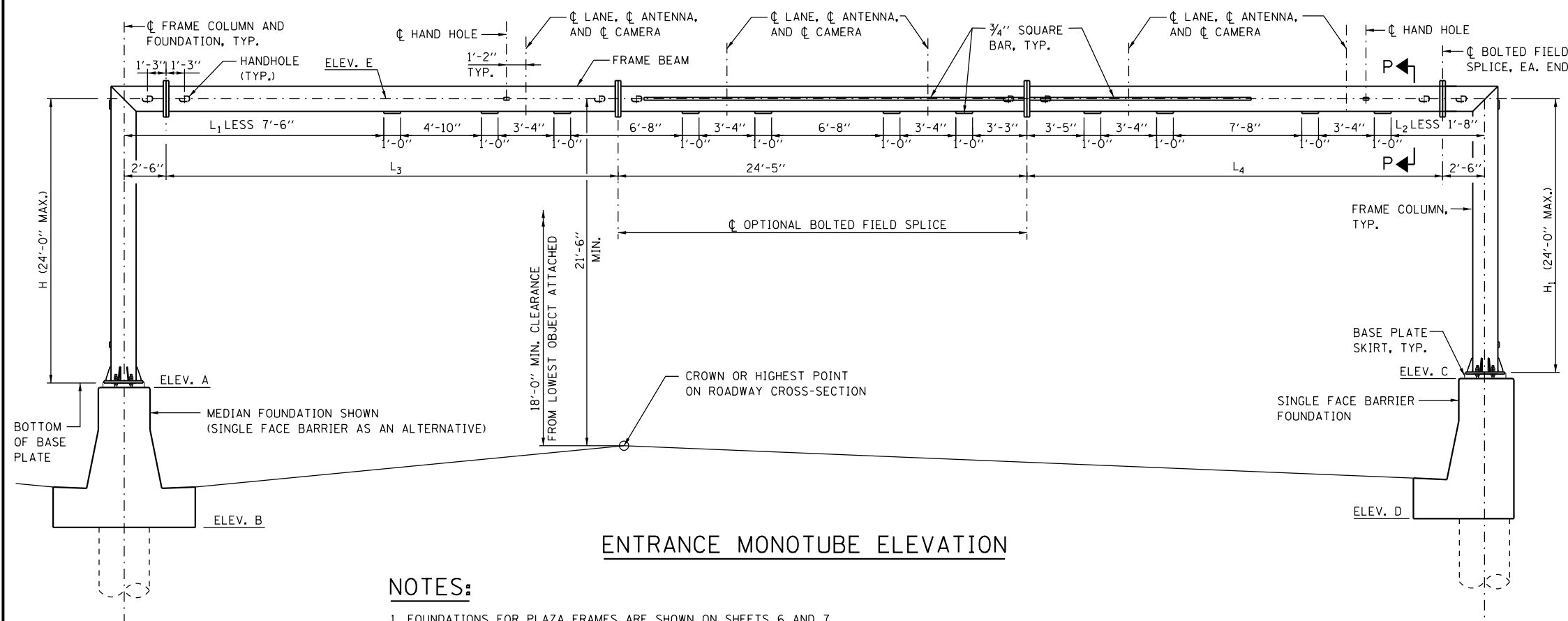
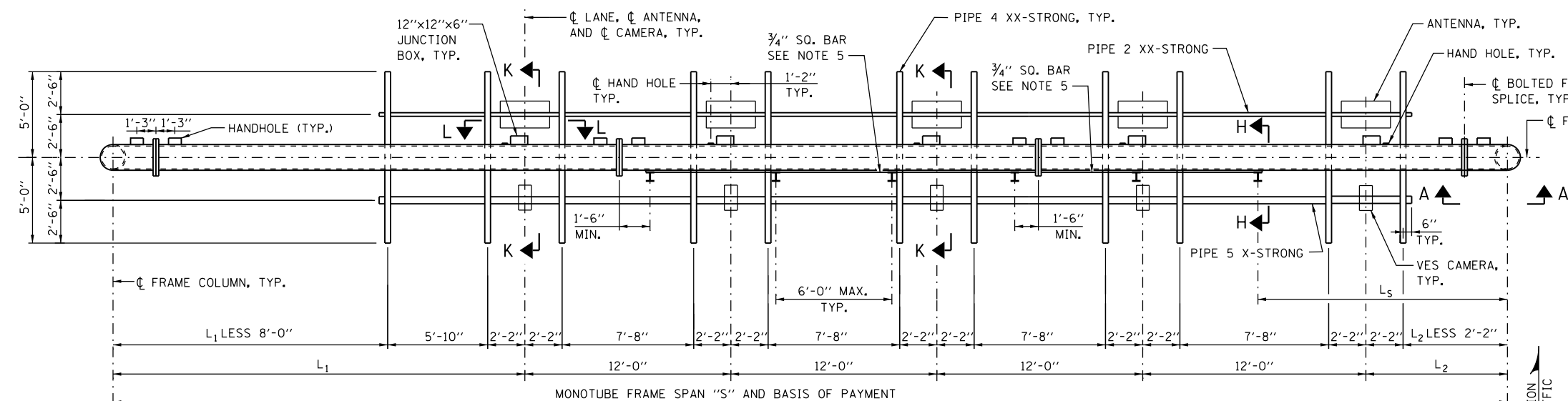
OVERHEAD SIGN STRUCTURE
MONOTUBE TYPE (STEEL)
MAINLINE STRUCTURE DETAILS

STANDARD F13-06

APPROVED.....
CHIEF ENGINEERING OFFICER

Paul Kovacs

DATE 3-31-2014



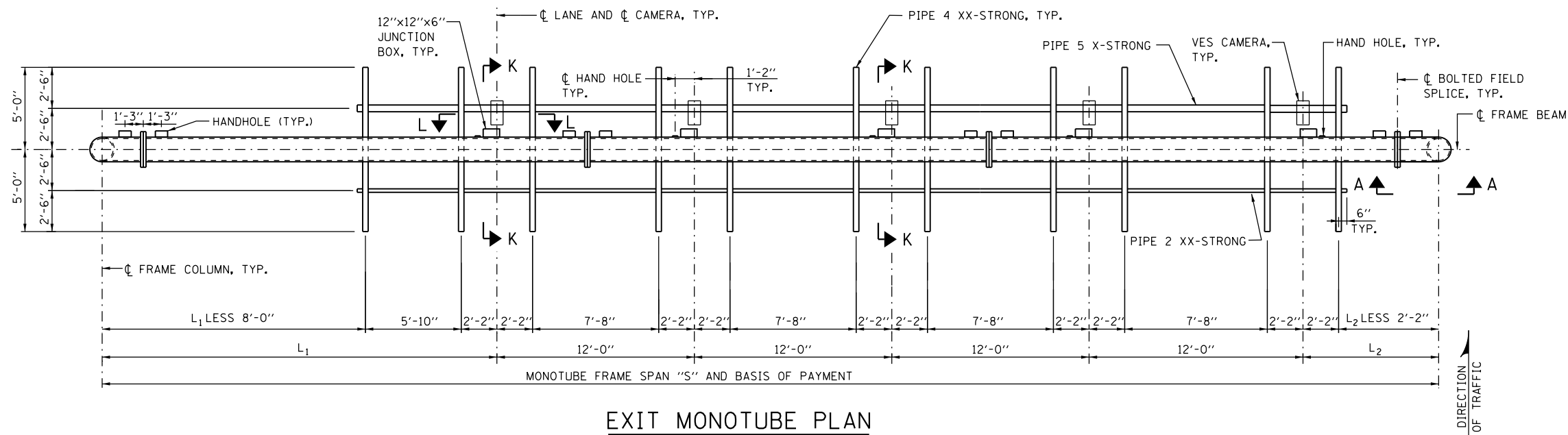
NOTES:

1. FOUNDATIONS FOR PLAZA FRAMES ARE SHOWN ON SHEETS 6 AND 7 OF THIS SERIES.
2. FOR SECTIONS A-A, H-H, K-K, BASE PLATE SKIRT AND HAND HOLE DETAILS, SEE SHEET 5 OF THIS SERIES.
3. FOR SECTION P-P SEE SHEET 4 OF THIS SERIES.
4. PROVIDE CAMBER AT MIDSPAN OF STRUCTURE.
5. DISCONTINUE $\frac{3}{4}$ " SQUARE BAR TO ALLOW $\frac{1}{2}$ " \emptyset U-BOLT INSTALLATION.
6. WORK THIS SHEET WITH, OVERHEAD SIGN STRUCTURES ENTRANCE MONOTUBE TYPE (STEEL) MAINLINE SUMMARY AND TOTAL BILL OF MATERIAL SHEET.

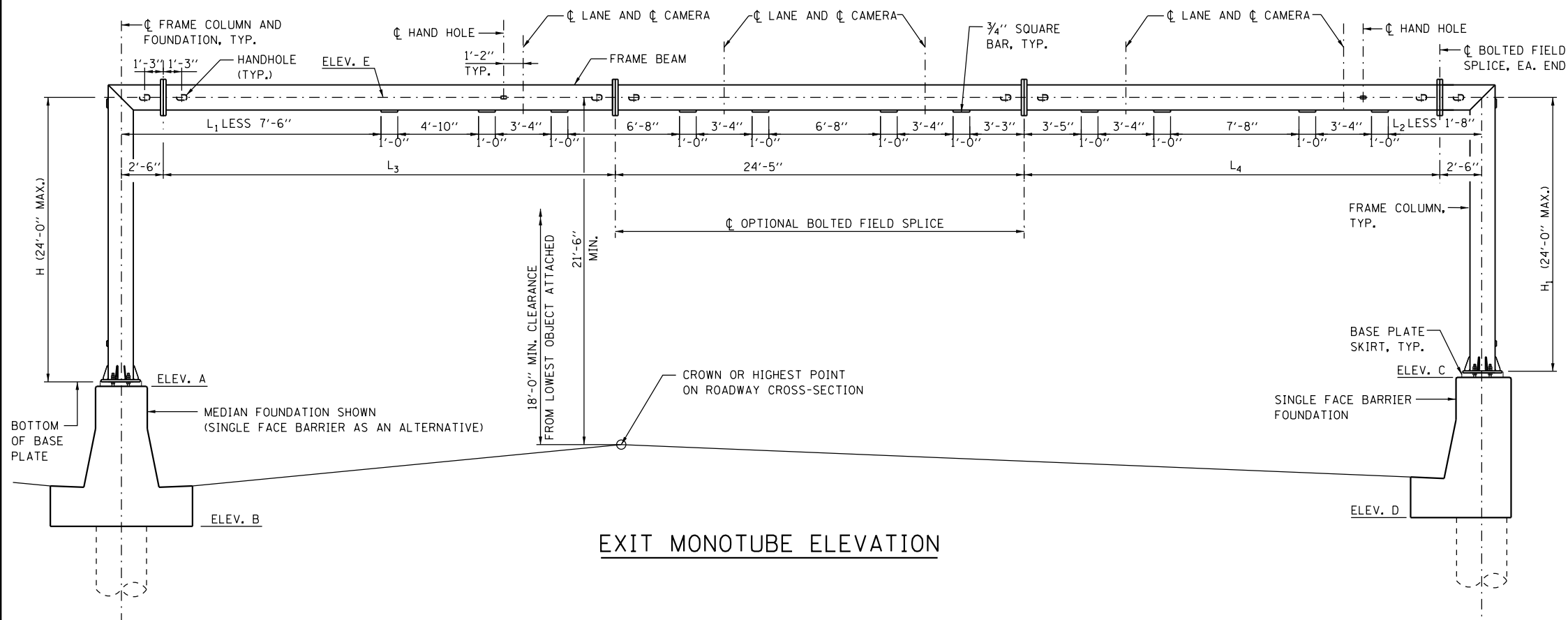
MONOTUBE FRAME TABLE

| TYPE | SPAN "S" | FRAME COLUMN | FRAME BEAM | CAMBER | "A" | "B" |
|------|-------------|-----------------|--------------|--------|--------|-------|
| I | ≤ 70' | HSS 16x0.500 | HSS 16x0.500 | 2 3/4" | 1'-8" | 2'-2" |
| II | 71'-80' | HSS 18x0.500 | HSS 18x0.500 | 4" | 1'-10" | 2'-4" |
| III | 81'-90' | HSS 18x0.500 | HSS 18x0.500 | 4 1/2" | 1'-10" | 2'-4" |

APPROVED.....DATE 3-31-2014
CHIEF ENGINEERING OFFICER



EXIT MONOTUBE PLAN



EXIT MONOTUBE ELEVATION

NOTES:

1. SEE SHEET 2 OF THIS SERIES FOR MONOTUBE FRAME TABLE, VIEW L-L, BASE PLATE DETAIL, AND ADDITIONAL NOTES.
2. WORK THIS SHEET WITH, OVERHEAD SIGN STRUCTURES EXIT MONOTUBE TYPE (STEEL) SUMMARY AND TOTAL BILL OF MATERIAL SHEET.

GENERAL NOTES:

1. SEE THE ILLINOIS TOLLWAY STRUCTURE DESIGN MANUAL FOR MINIMUM VERTICAL CLEARANCE.
2. AFTER ADJUSTMENTS TO LEVEL FRAME BEAM AND ENSURE ADEQUATE VERTICAL CLEARANCE, TIGHTEN ALL TOP AND LEVELING NUTS AGAINST THE BASE PLATE WITH A MINIMUM TORQUE OF 200 LB.-FT. THEN PLACE STAINLESS STEEL MESH AROUND THE PERIMETER OF THE BASE PLATE. SECURE TO BASE PLATE WITH STAINLESS STEEL BANDING.
3. REINFORCEMENT BARS DESIGNATED "E)" SHALL BE EPOXY COATED.

STRUCTURAL STEEL:

1. MATERIAL FOR THE MONOTUBE FRAME SHALL CONFORM TO THE REQUIREMENT OF ASTM A500 GRADE B. OTHER STRUCTURAL STEEL SHAPES AND PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A36, UNLESS NOTED OTHERWISE.
2. PIPES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A53 GRADE B.
3. ANCHOR BOLTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM F1554 (AASHTO M314) GRADE 55, WITH A MINIMUM TENSILE STRENGTH OF 75,000 PSI. INSTALLATION AND INSPECTION OF ANCHOR BOLTS SHALL COMPLY WITH ILLINOIS TOLLWAY SPECIAL PROVISION "INTELLENT TRANSPORTATION SYSTEMS GANTRY FRAME "STEEL". ANCHORS SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM A153 (AASHTO M232). SEE SHEET 6 OF THIS SERIES FOR GALVANIZED LENGTH.
4. U-BOLTS SHALL BE STAINLESS STEEL AND SHALL CONFORM TO ASTM 193, CLASS I, GRADE B8 (AISI TYPE 304). WASHERS FOR U-BOLTS SHALL CONFORM TO ASTM A240, TYPE 302. NUTS FOR U-BOLTS SHALL CONFORM TO ASTM A194 (AASHTO M292), GRADE 8F (AISI TYPE 303).
5. BOLTS (EXCLUDING ANCHOR BOLTS AND U-BOLTS) SHALL BE HIGH STRENGTH AND SHALL CONFORM TO THE REQUIREMENTS OF ASTM A325 (AASHTO M164). THEY SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM A153 (AASHTO M232).
6. NUTS SHALL CONFORM TO ASTM A563 GRADE DH AND GALVANIZED ACCORDING TO ASTM A153 (AASHTO M232).
7. HARDENED STEEL WASHERS SHALL CONFORM TO ASTM F436 AND GALVANIZED ACCORDING TO ASTM A153 (AASHTO M232).
8. TUBES FOR MONOTUBE FRAME, PIPES, STRUCTURAL STEEL SHAPES AND PLATES SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM A123 AFTER FABRICATION.
8. THE MONOTUBE FRAME BEAM, COLUMNS, BASE PLATE MATERIAL, AND SPLICES ARE CONSIDERED TENSION MEMBERS AND SHALL CONFORM TO THE IMPACT TESTING REQUIREMENT, ZONE 2.
10. WELDING SHALL BE PERFORMED BY CERTIFIED WELDERS USING E70-XX ELECTRODES, AND SHALL CONFORM TO AWS D1.1-08 "STRUCTURAL WELDING CODE - STEEL". ALL WELDS ON ARCHITECTURAL EXPOSED STEEL (AES) MEMBERS ARE TO BE GROUND SMOOTH AND FILLED.

DESIGN LOADING:

WIND LOAD CRITERIA
BASIC WIND SPEED = 90 MPH
G = 1.14
I_r = 1.00 (50 YR. RECURRENCE INTERVAL)

EQUIPMENT LOADS:

CAMERA ASSEMBLY 8 LB.
ANTENNA 20 LB.

DESIGN STRESSES FOR REINFORCED CONCRETE:

f'c = COMPRESSIVE STRENGTH OF CONCRETE AT 14 DAYS (CLASS SI) = 3,500 P.S.I.
f'c = COMPRESSIVE STRENGTH OF CONCRETE AT 14 DAYS (CLASS DS) = 4,000 P.S.I.
f_y = YIELD STRENGTH OF REINFORCEMENT BARS (GRADE 60) = 60,000 P.S.I.

FOUNDATION:

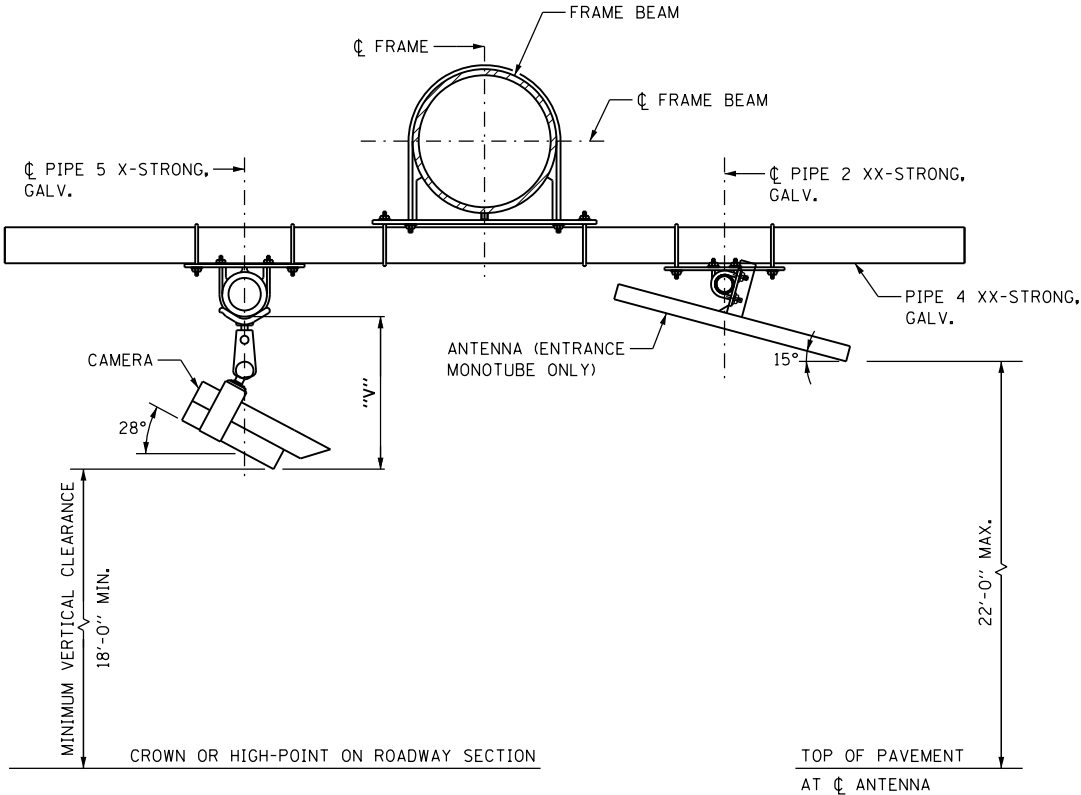
MINIMUM UNCONFINED COMPRESSIVE STRENGTH, Q_u FOR ALL LAYERS OF COHESIVE SOILS (CLAYS) SHALL BE 1.25 TON/SQ.FT. AT PLAZA FRAMES.

DESIGN SPECIFICATIONS:

1. ILLINOIS TOLLWAY STRUCTURE DESIGN MANUAL, LATEST EDITION.
2. AASHTO STANDARD SPECIFICATION FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 6TH EDITION.
3. AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 6TH EDITION DATED FEBRUARY 2012.
4. ILLINOIS DEPARTMENT OF TRANSPORTATION BRIDGE MANUAL, JANUARY 2012

CONSTRUCTION SPECIFICATIONS:

1. ILLINOIS TOLLWAY SUPPLEMENTAL SPECIFICATIONS TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.
2. ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.



NOTE:
VERIFY DIMENSION "V" WITH
CAMERA MANUFACTURER.

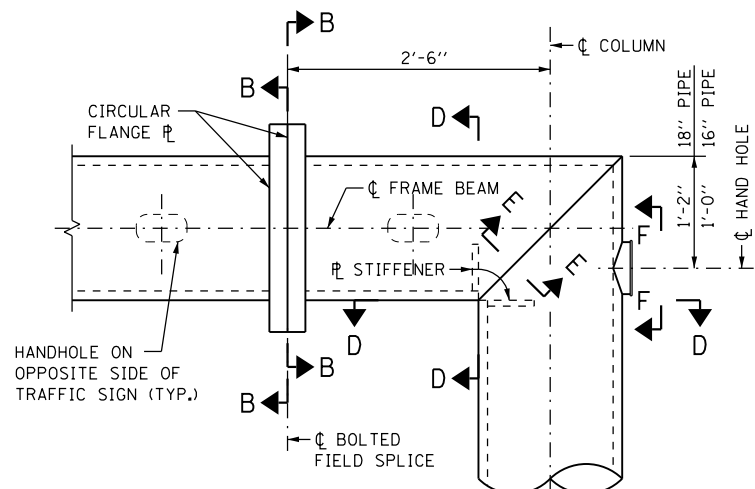
APPROVED..... DATE 3-31-2014..
CHIEF ENGINEERING OFFICER

SECTION P-P

SHEET 4 OF 8

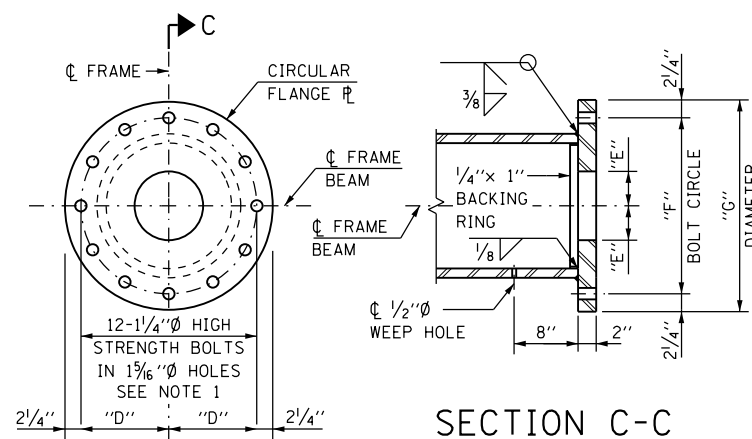
OVERHEAD SIGN STRUCTURE
MONOTUBE TYPE (STEEL)
MAINLINE STRUCTURE DETAILS

STANDARD F13-06



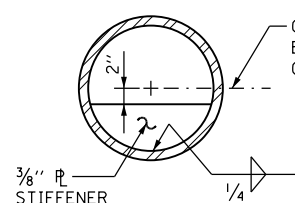
SECTION A-A

(SEE SHEET 1 OF THIS SERIES FOR LOCATION)

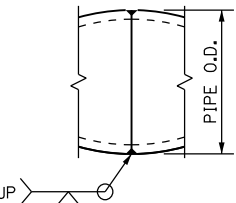


NOTE 1: INSTALLATION AND INSPECTION OF SPLICE BOLTS SHALL COMPLY WITH ILLINOIS TOLLWAY SPECIAL PROVISION "INTELLIGENT TRANSPORTATION SYSTEMS GANTRY FRAME (STEEL)".

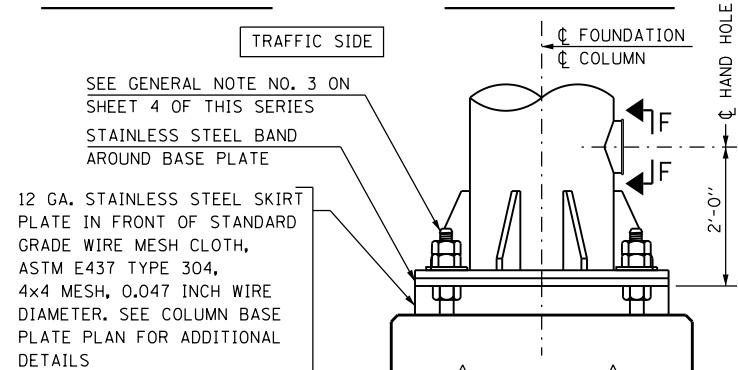
SECTION B-B



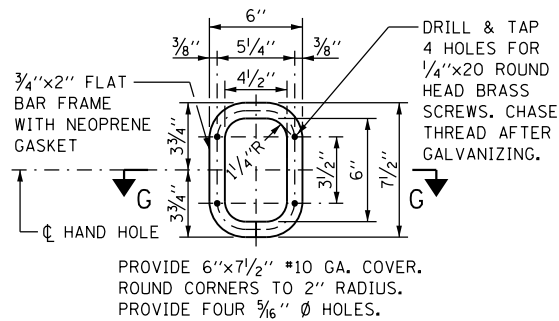
SECTION D-D



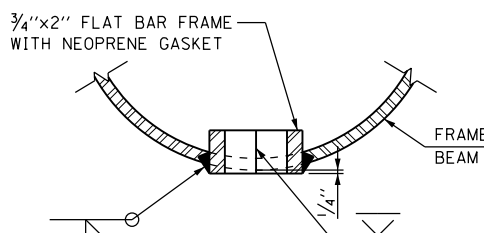
SECTION E-E



COLUMN BASE

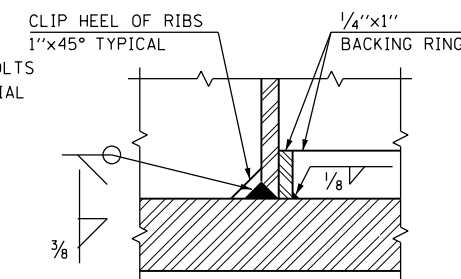


SECTION F-F

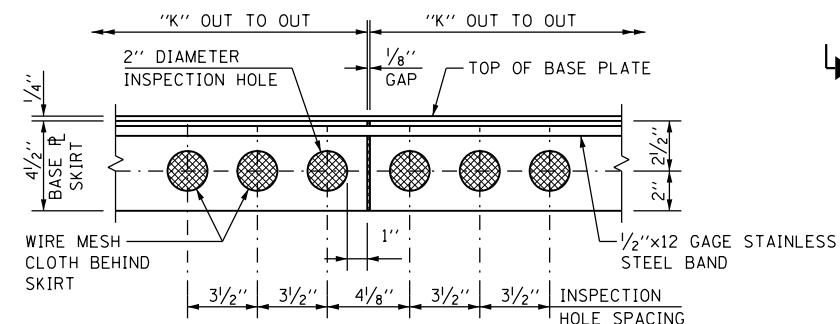


SECTION G-G

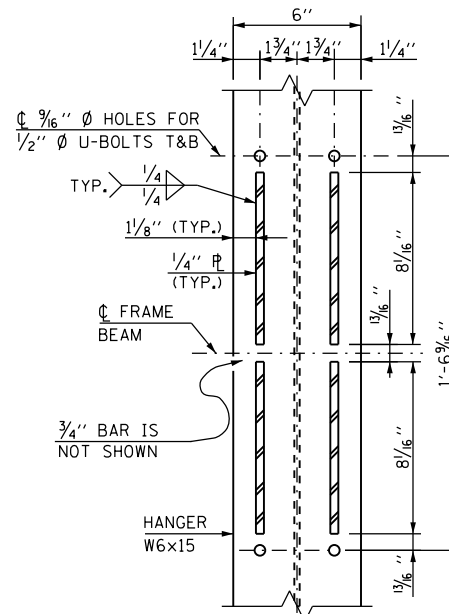
| FRAME BEAM | "D" | "E" | "F" | "G" |
|--------------|-----|-----|--------|-----------|
| HSS 16x0.500 | 10" | 6" | 1'-8" | 2'-0 1/2" |
| HSS 18x0.500 | 11" | 6" | 1'-10" | 2'-2 1/2" |



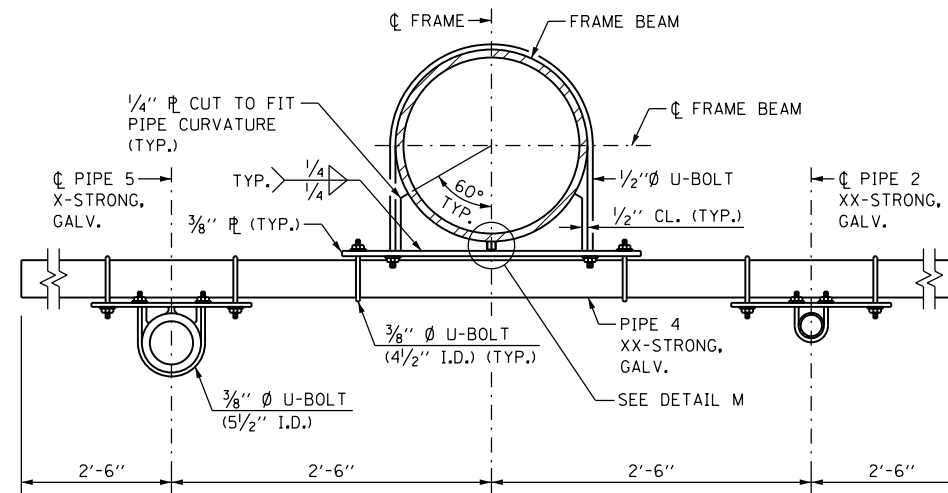
DETAIL T



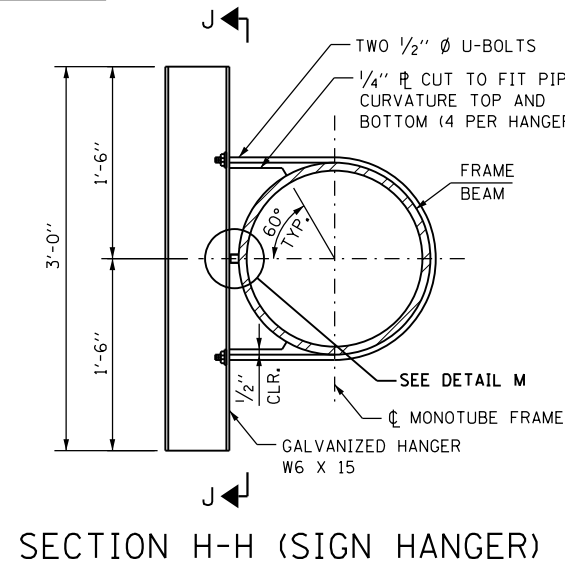
VIEW R-R (BASE PLATE SKIRT)



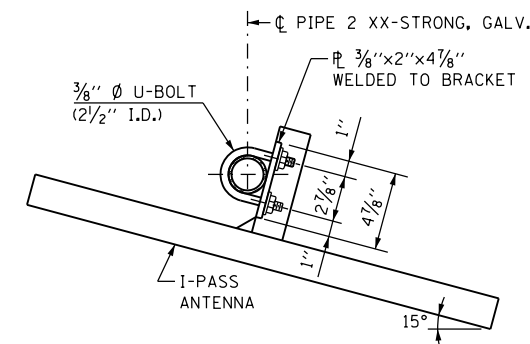
SECTION J-J



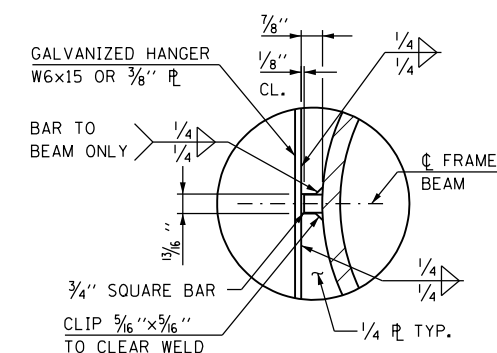
SECTION K-K



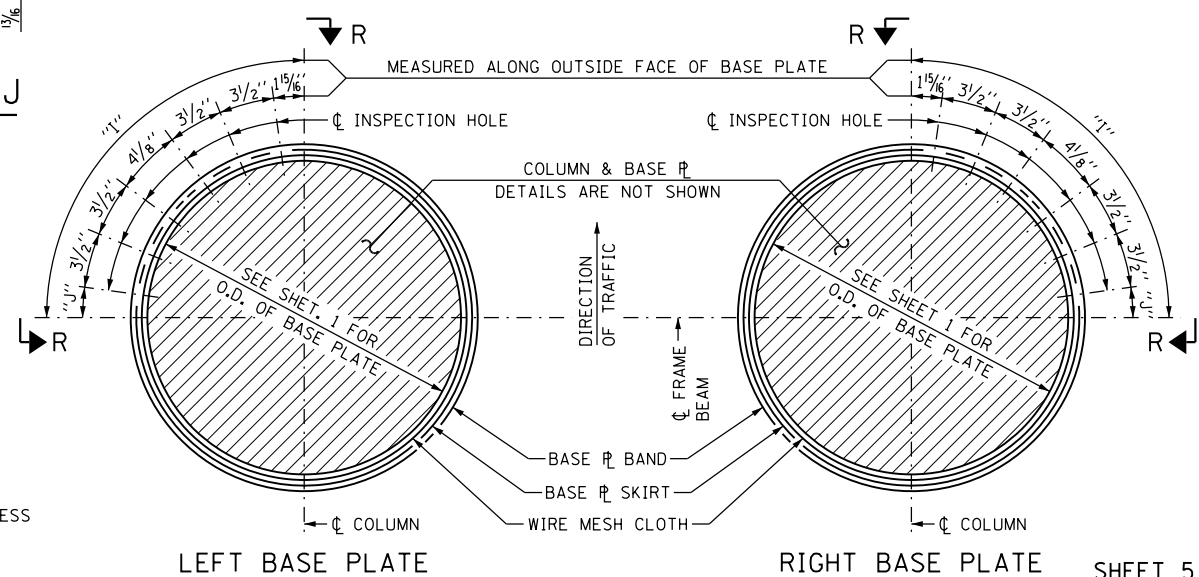
SECTION H-H (SIGN HANGER)



ANTENNA HANGER



DETAIL M



LEFT BASE PLATE

RIGHT BASE PLATE

SHEET 5 OF 8

COLUMN BASE PLATE PLAN

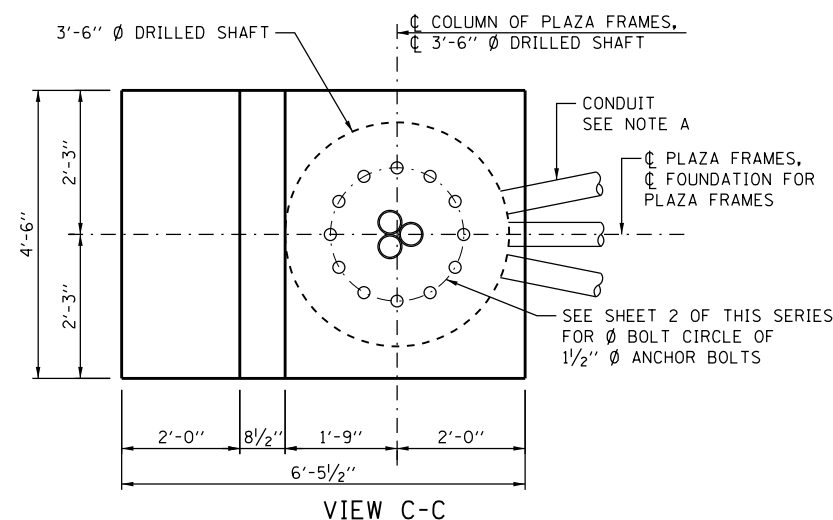
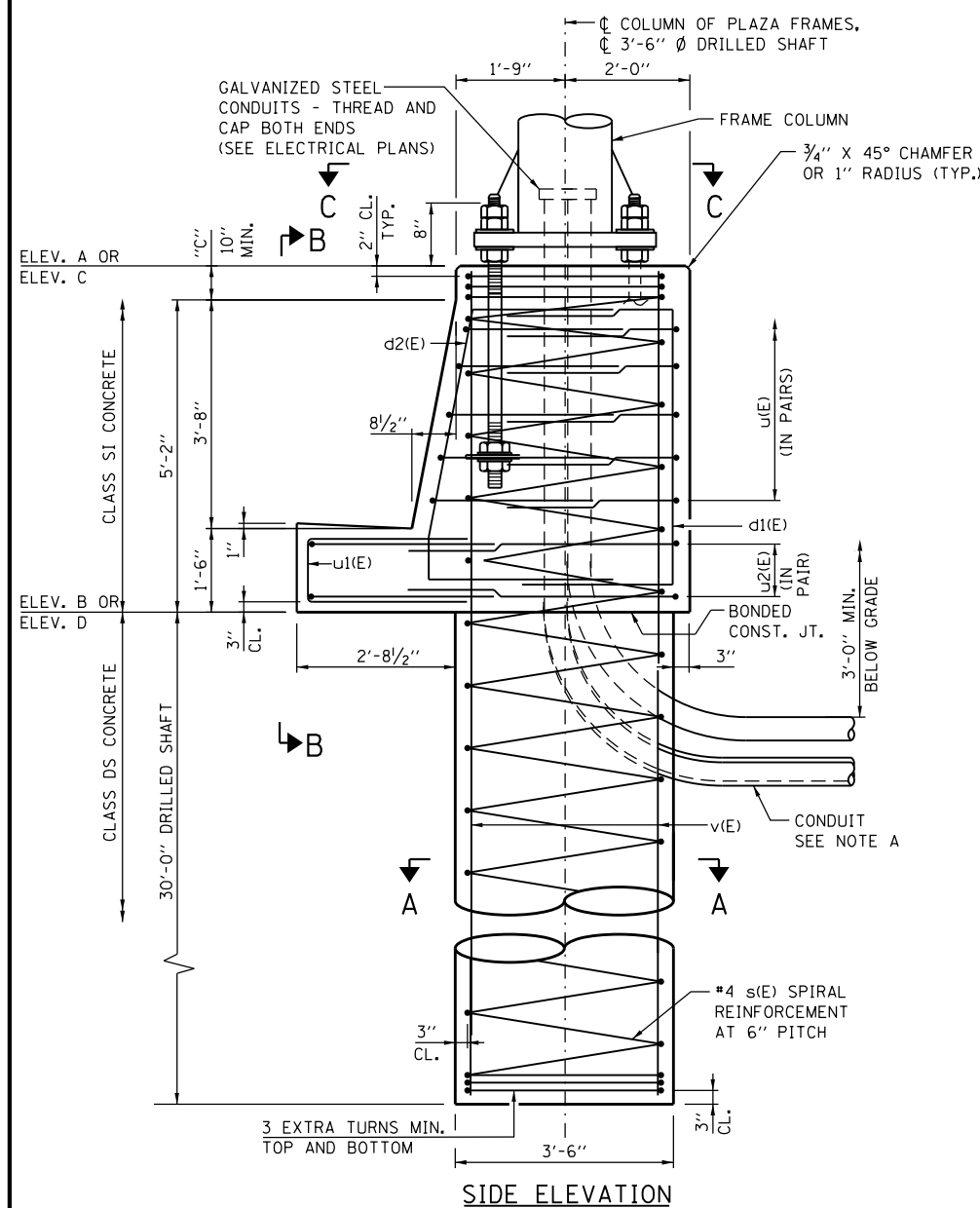
| FRAME COLUMN | "I" | "J" | "K" |
|--------------|------------|---------|------------|
| HSS 16x0.500 | 1'-8 7/16" | 3/8" | 6'-9 1/16" |
| HSS 18x0.500 | 1'-10" | 1 1/16" | 7'-3 7/8" |



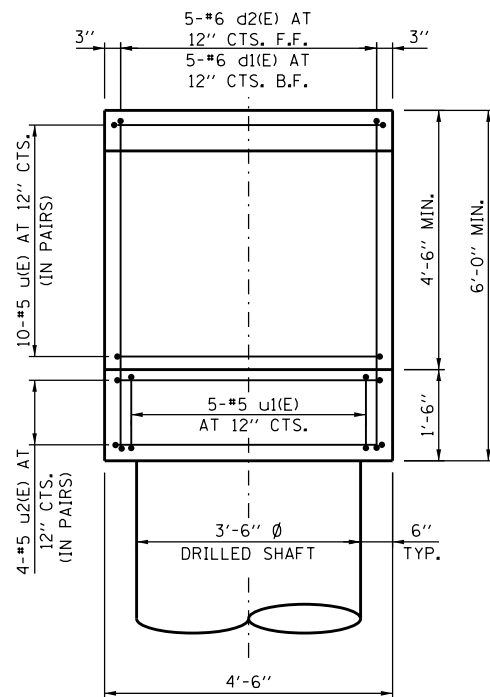
OVERHEAD SIGN STRUCTURE
MONOTUBE TYPE (STEEL)
MAINLINE STRUCTURE DETAILS

STANDARD F13-06

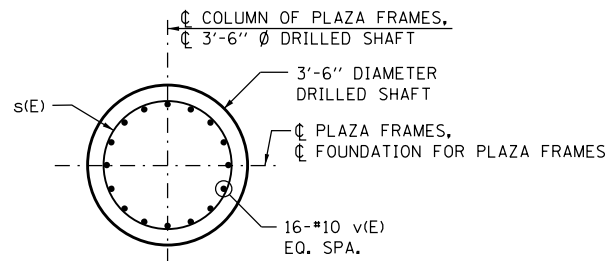
Paul Kovacs
APPROVED, CHIEF ENGINEERING OFFICER
DATE 3-31-2014



**SINGLE FACE BARRIER FOUNDATION
FOR PLAZA FRAMES**



VIEW B-B



SECTION A-A

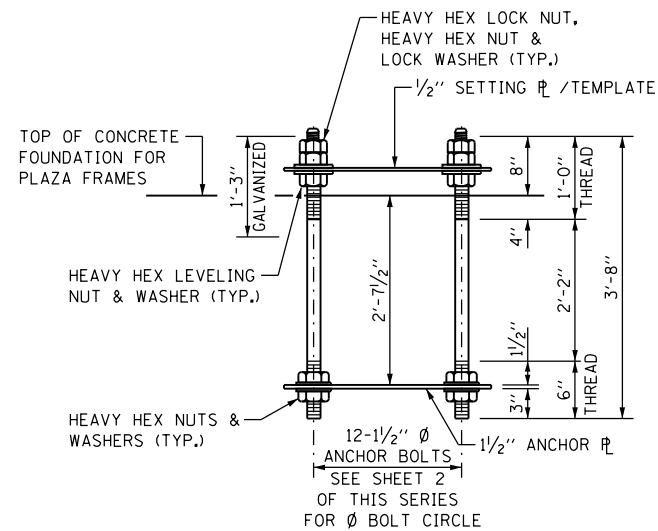
NOTE A:
COORDINATE CONDUIT SIZE, LOCATION AND QUANTITY WITH ELECTRICAL PLANS. CONDUITS SHALL BE PLACED TO MISS REINFORCEMENT. CUTTING OF REINFORCEMENT SHALL NOT BE ALLOWED.

FOUNDATIONS:

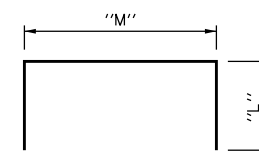
THE FOUNDATION DETAILS SHOWN ARE BASED ON THE PRESENCE OF MOSTLY COMMON COHESIVE SOIL CONDITIONS (SILTY OR SANDY CLAY), WITH AN AVERAGE UNCONFINED COMPRESSIVE STRENGTH (QU) > 1.25 TON/SQ. FT. WHICH MUST BE DETERMINED BY PREVIOUS SOIL INVESTIGATIONS AT THE JOBSITE. WHEN OTHER CONDITIONS ARE INDICATED, THE BORING DATA SHALL BE INCLUDED IN THE PLANS AND THE FOUNDATION DIMENSIONS SHOWN SHALL BE THE RESULT OF SITE SPECIFIC DESIGNS. IF CONDITIONS ENCOUNTERED IN THE FIELD ARE DIFFERENT THAN THOSE INDICATED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER TO DETERMINE IF THE FOUNDATION DIMENSIONS NEED TO BE MODIFIED.

LEGEND:

F.F. - FRONT FACE
B.F. - BACK FACE
CTS. - CENTERS

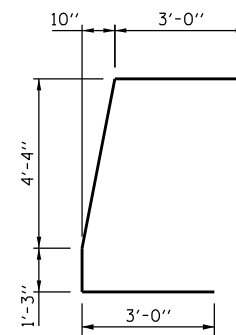


ANCHOR BOLT ASSEMBLY



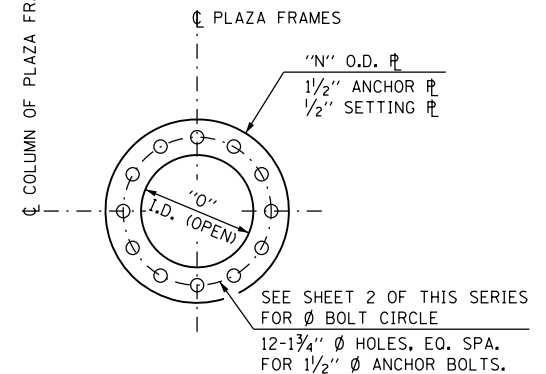
| BAR | "L" | "M" |
|-------|--------|-------|
| d1(E) | 3'-0" | 5'-7" |
| u(E) | 3'-0" | 4'-2" |
| u1(E) | 3'-6" | 1'-1" |
| u2(E) | 4'-1" | 4'-2" |
| u3(E) | 5'-10" | 4'-2" |

**BARS d1(E), u(E), u1(E),
u2(E) AND u3(E)**



BAR d2(E)

| FRAME COLUMN | "N" | "O" |
|--------------|-------|-------|
| HSS 16x0.500 | 2'-0" | 1'-4" |
| HSS 18x0.500 | 2'-2" | 1'-6" |



ANCHOR B / SETTING B

BAR LIST-ONE FOUNDATION

| BAR | NO. | | SIZE | LENGTH | SHAPE |
|-------|--------------------------|---------------------|------|---------|-------|
| | SINGLE FACE BARRIER FDN. | MEDIAN BARRIER FDN. | | | |
| d1(E) | 5 | 10 | #6 | 11'-7" | |
| d2(E) | 5 | 10 | #6 | 11'-8" | |
| s(E) | 1 | | #4 | 35'-7" | |
| s1(E) | | 1 | #4 | 35'-7" | |
| v(E) | 16 | | #10 | 35'-7" | |
| v1(E) | | 16 | #10 | 35'-7" | |
| u(E) | 10 | 10 | #5 | 10'-2" | |
| u1(E) | 5 | 10 | #5 | 8'-1" | |
| u2(E) | 4 | | #5 | 12'-4" | |
| u3(E) | | 4 | #5 | 15'-10" | |

- * THE LENGTH OF SPIRAL SHOWN IS THE HEIGHT OF SPIRAL, COMPUTED USING "C" = 10". ADJUST LENGTH ACCORDINGLY IF "C" IS GREATER THAN 10".
- ** BAR LENGTH IS COMPUTED USING "C" = 10". ADJUST BAR LENGTH ACCORDINGLY IF "C" IS GREATER THAN 10".

ESTIMATED QUANTITY

| ITEM | UNIT | SINGLE FACE BARRIER FDN. | MEDIAN BARRIER FDN. |
|---------------------------------|---------|--------------------------|---------------------|
| CLASS SI CONCRETE | CU. YD. | 4.7 | 4.9 |
| CLASS DS CONCRETE | CU. YD. | 10.7 | 10.7 |
| REINFORCEMENT BARS EPOXY COATED | POUND | 3,310 | 3,540 |
| PROTECTIVE COAT | SQ. YD. | 5.2 | 7.4 |

NOTES:

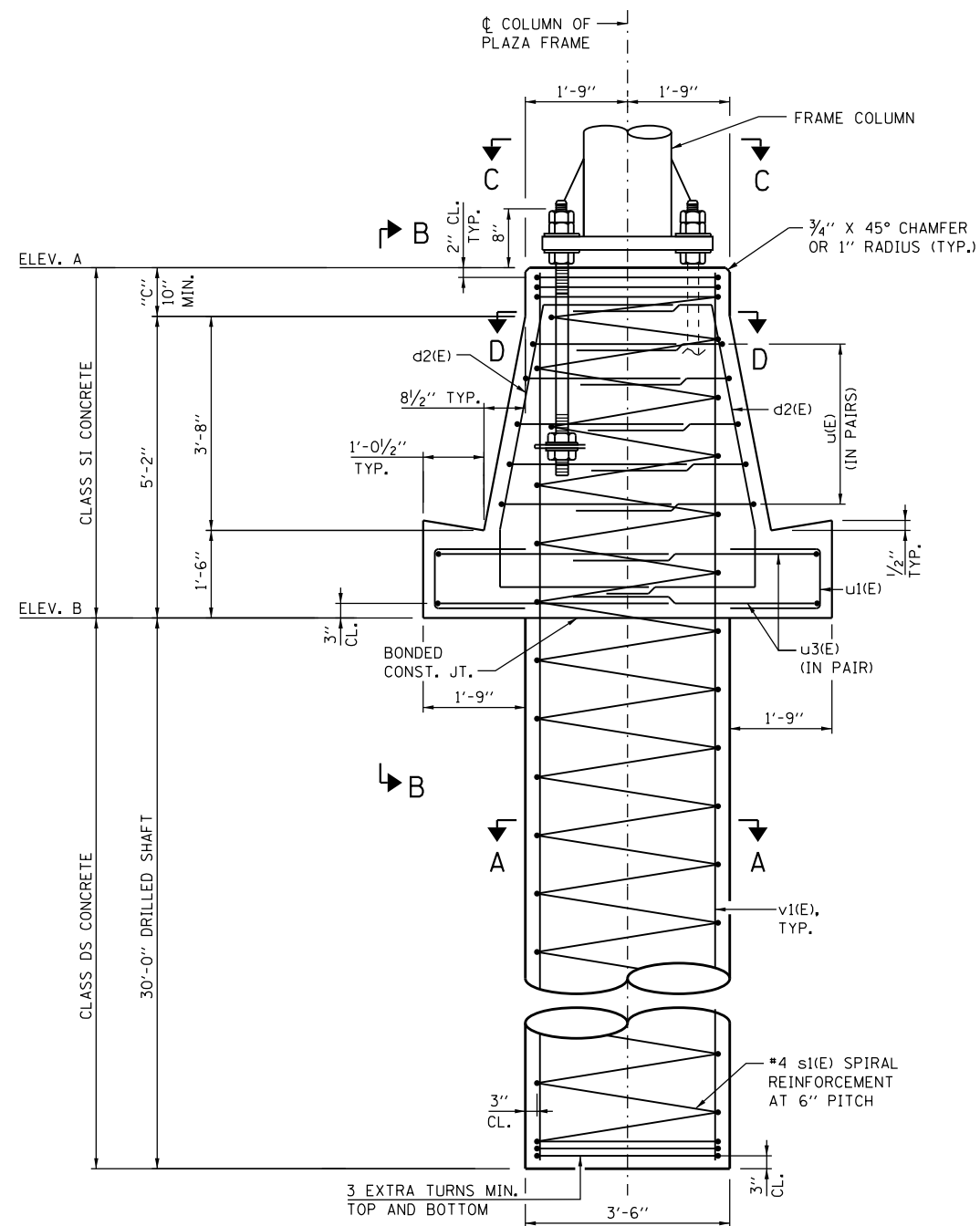
- QUANTITIES FOR SINGLE FACE BARRIER FOUNDATION ARE DETERMINED USING "C" = 10". IF DIMENSION "C" IS GREATER THAN 10", ADJUST QUANTITIES ACCORDINGLY.
- SITE GROUNDING ELECTRODE SYSTEM TO BE PROVIDED AS INDICATED ON THE PLANS.
- PROTECTIVE COAT SHALL BE APPLIED TO THE TRAFFIC AND TOP FACES OF THE BARRIER AND TOP OF GUTTER.



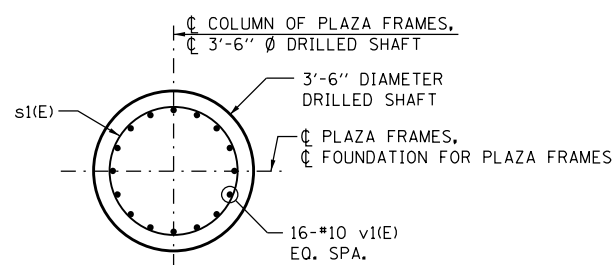
OVERHEAD SIGN STRUCTURE
MONOTUBE TYPE (STEEL)
MAINLINE STRUCTURE DETAILS

STANDARD F13-06

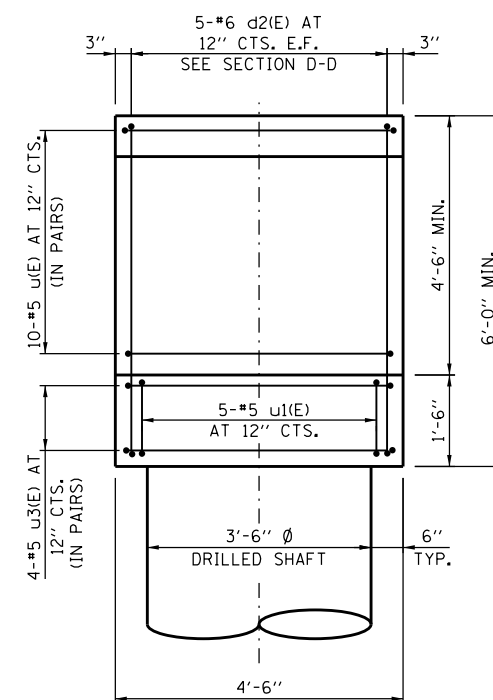
Paul Kovacs
APPROVED..... DATE 3-31-2014..
CHIEF ENGINEERING OFFICER



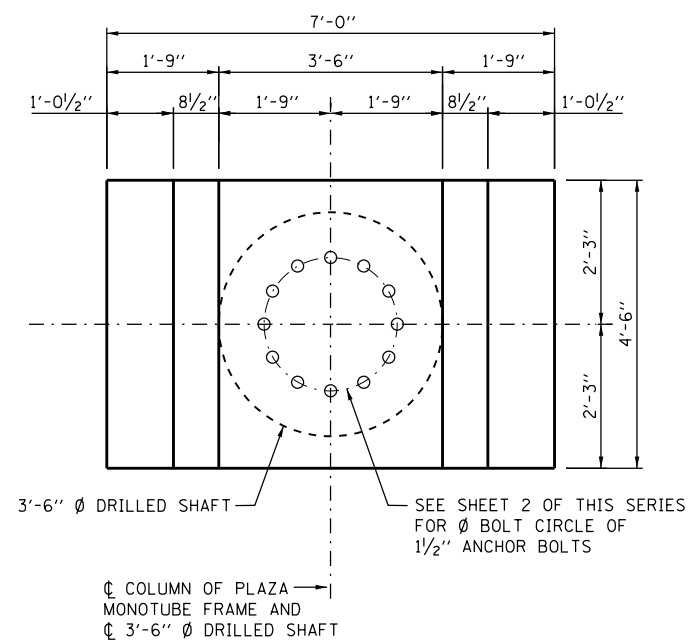
MEDIAN FOUNDATION FOR PLAZA FRAMES



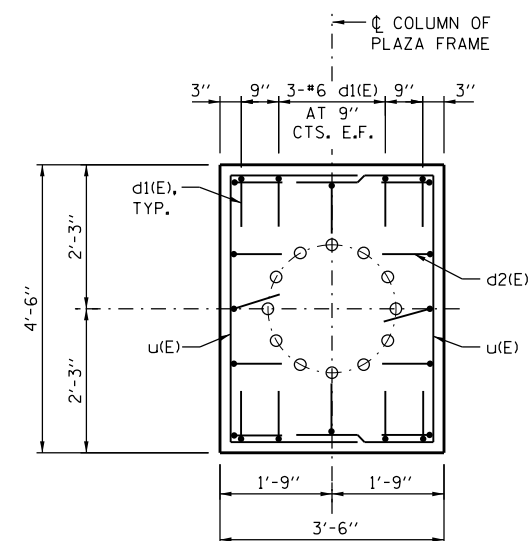
SECTION A-A



VIEW B-B



VIEW C-C



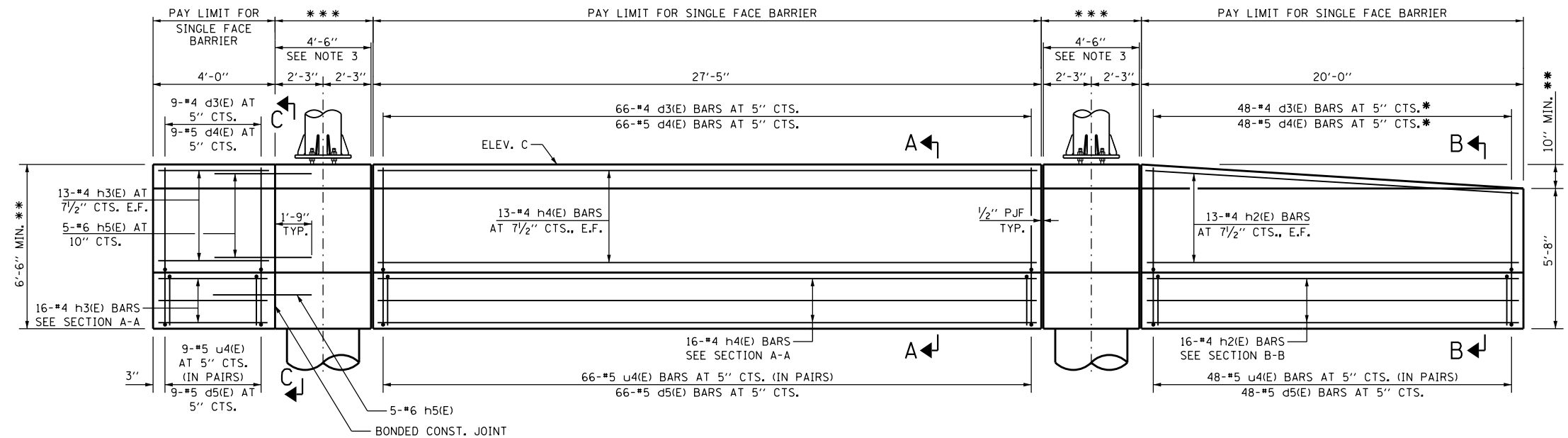
SECTION D-D

NOTES:

1. ANCHOR BOLT ASSEMBLY DETAIL, ANCHOR PLATE DETAIL AND BAR BENDING DIAGRAMS AND QUANTITIES ARE SHOWN ON SHEET 6 OF THIS SERIES.
2. SEE SHEET 6 OF THIS SERIES FOR ADDITIONAL NOTES.
3. SITE GROUNDING ELECTRODE SYSTEM TO BE PROVIDED AS INDICATED ON THE PLANS.

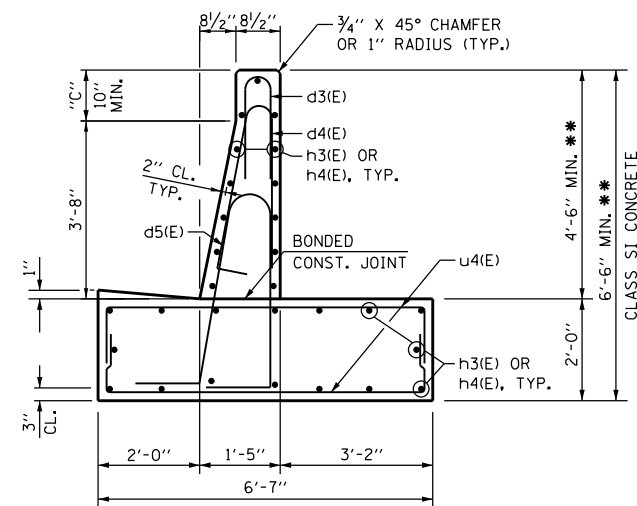
LEGEND:

E.F. - EACH FACE
CTS. - CENTERS

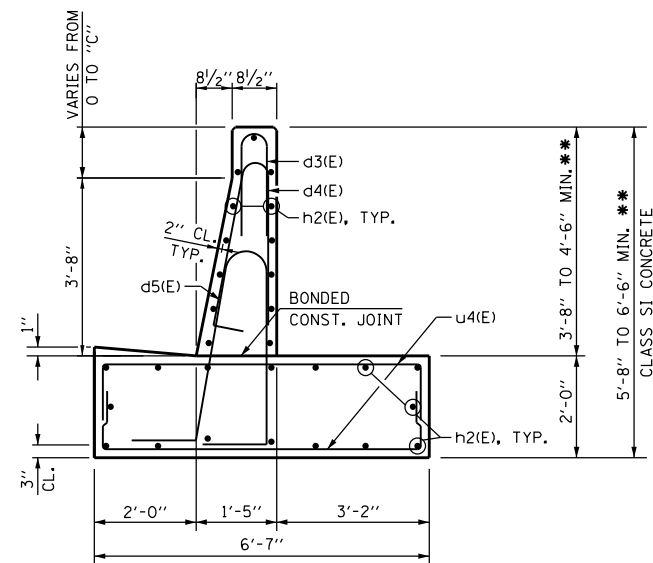


SINGLE FACE BARRIER ELEVATION
INSIDE FACE BARRIER IS SHOWN

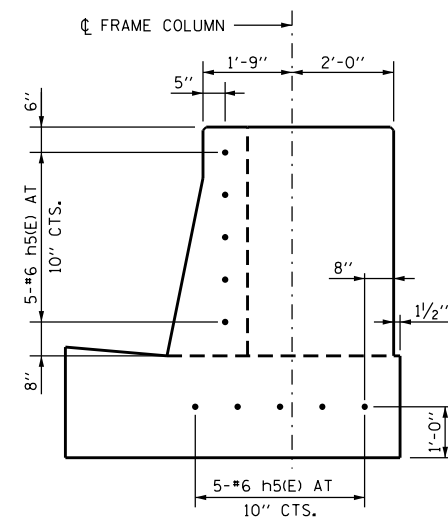
- * CUT IN FIELD AS REQUIRED TO FIT TAPER
 ** BASED ON DIMENSION "C" = 10"
 *** PAY LIMIT FOR FOUNDATION FOR OVERHEAD SIGN STRUCTURE



SECTION A-A



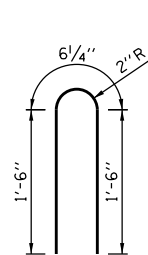
SECTION B-B



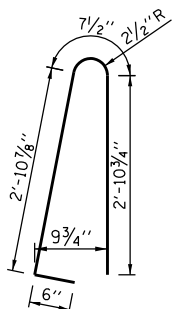
SECTION C-C

BAR LIST - ONE BARRIER

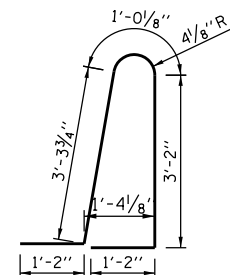
| BAR | NO. | SIZE | LENGTH | SHAPE |
|-------|-----|------|--------|-------|
| d3(E) | 123 | #4 | 3'-7" | U |
| d4(E) | 123 | #5 | 7'-0" | U |
| d5(E) | 123 | #5 | 9'-10" | U |
| h2(E) | 29 | #4 | 19'-7" | I |
| h3(E) | 29 | #4 | 2'-8" | I |
| h4(E) | 29 | #4 | 27'-1" | I |
| h5(E) | 10 | #6 | 3'-9" | I |
| u4(E) | 246 | #5 | 9'-3" | U |



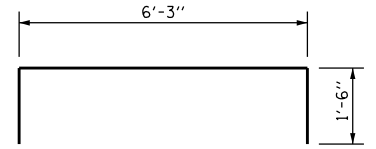
BAR d3(E)



BAR d4(E)



BAR d5(E)



BAR u4(E)

NOTES:

- PROTECTIVE COAT SHALL BE APPLIED TO THE TRAFFIC AND TOP FACES OF THE BARRIER, TOP FACE OF THE GUTTER AND TO THE ENTRANCE SIDE FACE (AT THE BEGINNING OF THE PLAZA PAVEMENT) FOR THE FULL HEIGHT OF THE BARRIER.
- FOR LOCATION OF ELECTRICAL JUNCTION BOXES ON THE WALL, SEE ELECTRICAL DETAIL SHEETS.
- FOR SINGLE FACE BARRIER FOUNDATION DETAILS FOR PLAZA FRAMES SEE SHEET 6 OF THIS SERIES.
- QUANTITIES FOR SINGLE FACE BARRIER ARE DETERMINED USING "C" = 10". IF DIMENSION "C" IS GREATER THAN 10", ADJUST QUANTITIES ACCORDINGLY.

ESTIMATED QUANTITY

(FOR ONE SINGLE FACE BARRIER)

| ITEM | UNIT | TOTAL |
|---------------------------------|---------|-------|
| CONCRETE STRUCTURES | CU. YD. | 33.6 |
| REINFORCMENT BARS, EPOXY COATED | POUND | 5,840 |
| PROTECTIVE COAT | SQ. YD. | 40.7 |

SHEET 8 OF 8



OVERHEAD SIGN STRUCTURE
MONOTUBE TYPE (STEEL)
MAINLINE STRUCTURE DETAILS

STANDARD F13-06

APPROVED, *Paul Kovacs* DATE 3-31-2014.
CHIEF ENGINEERING OFFICER

ALTERNATE DIRECTION OF HORIZONTAL
DIAGONALS FOR EACH BAY IN
PLANES OF UPPER AND LOWER CHORDS

UPPER HORIZONTAL
DIAGONALS, TYP.

LOWER HORIZONTAL
DIAGONALS, TYP.

TYPICAL PLAN

ALTERNATE VERTICAL DIAGONAL BRACING FOR EACH
BAY IN PLANES OF FRONT AND BACK CHORDS

STEEL POST,
COLUMN AND CABINET
(SEE NOTE 4)

DMS TYPE 2

HEIGHT OF
SIGN (D_s)

LOWEST PART OF
STRUCTURE ABOVE
ELEVATION A.

MINIMUM VERTICAL CLEAR.

ELEV. A_1
(LOCATION VARIES)

D_1
(ALONG ϕ OF TRUSS)

EDGE OF PAVEMENT

* D_2

ELEV. B

*ELEV. A_2
(LOCATION VARIES)

ELEV. A = ELEVATION AT POINT OF MINIMUM
CLEARANCE TO SIGN OR TRUSS.

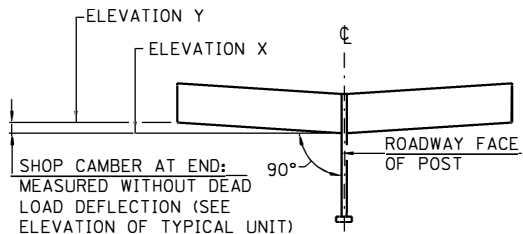
* ELEVATION A_2 AND DIMENSION D_2
NOT USED WHEN BUTTERFLY
STRUCTURE IS MOUNTED ON
RIGHT SIDE OF THE SHOULDER.

TYPICAL ELEVATION

LOOKING IN DIRECTION OF TRAFFIC

SHOP CAMBER TABLE

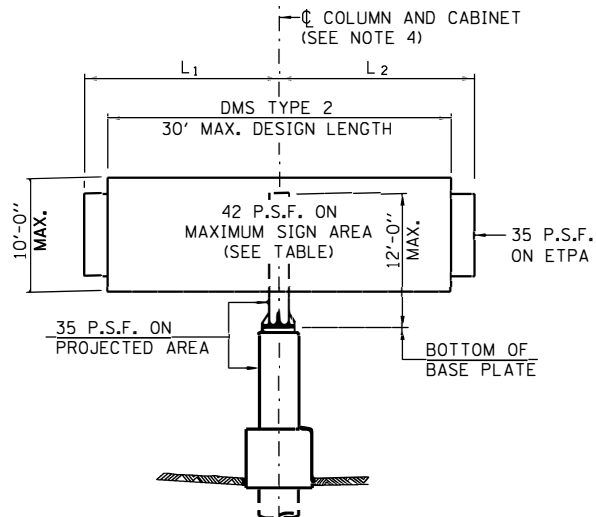
| UNIT LENGTH L_1 OR L_2 | SHOP CAMBER AT END |
|----------------------------------|-----------------------|
| 15' | $\frac{1}{4}$ " |
| 20' | $\frac{1}{2}$ " |
| 25' | $\frac{3}{4}$ " |



CAMBER DIAGRAM
(FOR FABRICATION ONLY)

DMS TYPE 2 TABLE

| TRUSS MOUNTING | MAXIMUM TOTAL AREA | MAXIMUM ALLOWABLE WEIGHT |
|----------------|--------------------|----------------------------------|
| ONE FACE | 300 SQ. FT. | 5000 LB. - CENTERED ON STRUCTURE |
| TWO FACE | 300 SQ. FT. | 6000 LB. - CENTERED ON STRUCTURE |



DESIGN WIND LOADING DIAGRAM

ETPA = EFFECTIVE TRUSS PROJECTED AREA

FABRICATION NOTES:

- MATERIALS: ALL STRUCTURAL STEEL PIPE SHALL BE ASTM A53 GRADE B OR ASTM A106 GRADE B OR API 5L GRADE X42 OR API 5L GRADE X52. ALL STRUCTURAL STEEL TUBE SHALL CONFORM TO ASTM A500 GRADE B. ALL STRUCTURAL STEEL PLATES AND SHAPE SHALL CONFORM TO AASHTO M270 GRADE 50 (M183 OR M223 GRADE 50). STAINLESS STEEL FOR SHIMS, SLEEVES AND HANDHOLE COVERS SHALL BE ASTM A240, TYPE 302 OR 304, OR ANOTHER ALLOY SUITABLE FOR EXTERIOR EXPOSURE AND ACCEPTABLE TO THE ENGINEER. THE STEEL PIPE AND STIFFENING RIBS AT THE BASE PLATE FOR THE COLUMN SHALL HAVE A MINIMUM LONGITUDINAL CHARPY V-NOTCH (CVN) ENERGY OF 15 LB.-FT. AT 40° F (ZONE 2) BEFORE GALVANIZING.
- WELDING: ALL WELDS TO BE CONTINUOUS UNLESS OTHERWISE SHOWN. ALL WELDING TO BE DONE IN ACCORDANCE WITH CURRENT AWS D1.1 STRUCTURAL WELDING CODE AND THE STANDARD SPECIFICATIONS.
- FASTENERS: HIGH STRENGTH BOLTS SHALL SATISFY THE REQUIREMENTS OF AASHTO M164 (ASTM A325), OR APPROVED ALTERNATE, AND SHALL HAVE MATCHING LOCKNUTS. THREADED STUDS FOR SPLICES (IF MEMBERS INTERFERE) SHALL SATISFY THE REQUIREMENTS OF ASTM A449, ASTM A193, GRADE B7, OR APPROVED ALTERNATE, AND SHALL HAVE MATCHING LOCKNUTS. BOLTS AND LOCKNUTS NOT REQUIRED TO BE HIGH STRENGTH SHALL SATISFY THE REQUIREMENTS OF ASTM A307. ALL BOLTS AND LOCKNUTS SHALL BE HOT DIP GALVANIZED PER AASHTO M232, EXCEPT STAINLESS STEEL FASTENERS, NUTS AND WASHERS. THE LOCKNUTS SHALL HAVE NYLON OR STEEL INSERTS. A STAINLESS STEEL FLAT WASHER CONFORMING TO ASTM A240 TYPE 302 OR 304, IS REQUIRED UNDER BOTH HEAD AND NUT OR UNDER BOTH NUTS WHERE THREADED STUDS ARE USED. HIGH STRENGTH BOLT INSTALLATION SHALL CONFORM TO ARTICLE 505.04(f)(2)d OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. ROTATIONAL CAPACITY ("ROCAP") TESTING OF BOLTS WILL NOT BE REQUIRED.
- U-BOLTS & EYEBOLTS: U-BOLTS AND EYEBOLTS SHALL BE PRODUCED FROM ASTM A276 TYPE 304, 304L, 316 OR 316L, CONDITION A, COLD FINISHED STAINLESS STEEL, OR AN EQUIVALENT MATERIAL ACCEPTABLE TO THE ENGINEER. ALL NUTS FOR U-BOLTS AND EYEBOLTS SHALL BE LOCK NUTS EQUIVALENT TO ASTM A307 WITH NYLON OR STEEL INSERTS AND HOT DIP GALVANIZED PER AASHTO M232. A STAINLESS STEEL FLAT WASHER CONFORMING TO ASTM A240, TYPE 302 OR 304, IS REQUIRED UNDER EACH U-BOLT AND EYEBOLT LOCKNUT.
- GALVANIZING: ALL STEEL PLATES, SHAPES AND PIPE SHALL BE HOT DIP GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH AASHTO M111. PAINTING IS NOT PERMITTED.
- ANCHOR BOLTS: SHALL CONFORM TO AASHTO M314 OR ASTM F1554 GRADE 55.

GENERAL NOTES:

- WORK THIS SHEET WITH OVERHEAD SIGN STRUCTURE BUTTERFLY TYPE (STEEL) SUMMARY AND TOTAL BILL OF MATERIAL SHEET.
- AFTER ADJUSTMENTS TO LEVEL TRUSS AND ENSURE ADEQUATE VERTICAL CLEARANCE, ALL TOP AND BOTTOM LEVELING NUTS SHALL BE TIGHTENED AGAINST THE BASE PLATE WITH A MINIMUM TORQUE OF 200 LB.-FT. STAINLESS STEEL MESH SHALL THEN BE PLACED AROUND THE PERIMETER OF THE BASE PLATE. SECURE TO BASE PLATE WITH STAINLESS STEEL BANDING.
- CENTERLINE DMS TYPE 2 SHALL BE LOCATED AT CENTERLINE OF COLUMN.
- SIGN SUPPORT STRUCTURES MAY BE SUBJECT TO DAMAGING VIBRATIONS AND OSCILLATIONS WHEN DMS ARE NOT IN PLACE DURING ERECTION OR MAINTENANCE OF THE STRUCTURE. TO AVOID THESE VIBRATIONS AND OSCILLATIONS, CONSIDERATION SHOULD BE GIVEN TO ATTACHING TEMPORARY BLANK SIGN PANELS TO THE STRUCTURE.
- TRUSSES SHALL BE SHIPPED INDIVIDUALLY WITH ADEQUATE PROVISION TO PREVENT DETRIMENTAL MOTION DURING TRANSPORT. THIS MAY REQUIRE ROPES BETWEEN HORIZONTALS AND DIAGONALS OR ENERGY DISSIPATING (ELASTIC) TIES TO THE VEHICLE. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE CONFIGURATION AND PROTECTION OF THE TRUSSES.
- PROVIDE RUBBED SURFACE FINISH FOLLOWED BY CONCRETE SEALER APPLICATION ON ENTIRE SURFACE OF CONCRETE COLUMN AND ALL SURFACES OF CRASHWALL, EXCEPT BOTTOM SURFACE.
- REINFORCEMENT BARS: REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
- PARAMETERS SHOWN ARE BASIS FOR THIS STANDARD. INSTALLATION NOT WITHIN DIMENSIONAL LIMITS SHOWN REQUIRE SPECIAL ANALYSIS FOR ALL COMPONENTS.
- IT IS PERMISSIBLE TO MOUNT TWO DMS TYPE 2 ON THE BUTTERFLY TRUSS, ONE ON EACH FACE OF THE TRUSS. THE TOTAL COMBINED DEPTH OF DMS TYPE 2 SHALL NOT EXCEED 4'-4" AND THE TOTAL COMBINED WEIGHT SHALL NOT EXCEED 6000 LB. CENTER THE DMS TYPE 2 ON ϕ STEEL POST. DO NOT INSTALL SIGN PANEL IN CONJUNCTION WITH DMS TYPE 2 SIGN CABINETS ON ONE FACE OF THE TRUSS. A SIGN PANEL ON ONE FACE AND DMS TYPE 2 ON THE OTHER IS PERMITTED.

CONSTRUCTION SPECIFICATIONS:

- ALL MATERIALS, EXCEPT AS SHOWN, FABRICATION, ERECTION AND CONSTRUCTION REQUIREMENTS SHALL BE IN ACCORDANCE WITH SECTION 733 AND 734 OF THE LATEST ILLINOIS TOLLWAY SUPPLEMENTAL SPECIFICATIONS.

LOADING:

- 90 M.P.H. WIND VELOCITY. WIND LOADING: 42 P.S.F. NORMAL TO DMS TYPE 2 CABINET AREA AND 35 P.S.F. NORMAL TO TRUSS ELEMENTS NOT BEHIND SIGN LOADING DIAGRAM.
- THE AASHTO GROUP II AND III ALLOWABLE STRESS SHALL BE 133% (ALLOWABLE STRESS DESIGN).

DESIGN SPECIFICATIONS:

THESE STRUCTURES ARE DESIGNED TO SATISFY THE 2013 AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, SIXTH EDITION.

CONCRETE COLUMN, CRASHWALL AND DRILLED SHAFT ARE DESIGNED IN ACCORDANCE WITH THE 2012 EDITION OF THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS (INCLUDING THE 2013 INTERIM REVISIONS).

DESIGN UNIT STRESSES FOR REINFORCED CONCRETE:

CLASS SI CONCRETE: $f'_c = 3,500$ P.S.I.
CLASS DS CONCRETE: $f'_c = 4,000$ P.S.I.
REINFORCING STEEL: $f_y = 60,000$ P.S.I.

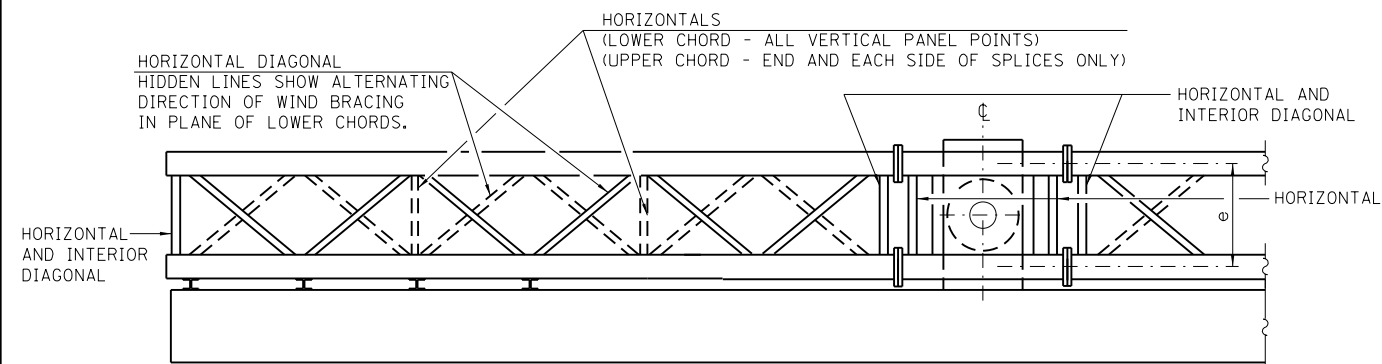
| DATE | REVISIONS |
|-----------|----------------------------------------------------------------------|
| 7-01-2014 | REVISED NOTES |
| 3-11-2015 | REVISED NOTES |
| 3-31-2016 | ADDED FOUNDATION NOTE AND REMOVED WALKWAY GRATING |
| 3-01-2018 | REVISED SIGN STRUCTURE |
| 3-01-2019 | REVISED NOTE TO APPLY PROTECTIVE COAT TO THE PERIMETER OF THE COLUMN |
| 2-13-2020 | UPDATED CRASH WALL HEIGHT. ADDED HEAVY HEX NUT TO ANCHORS |



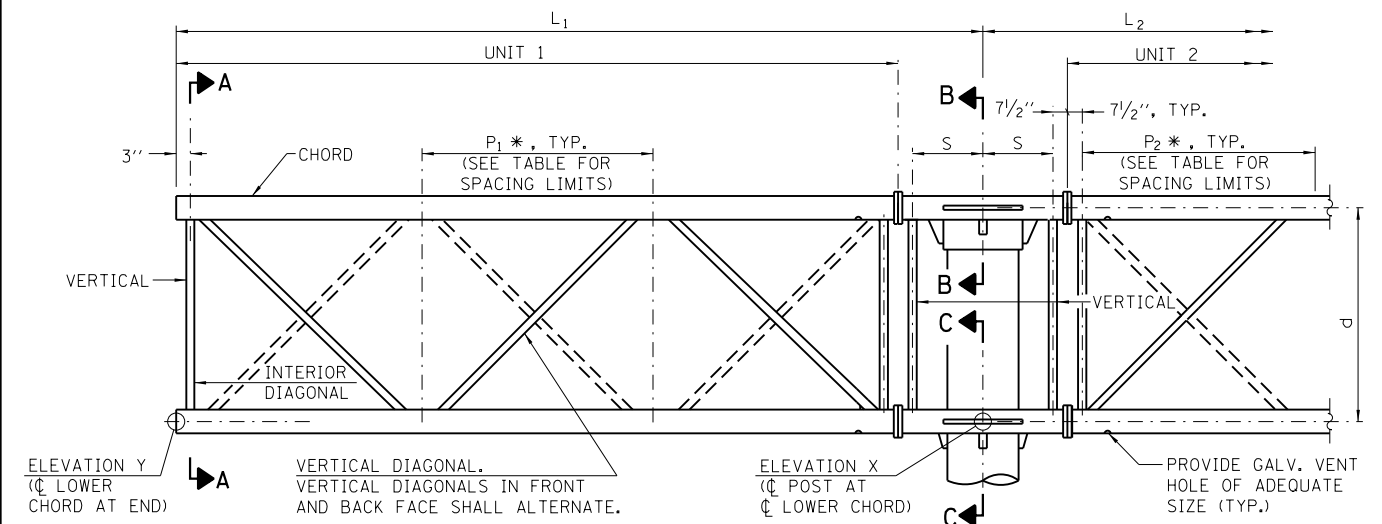
OVERHEAD SIGN STRUCTURE
BUTTERFLY TYPE
STRUCTURE DETAILS

STANDARD F14-05

APPROVED DATE 3-31-2014.
CHIEF ENGINEERING OFFICER



PLAN

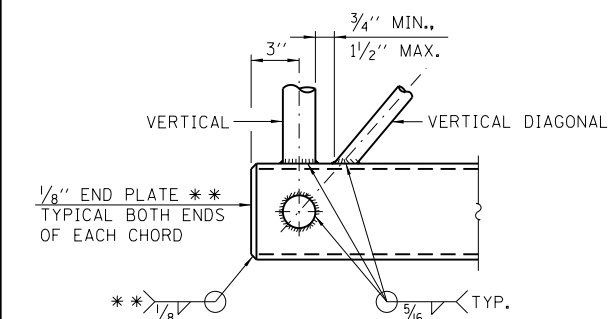


ELEVATION

(SIGN OMITTED FOR CLARITY)

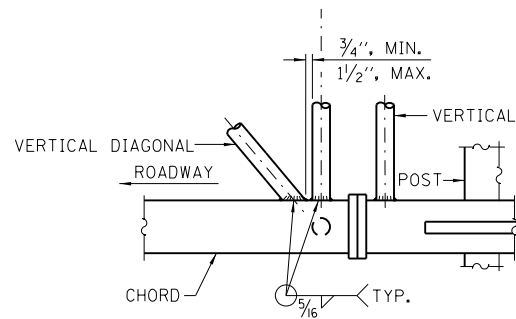
TYPICAL TRUSS UNIT

FOR SECTION B-B AND SECTION C-C, SEE SHEET 3 OF THIS SERIES

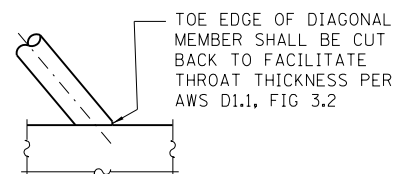


BUTTERFLY END JOINT DETAIL

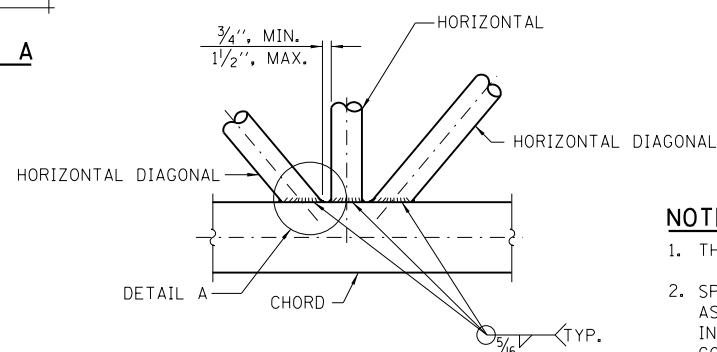
** CONTRACTOR MAY ALTERNATIVELY USE STANDARD STEEL DRIVE-FIT CAP TO CLOSE ENDS. 1/2" Ø DRAIN HOLE IN END PLATE / DRIVE-FIT CAP.



POST END JOINT DETAIL



DETAIL A

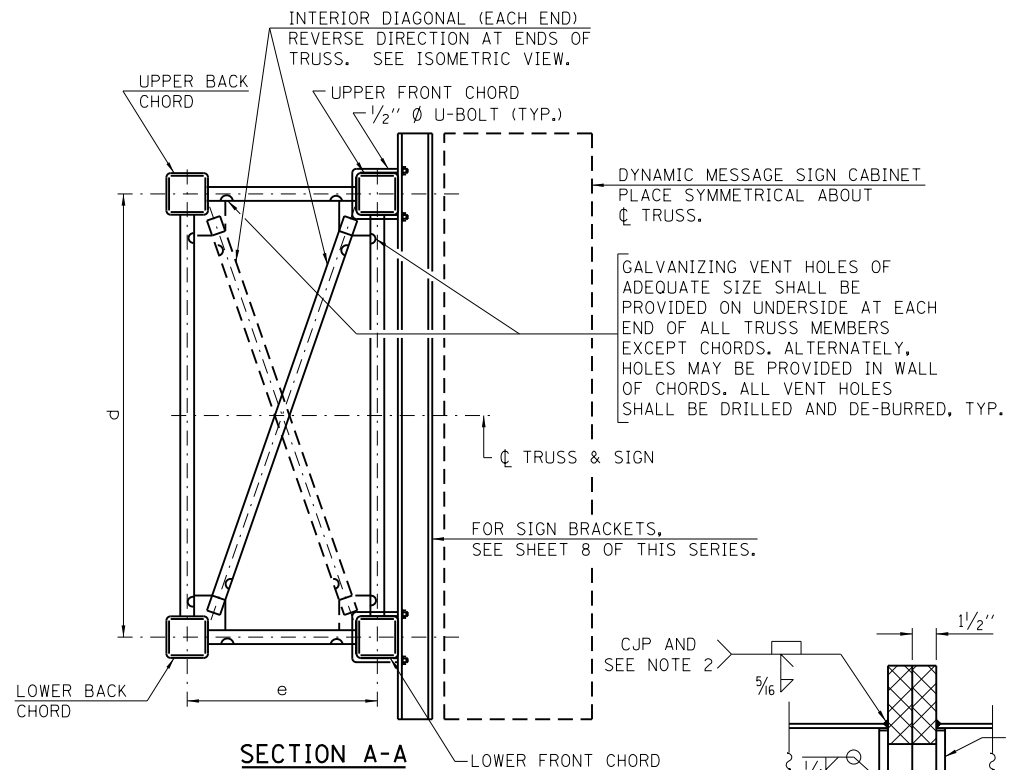


TRUSS INTERIOR JOINT DETAIL

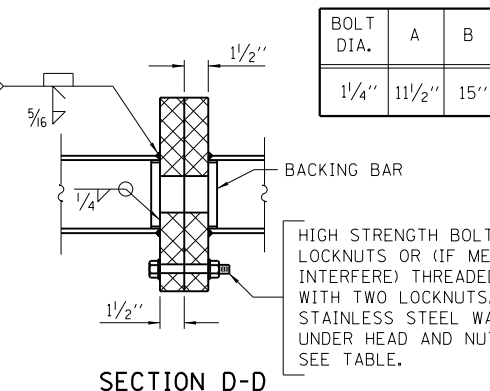
TRUSS UNIT TABLE

| TRUSS SIZE | | MAXIMUM DMS TYPE 2 SIGN LENGTH | STEEL SUPPORT POST (COLUMN) | | | | TRUSS MEMBERS AND DETAILS | | | | | | |
|------------|-------|--------------------------------|-----------------------------|---------------|-----------------------------|----------|---------------------------|----------|----------------|------------|------------------|----------------|--------------------------------|
| | | | DIAMETER | WEIGHT | WALL THICKNESS (SEE NOTE 3) | H (MAX.) | TOP & BOTTOM CHORD | VERTICAL | VERTICAL DIAG. | HORIZONTAL | HORIZONTAL DIAG. | INTERIOR DIAG. | LIMITS FOR PANEL SPACING (P) * |
| e | d | | | | | | | PIPE | PIPE | PIPE | PIPE | PIPE | |
| 3'-9" | 7'-0" | 30'-0" | 24" | 125.61 (#/FT) | 1/2" | 12'-0" | HSS 6x6x5/16 | 3"Ø X.S | 4"Ø X.X.S | 2"Ø X.S | 2 1/2"Ø X.S | 2"Ø X.S | 48" MIN. TO 66" MAX. |

* P = $\frac{L-S-1'-6''}{\# \text{ PANELS}}$

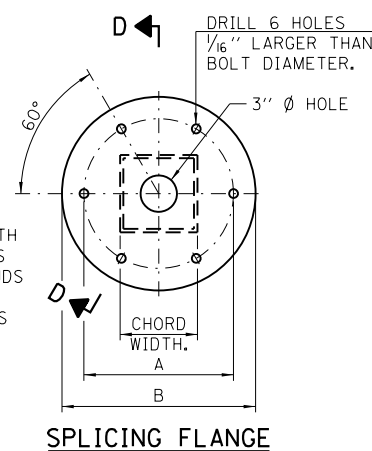


SECTION A-A

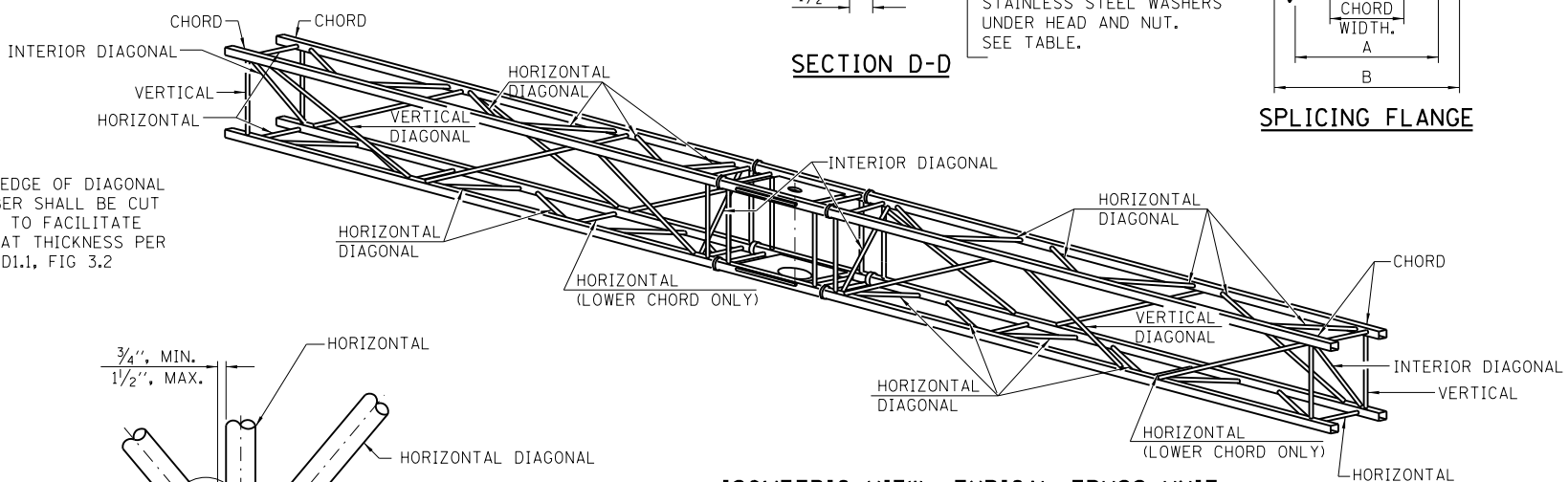


SECTION D-D

| BOLT DIA. | A | B |
|-----------|---------|-----|
| 1 1/4" | 11 1/2" | 15" |



SPlicing FLANGE

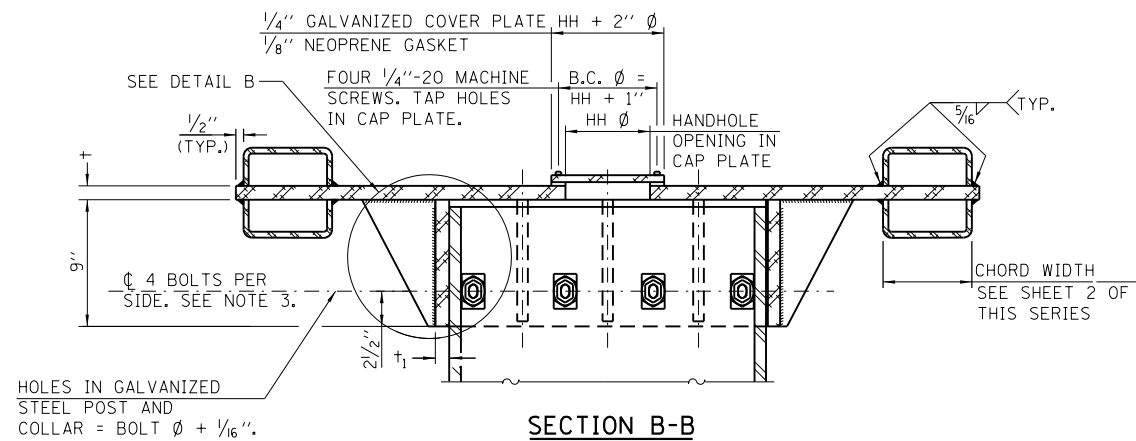


ISOMETRIC VIEW- TYPICAL TRUSS UNIT

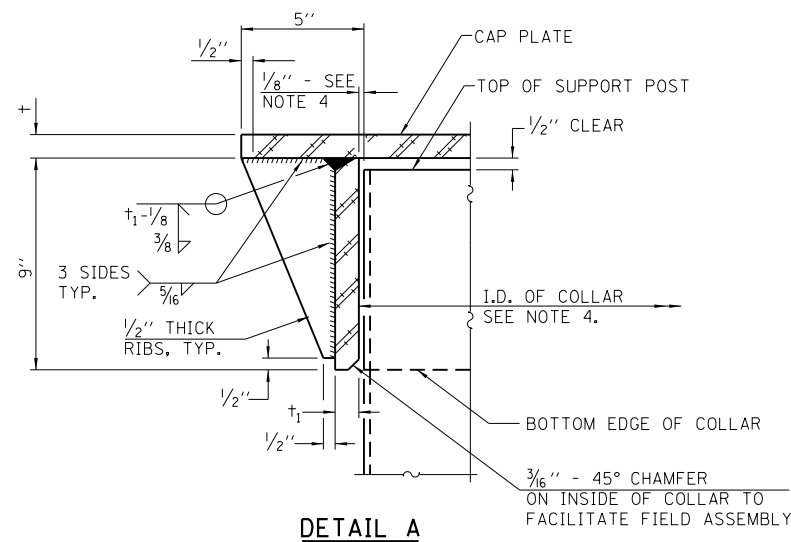
NOTES:

1. THERE ARE TWICE AS MANY HORIZONTAL DIAGONALS AS THERE ARE VERTICAL DIAGONALS.
2. SPlicing FLANGES SHALL BE ATTACHED TO EACH TRUSS UNIT WITH THE TRUSS SHOP ASSEMBLED TO CAMBER SHOWN ON SHEET 1 OF THIS SERIES. TRUSS UNITS SHALL BE IN PROPER ALIGNMENT AND FLANGE SURFACES SHALL BE SHOP BOLTED INTO FULL CONTACT BEFORE WELDING. SUFFICIENT EXTERNAL WELDS OR TACKS SHALL BE MADE TO SECURE FLANGES UNTIL REMAINING WELDS ARE MADE AFTER DISASSEMBLY. ADJACENT FLANGES SHALL BE "MATCH MARKED" TO INSURE PROPER FIELD ASSEMBLY.
3. NOMINAL WALL THICKNESS SHOWN. THICKER WALL IS PERMITTED UPON ENGINEER'S APPROVAL.

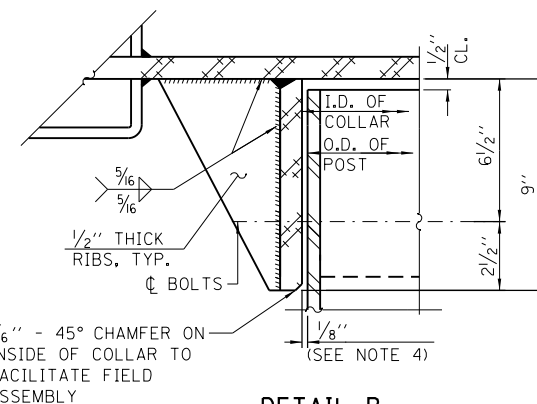




SECTION B-B
BOLTS SHALL BE HIGH STRENGTH WASHERS (INCLUDING CONTOURED WASHERS), AND LOCKNUTS SHALL BE STAINLESS STEEL.

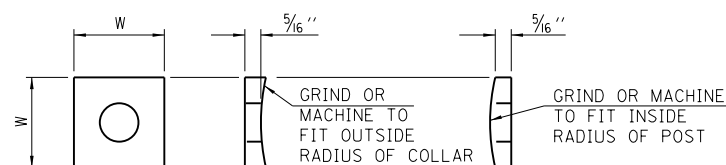


DETAIL A



DETAIL B

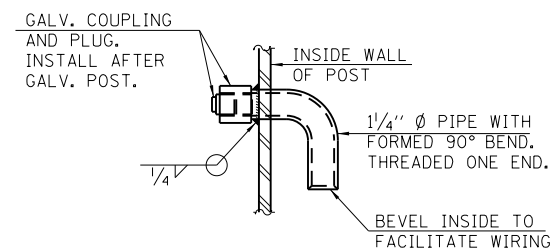
(FOR DETAILS NOT SHOWN, SEE DETAIL C)



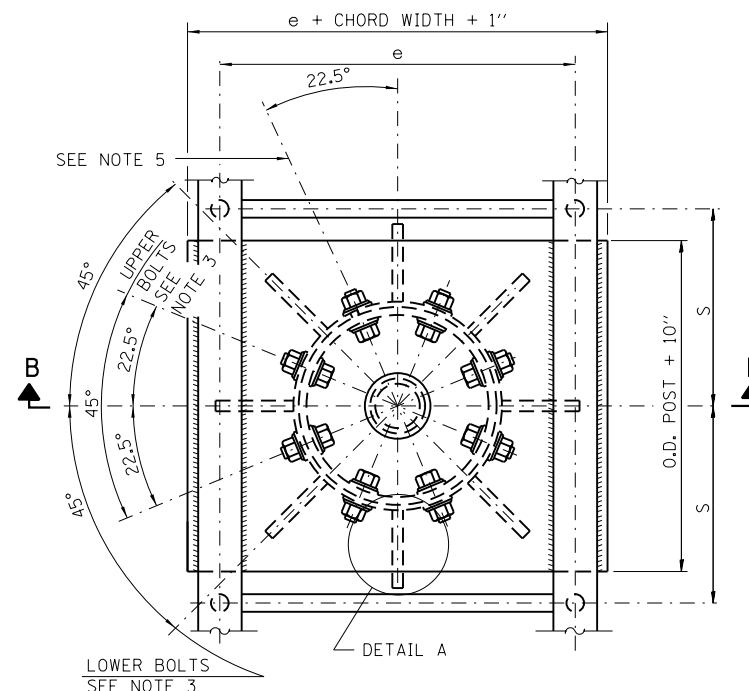
CONTOURED WASHERS

| BOLT SIZE | CONTOURED WASHERS HOLE DIA. | W |
|-----------|-----------------------------|--------|
| 7/8" | 1" | 2 1/2" |
| 1" | 1 1/8" | 3" |
| 1 1/4" | 1 3/8" | 3 1/4" |

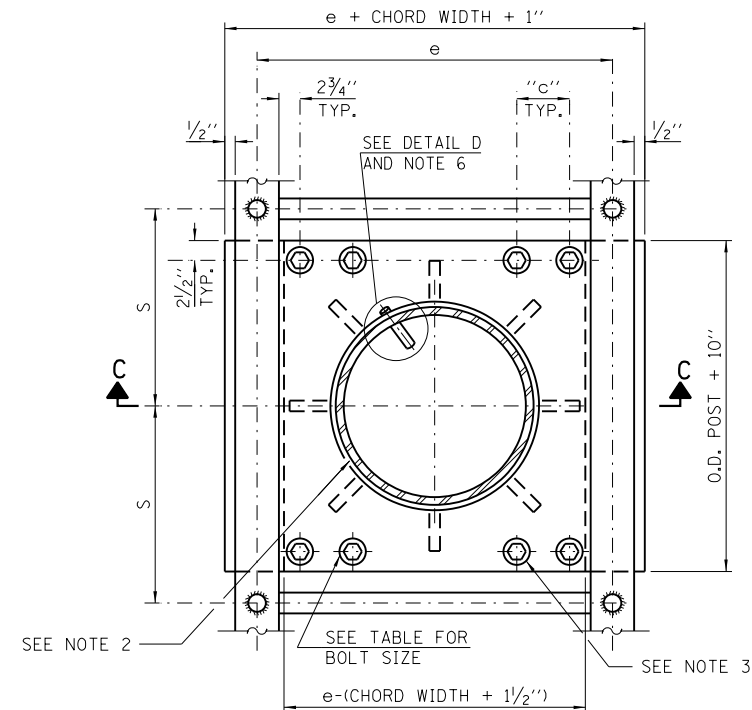
(ASTM A240, TYPE 304)



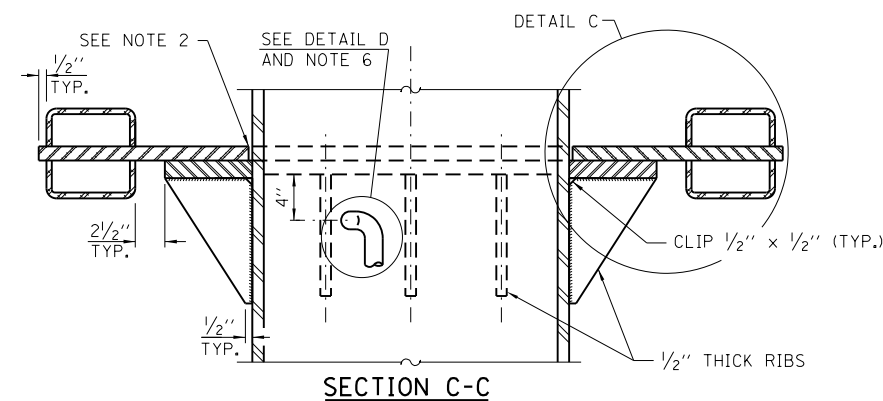
DETAIL D



PLAN VIEW - TOP OF COLUMN



SECTION THRU POST ABOVE LOWER CHORDS



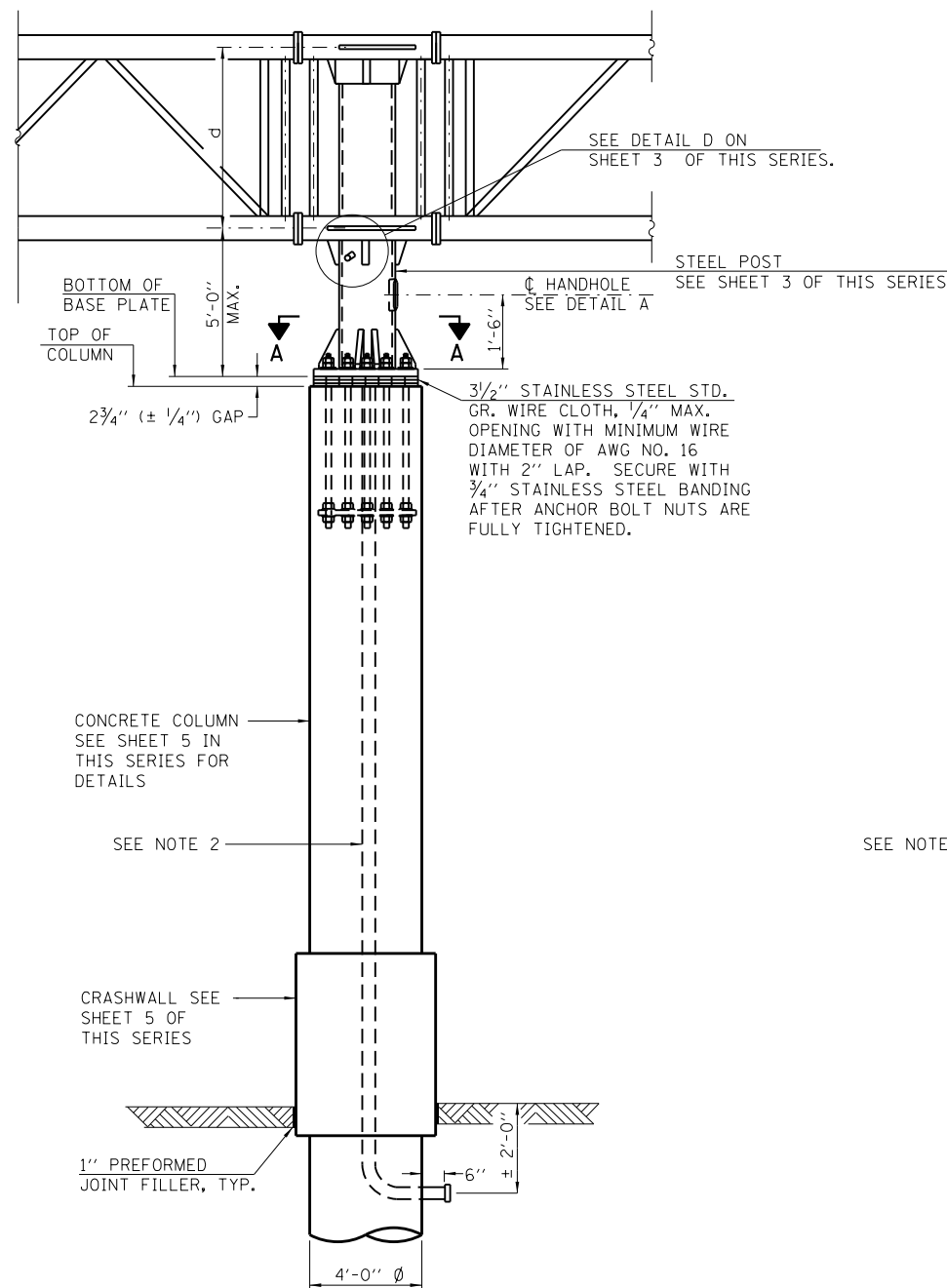
SECTION C-C

NOTES:

- GRIND TOP IF REQUIRED TO FULLY SEAT PLATE. REPAIR DAMAGED GALVANIZING BEFORE ASSEMBLY.
- AFTER TIGHTENING LOWER CONNECTION BOLTS, FILL GAP WITH NON-HARDENING, SILICONE CAULK SUITABLE FOR EXTERIOR EXPOSURE AND ACCEPTABLE TO THE ENGINEER.
- CONNECTION BOLTS IN COLLAR AND BOLTS AT LOWER CHORD CONNECTION SHALL BE HIGH STRENGTH WITH MATCHING LOCKNUTS. CONNECTION BOLTS SHALL HAVE TWO STAINLESS STEEL FLAT WASHERS EACH.
- COLLAR I.D. SHALL BE MANUFACTURED TO CORRESPOND TO O.D. OF ACTUAL GALVANIZED POST PLUS 1/8" ($\pm 1/16$ "). MAXIMUM GAP BETWEEN POST AND COLLAR AT ANY LOCATION EQUALS 1/8" BEFORE TIGHTENING BOLTS.
- OPTIONAL FULL PENETRATION WELD IN COLLAR. (TWO LOCATIONS MAXIMUM (180° APART) X-RAY OR UT 100%)
- ORIENT PIPE TOWARD WALKWAY SIDE. HOLE IN POST = O.D. PIPE + 1/8".

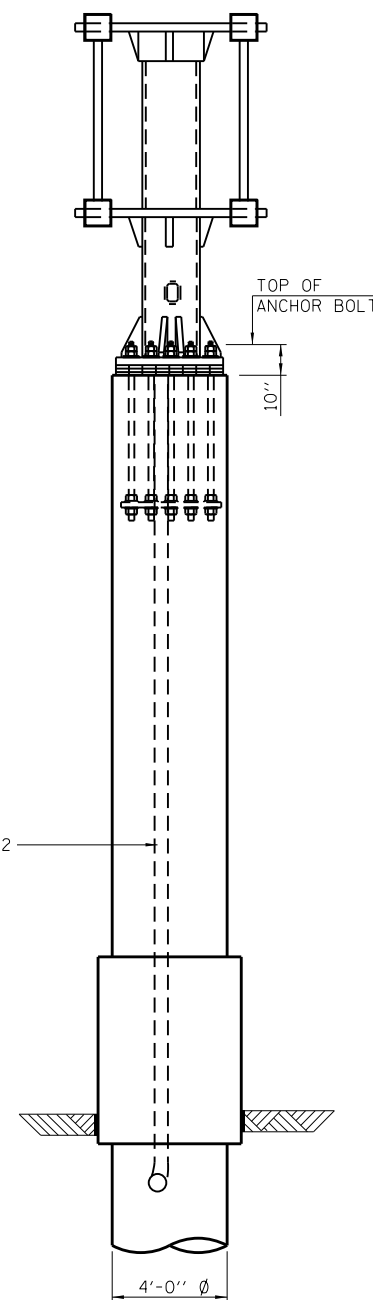
CONNECTION TABLE

| POST OUTSIDE DIAMETER | UPPER & LOWER CONNECTION BOLT DIAMETER (SEE NOTE 3) | LOWER JUNCTURE BOLT SPACING DIMENSION "C" (SEE NOTE 3) | OPENING IN CAP PLATE "HH" | PLATE THICKNESS (+) | COLLAR THICKNESS (t ₁) |
|-----------------------|-----------------------------------------------------|--------------------------------------------------------|---------------------------|---------------------|------------------------------------|
| 24" | 1 1/4" | 3 1/2" | 6" | 1" | 7/8" |

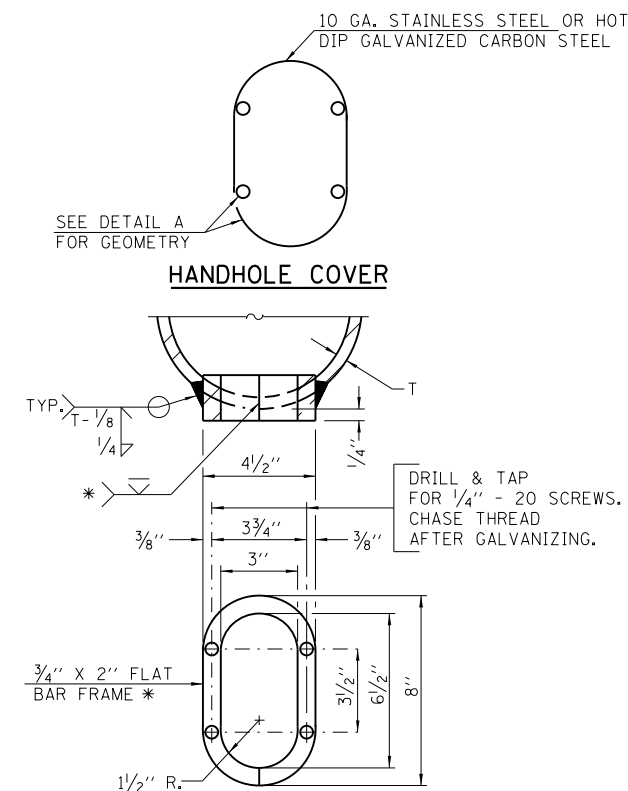


FRONT ELEVATION

SEE SHEET 5 OF THIS SERIES FOR FOUNDATION DETAILS.
(DMS TYPE 2 SIGN CABINET NOT SHOWN FOR CLARITY)



SIDE ELEVATION



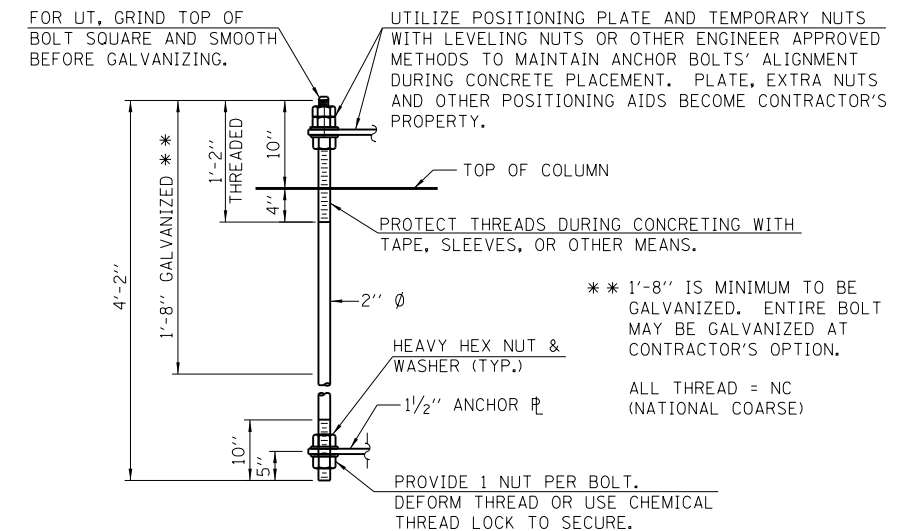
PROVIDE 8" X 4 1/2" COVER. OUTSIDE CORNERS = 2 1/4" RADIUS. PROVIDE 4-5/16" Ø HOLES IN FOR 1/4" - 20 ROUND HEAD HOT DIP GALVANIZED OR STAINLESS STEEL MACHINE SCREWS. (SEE COVER DETAILS.)

DETAIL A

* BENT BARS MAY BE BUTT WELDED TOP AND BOTTOM OR BOTTOM ONLY. IN LIEU OF FABRICATED HANDHOLE FRAME AS SHOWN, MAY CUT FROM 2" PLATE (ROLLING DIRECTION VERTICAL). ALL CUT FACES TO BE GROUND TO ANSI ROUGHNESS OF 500 µin OR LESS.

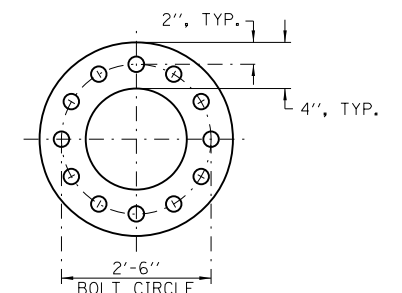
NOTE:

1. SITE GROUNDING ELECTRODE SYSTEM TO BE PROVIDED AS INDICATED ON PLANS.
2. SEE PLAN SHEETS FOR TYPE, SIZE AND NUMBER OF CONDUITS.

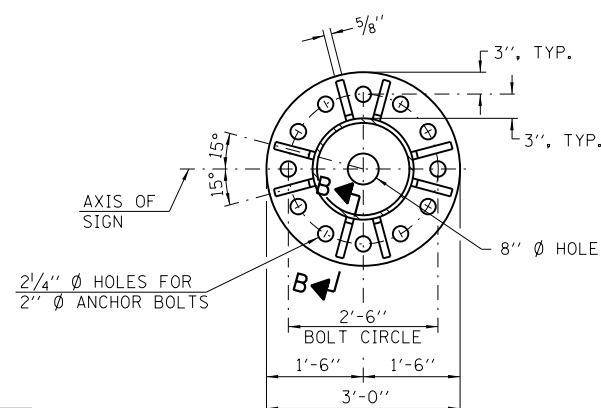


ANCHOR BOLT DETAIL

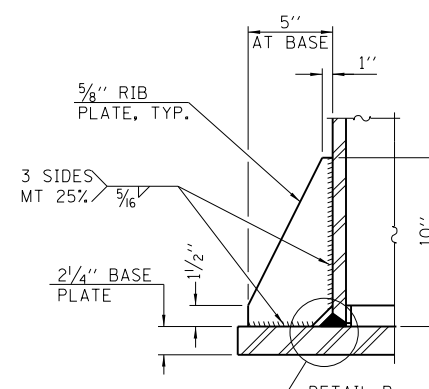
ANCHOR BOLTS SHALL CONFORM TO AASHTO M314 OR ASTM F1554 GRADE 55. GALVANIZE THE UPPER 1'-8" (MINIMUM (**)) AND ASSOCIATED AASHTO M291, GRADE A, C OR DH HEAVY HEX NUTS AND HARDENED WASHERS PER AASHTO M232. NO WELDING SHALL BE PERMITTED ON BOLTS. PROVIDE A NUT AT BOTTOM, A HEXAGON LOCKNUT AND WASHER ABOVE BASE PLATE AND A LEVELING NUT AND WASHER BELOW BASE PLATE. NUTS SHALL EACH BE TIGHTENED WITH 200 LB.-FT. MINIMUM TORQUE AGAINST BASE PLATE. BEFORE OR AFTER THREADING, BUT BEFORE GALVANIZING, EACH ANCHOR BOLT SHALL BE ULTRASONICALLY TESTED (UT) BY A LEVEL II OR III INSPECTOR, QUALIFIED IN ACCORD WITH ANSI GUIDELINES, TO ENSURE NO REJECTABLE FLAWS EXIST IN THE UPPER 18" (TENSION CRITERIA).



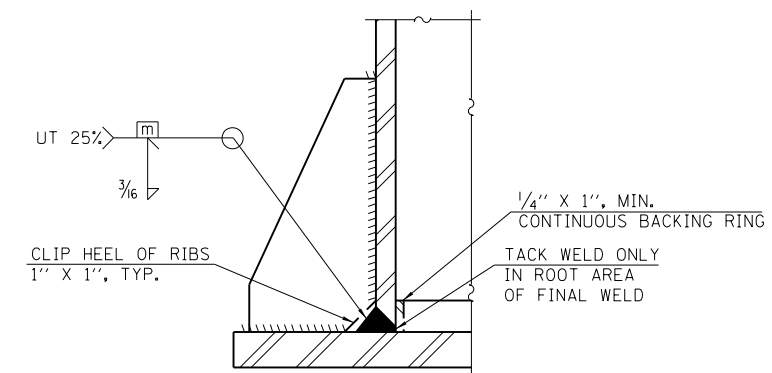
POSITIONING PLATE/ANCHOR BOLT



SECTION A-A



SECTION B-B



DETAIL B (TYPICAL RIB)

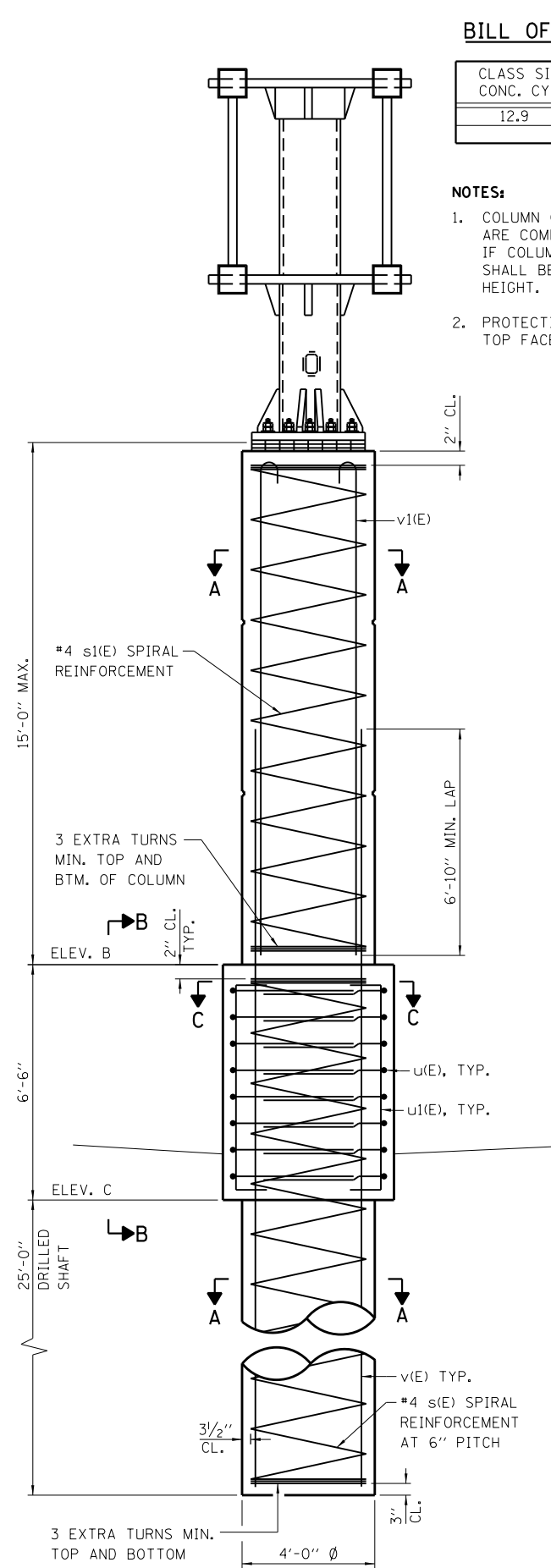
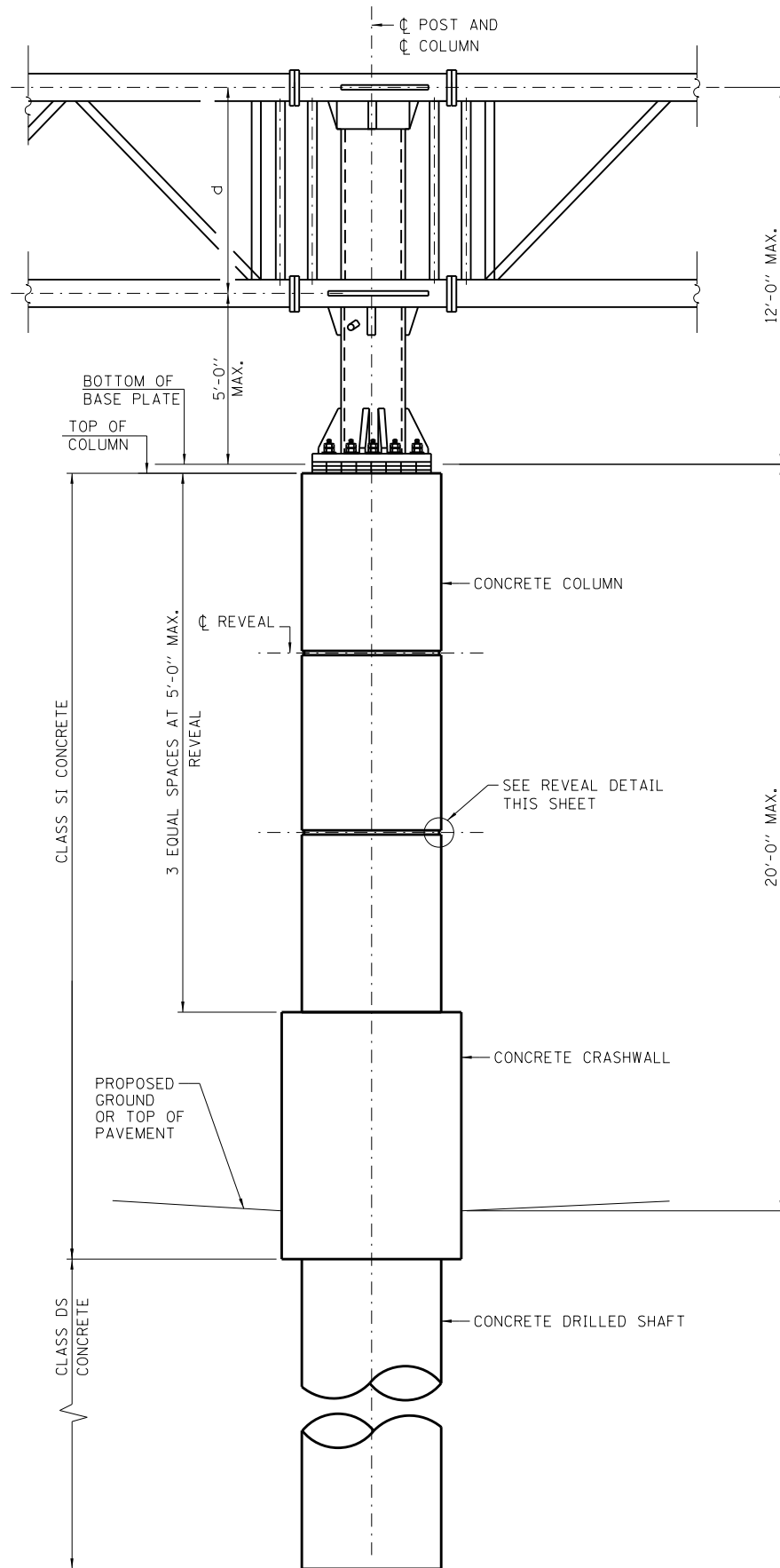
SHEET 4 OF 8



OVERHEAD SIGN STRUCTURE
BUTTERFLY TYPE
STRUCTURE DETAILS

STANDARD F14-05

APPROVED DATE 3-31-2014.
CHIEF ENGINEERING OFFICER



BILL OF MATERIAL-EACH FOUNDATION

| CLASS SI CONC. CY | CLASS DS CONC. CY | REBAR POUNDS | PROTECTIVE COAT SQ. YD. |
|-------------------|-------------------|--------------|-------------------------|
| 12.9 | 11.7 | 4,790 | 6.0 |

NOTES:

- COLUMN CONCRETE VOLUME AND BAR s1(E) LENGTH ARE COMPUTED BASED ON 15'-0" COLUMN HEIGHT. IF COLUMN HEIGHT IS NOT EQUAL 15'-0", QUANTITIES SHALL BE CALCULATED BASED ON ACTUAL COLUMN HEIGHT.
- PROTECTIVE COAT SHALL BE APPLIED TO TRAFFIC AND TOP FACES OF CRASHWALL AND PERIMETER OF THE COLUMN.

BAR LIST-EACH FOUNDATION

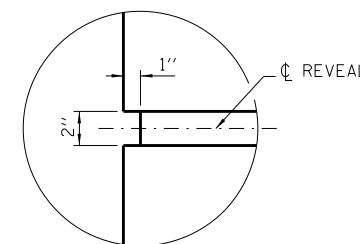
(COLUMN, CRASHWALL AND DRILLED SHAFT)

| BAR | NUMBER | SIZE | LENGTH | SHAPE |
|-------|--------|------|--------|-------|
| v(E) | 20 | #9 | 38'-3" | — |
| v1(E) | 20 | #9 | 15'-8" | — |
| s(E) | 1 | #4 | 31'-1" | WWWW |
| s1(E) | 1 | #4 | 14'-5" | WWWW |
| u(E) | 12 | #5 | 12'-2" | — |
| u1(E) | 18 | #5 | 8'-7" | — |

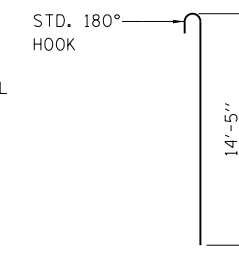
* THE LENGTH OF SPIRAL SHOWN IS THE HEIGHT OF SPIRAL

| BAR | "A" | "B" |
|-------|-------|-------|
| u(E) | 3'-9" | 4'-8" |
| u1(E) | 1'-3" | 6'-1" |

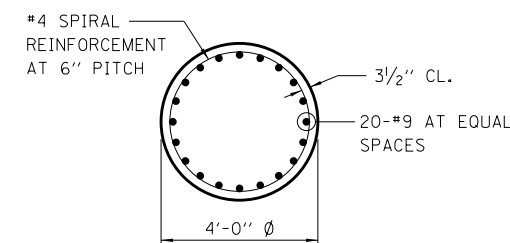
BAR u(E), u1(E)



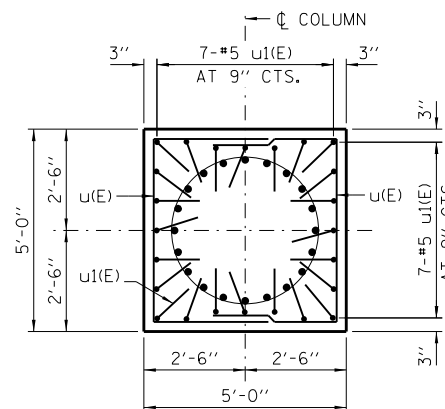
REVEAL DETAIL



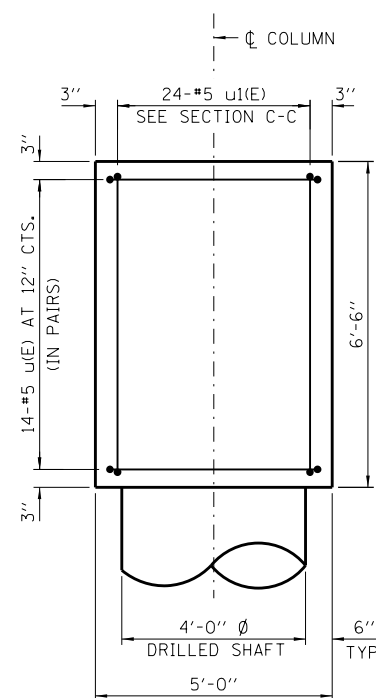
BAR v1(E)



SECTION A-A



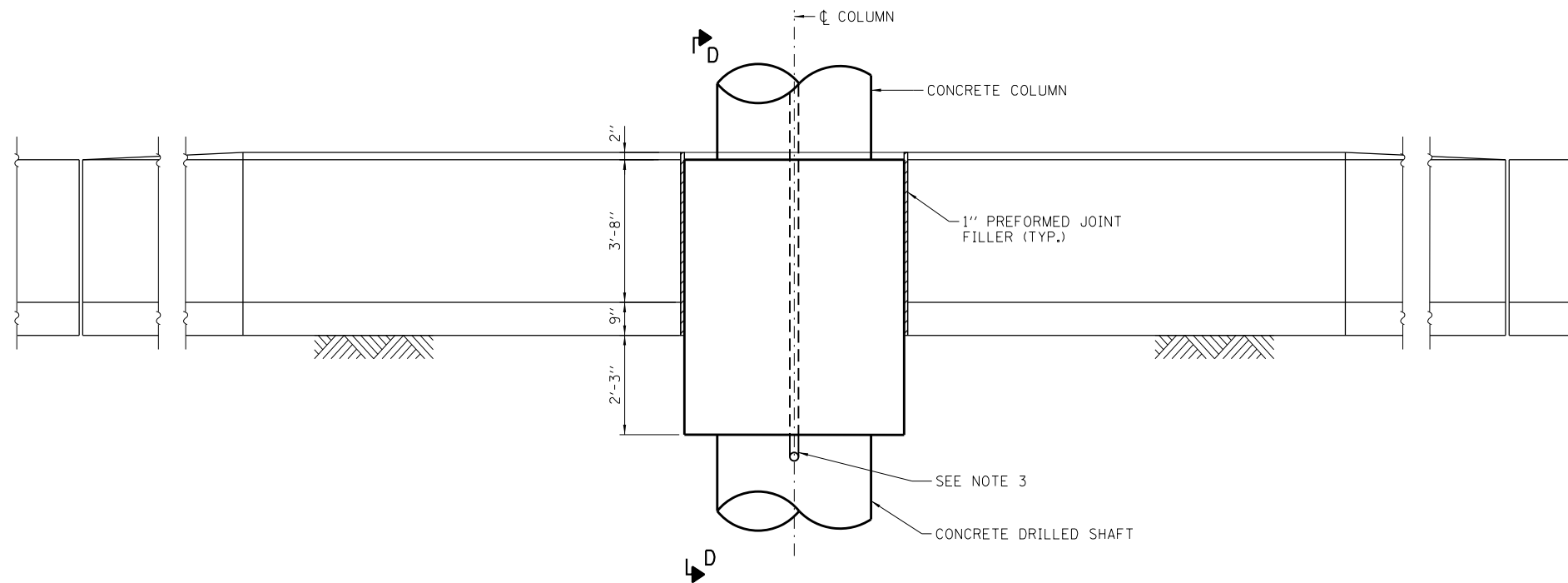
SECTION C-C



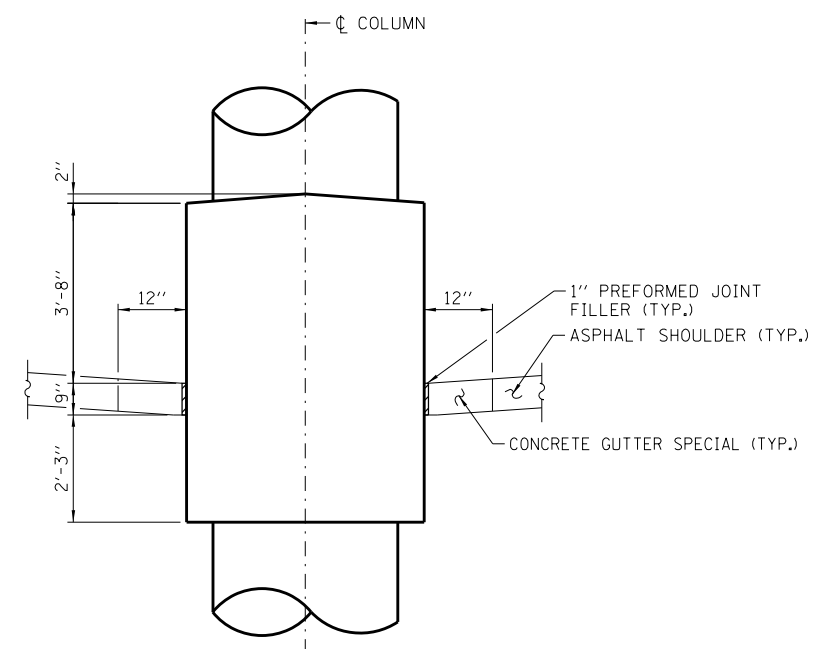
SECTION B-B

FOUNDATIONS:

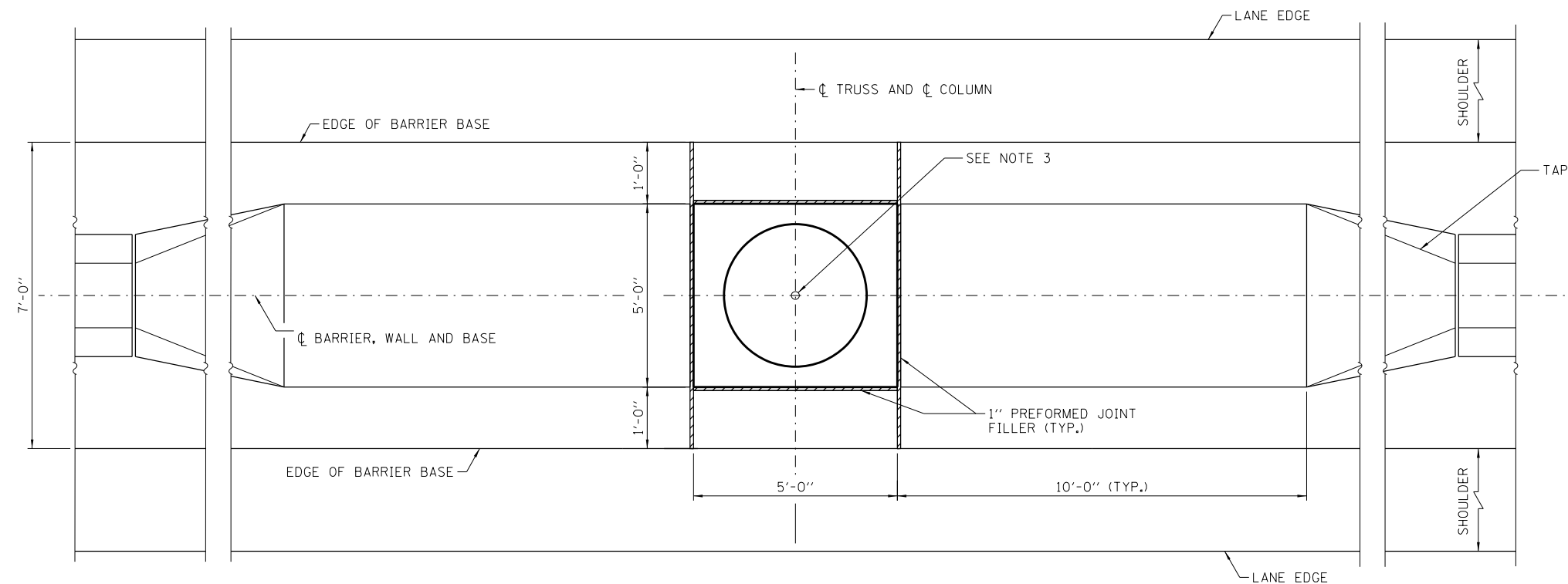
THE FOUNDATION DETAILS SHOWN ARE BASED ON THE PRESENCE OF MOSTLY COMMON COHESIVE SOIL CONDITIONS (SILTY OR SANDY CLAY), WITH AN AVERAGE UNCONFINED COMPRESSIVE STRENGTH (QU) > 1.25 TON/SQ. FT. WHICH SHALL BE DETERMINED BY PREVIOUS SOIL INVESTIGATIONS AT THE JOBSITE. WHEN OTHER CONDITIONS ARE INDICATED, THE BORING DATA SHALL BE INCLUDED IN THE PLANS AND THE FOUNDATION DIMENSIONS SHOWN SHALL BE THE RESULT OF SITE SPECIFIC DESIGNS. IF CONDITIONS ENCOUNTERED IN THE FIELD ARE DIFFERENT THAN THOSE INDICATED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER TO DETERMINE IF THE FOUNDATION DIMENSIONS NEED TO BE MODIFIED.



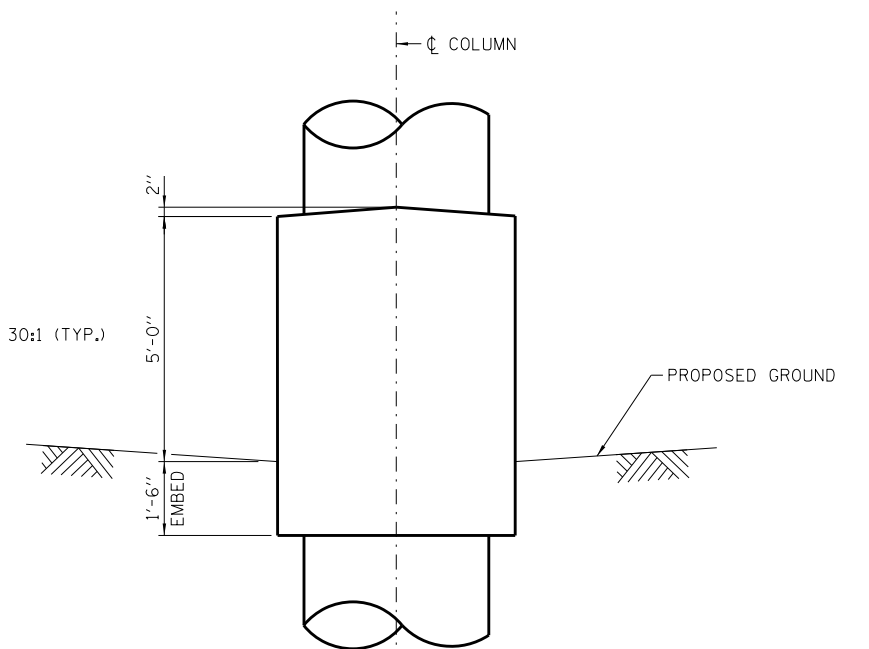
SIDE ELEVATION



SECTION D-D
FOUNDATION LOCATED IN PAVED ROADWAY MEDIAN



PLAN



SECTION D-D
FOUNDATION LOCATED IN UNPAVED ROADWAY MEDIAN

NOTES:

1. SIDE ELEVATION AND PLAN VIEW ARE SHOWN FOR FOUNDATION LOCATED IN PAVED MEDIAN.
2. SEE SHEET 5 OF THIS SERIES FOR REINFORCEMENT DETAILS.
3. COORDINATE CONDUIT SIZE, LOCATION AND QUANTITY WITH ELECTRICAL PLANS. CONDUITS SHALL BE PLACED TO MISS REINFORCEMENT BARS. DO NOT CUT REINFORCEMENT BARS.

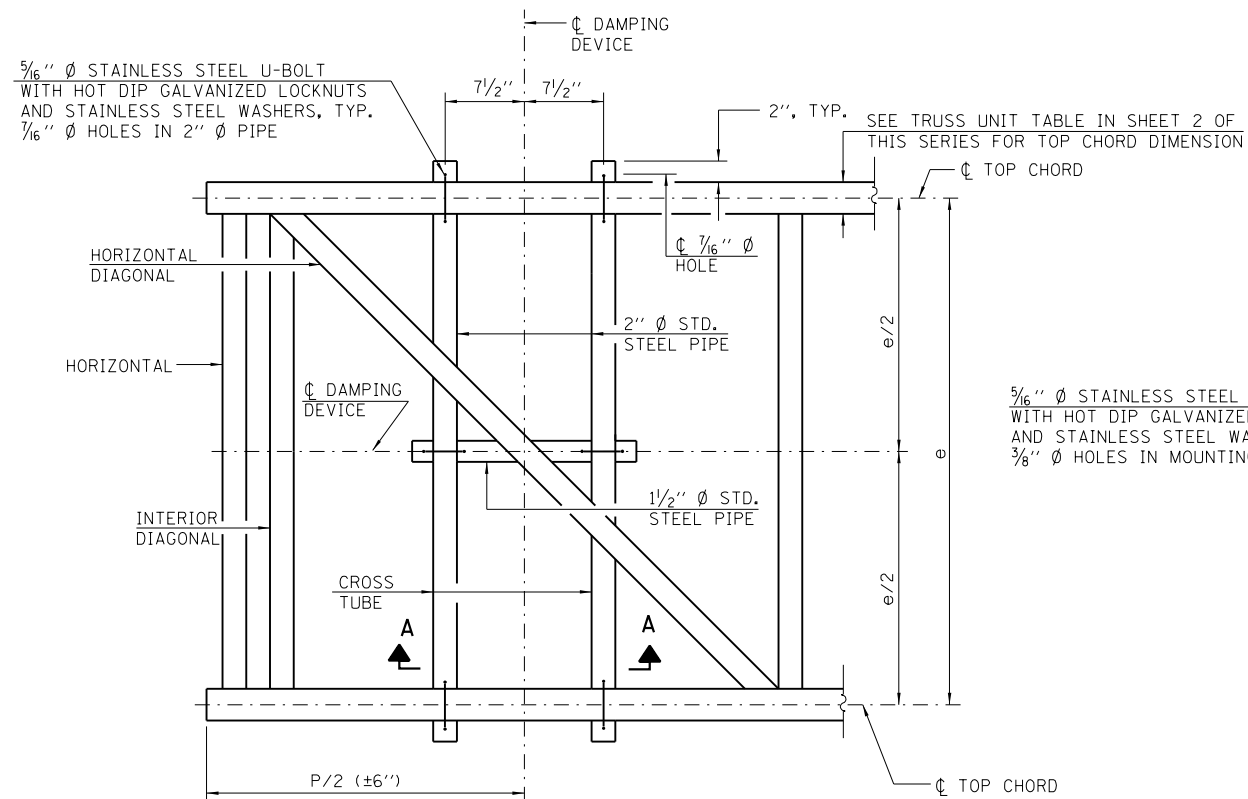
APPROVED.....
DATE 3-31-2014.
CHIEF ENGINEERING OFFICER

SHEET 6 OF 8

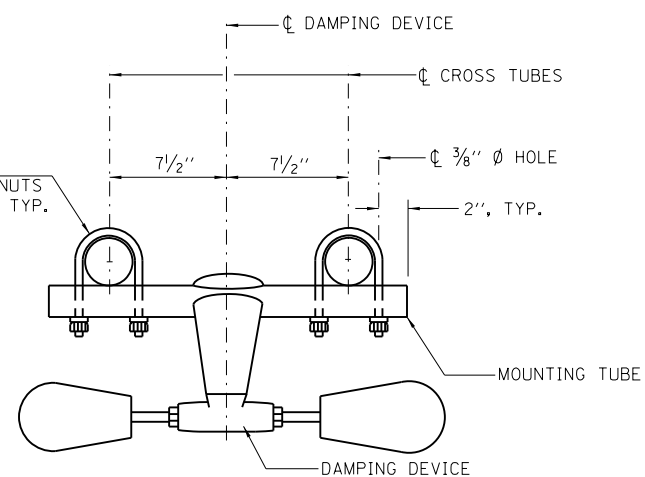


OVERHEAD SIGN STRUCTURE
BUTTERFLY TYPE
STRUCTURE DETAILS

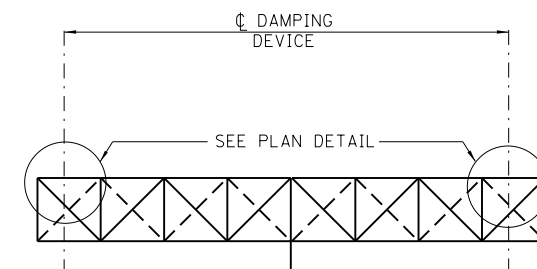
STANDARD F14-05



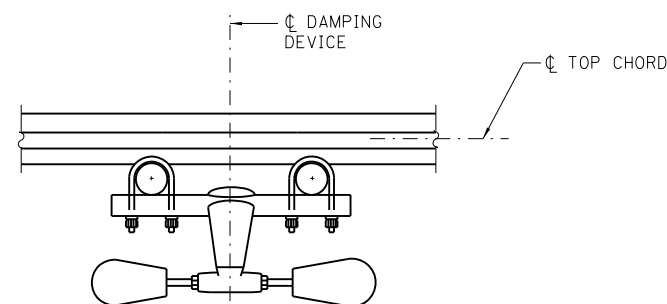
PLAN DETAIL



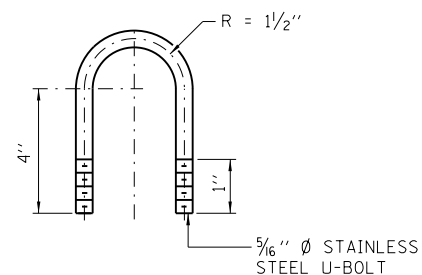
TRUSS DAMPING
DEVICE CONNECTION DETAIL



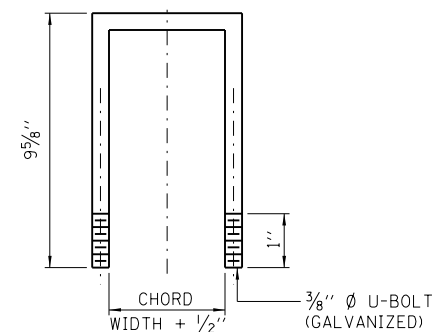
ELEVATION
STEEL BUTTERFLY SIGN STRUCTURE



SECTION A-A



DAMPING DEVICE MOUNTING
TUBE U-BOLT DETAIL
(TYPICAL)



TOP CHORD TO CROSS TUBE
U-BOLT DETAIL
(TYPICAL)

NOTE:

DAMPER: ONE DAMPER PER TRUSS. (31 LBS. STOCKBRIDGE-TYPE
29" MINIMUM BETWEEN ENDS OF WEIGHTS).

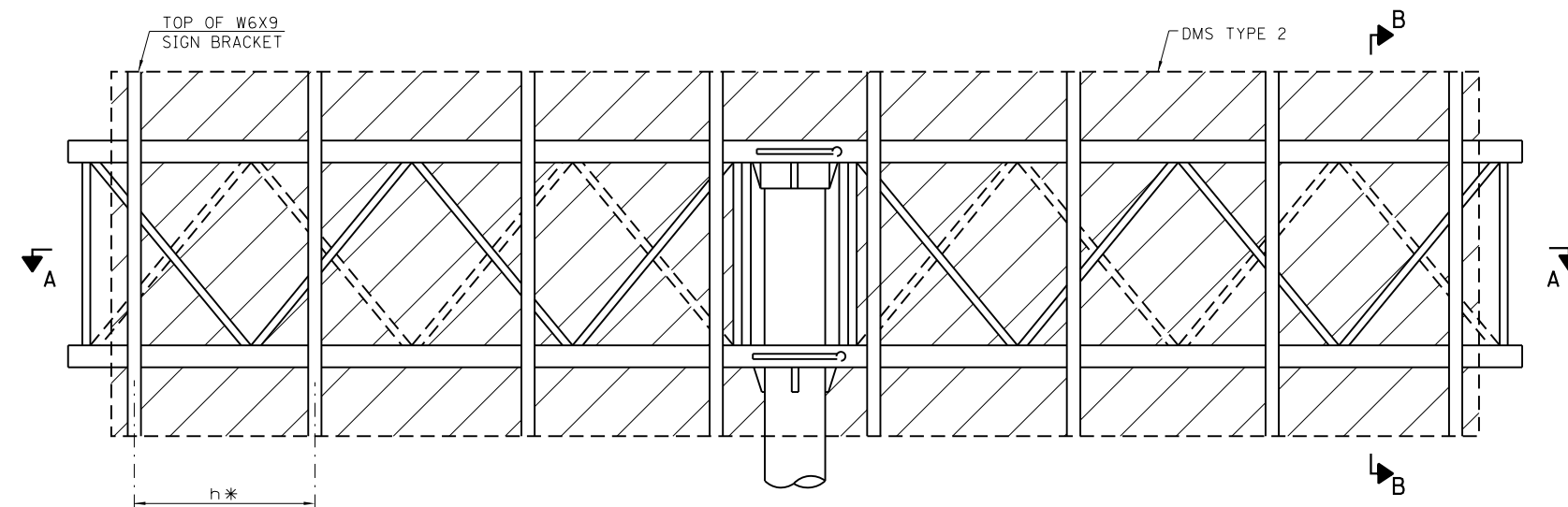
Paul Kovacs
APPROVED, DATE 3-31-2014.
CHIEF ENGINEERING OFFICER

SHEET 7 OF 8



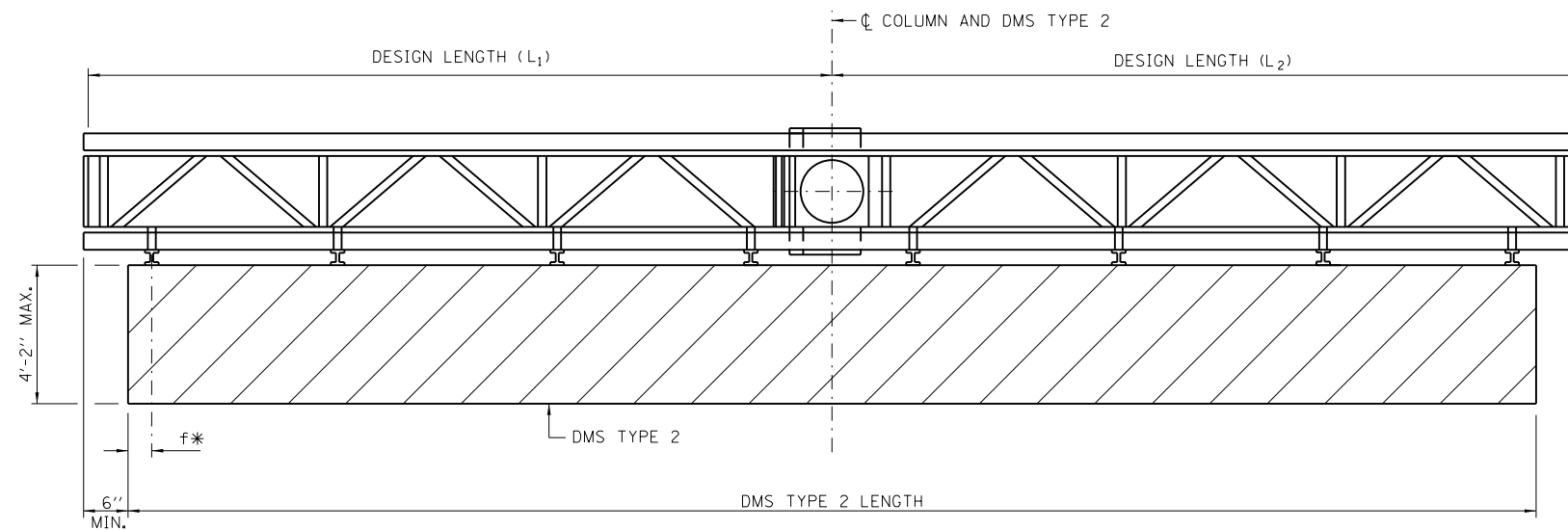
OVERHEAD SIGN STRUCTURE
BUTTERFLY TYPE
STRUCTURE DETAILS

STANDARD F14-05



* BRACKET DIMENSIONS ARE NOMINAL AND WILL VARY BASED ON ACTUAL DMS TYPE 2 DIMENSIONS PLUS MANUFACTURER'S MOUNTING DEVICES.

TYPICAL FRONT ELEVATION



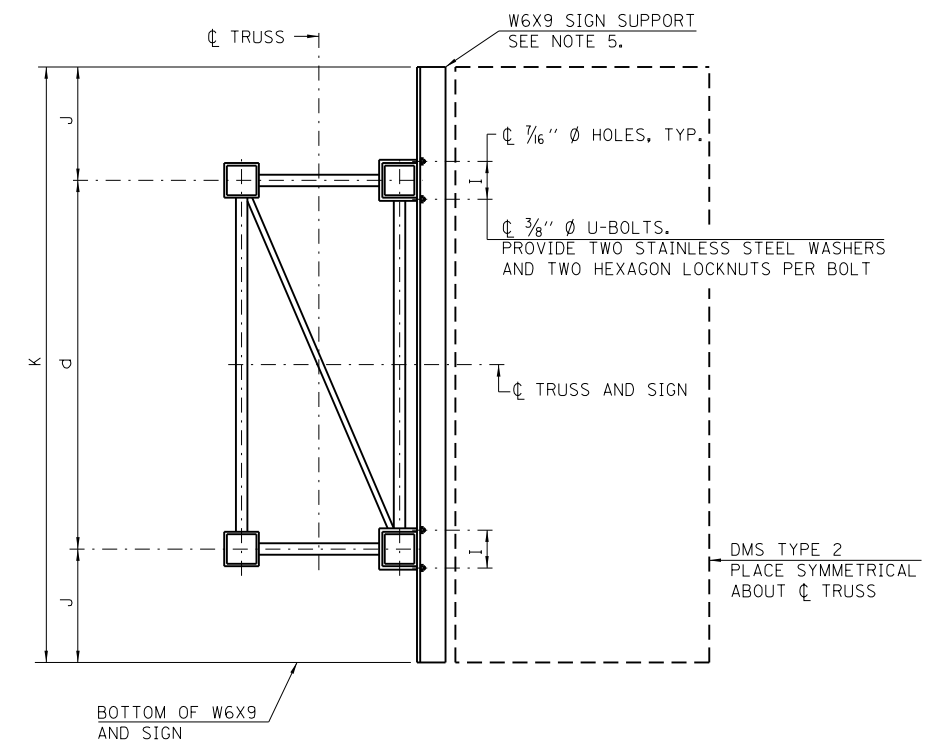
SECTION A-A

PLACE ALL SIGN BRACKETS AS CLOSE TO PANEL POINTS AS PRACTICAL.

(ROAD PLAN BENEATH TRUSS VARIES)
BUTTERFLY MAY BE LOCATED IN SHOULDER AREA.

NOTES:

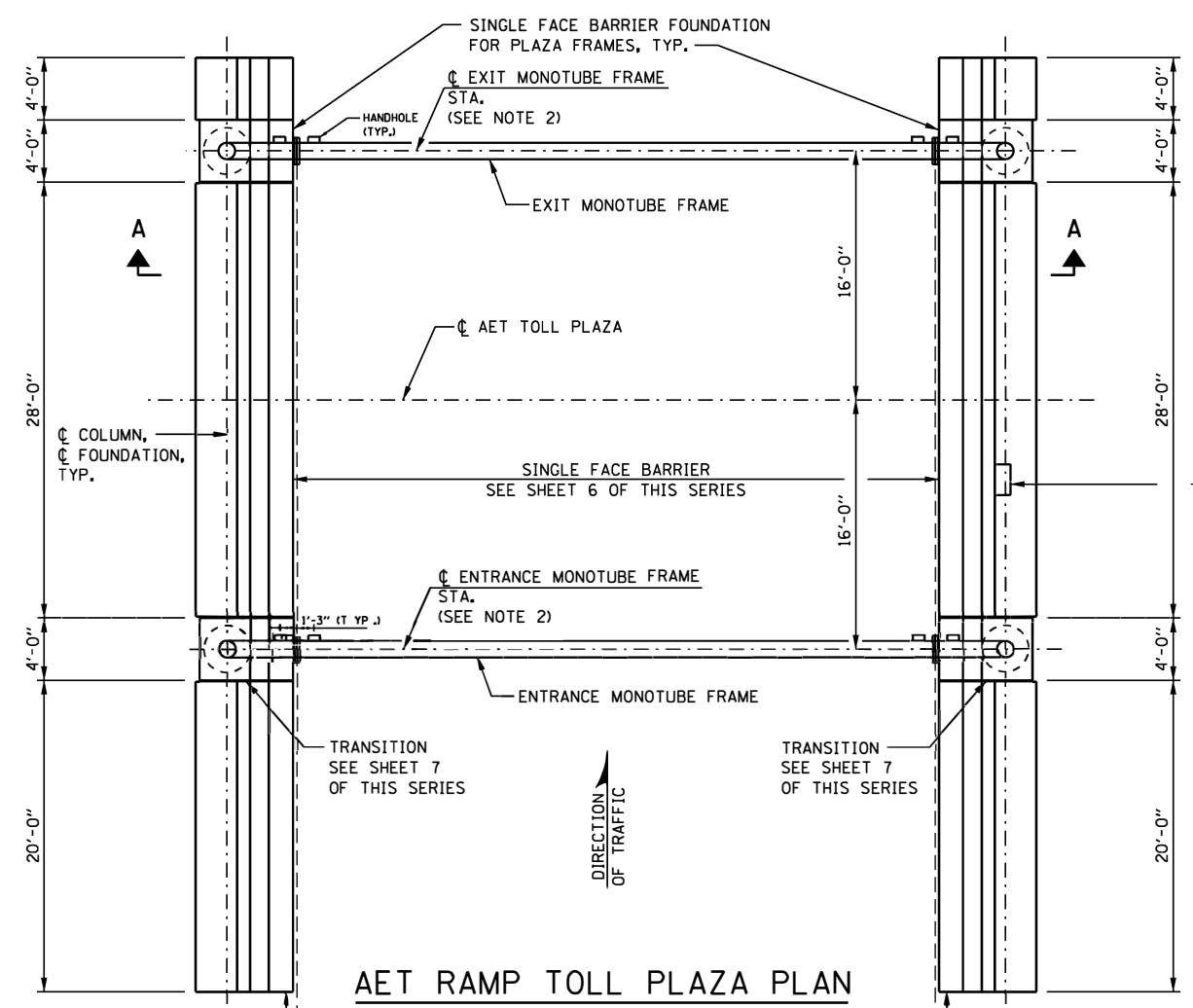
1. SPACE SIGN BRACKETS W6X9 FOR EFFICIENCY AND WITHIN LIMITS SHOWN:
2. $f = 12''$ MAXIMUM, $4''$ MINIMUM (END OF SIGN TO ϕ OF NEAREST BRACKET)
 $h = 6'-0''$ MAXIMUM (ϕ TO ϕ SIGN SUPPORT BRACKETS, W6X9)
3. MAXIMUM DMS TYPE 2 WEIGHT = 5000 LBS.
4. $4'-2''$ MAXIMUM DEPTH INCLUDES DEPTH OF DMS TYPE 2 PLUS CONNECTION TO W6X9.
5. DMS TYPE 2 MANUFACTURER SHALL DESIGN AND SUPPLY HARDWARE FOR CONNECTION TO W6X9. BOLTS SHALL BE STAINLESS STEEL OR HOT DIP GALVANIZED HIGH STRENGTH PER THE STANDARD SPECIFICATION.



SECTION B-B

BRACKET TABLE

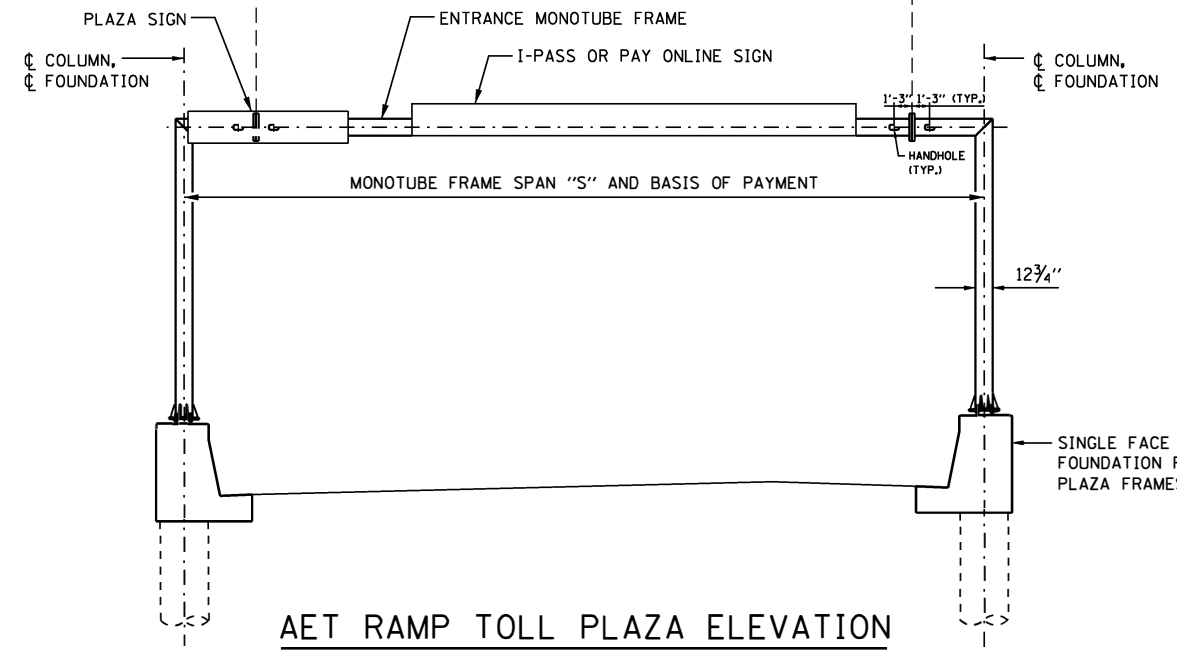
| W6X9 | | |
|--------------|-----------------------|-----------------------------|
| SIGN WIDTH | | NUMBER OF BRACKETS REQUIRED |
| GREATER THAN | LESS THAN OR EQUAL TO | |
| | 8'-0" | 2 |
| 8'-0" | 14'-0" | 3 |
| 14'-0" | 20'-0" | 4 |
| 20'-0" | 26'-0" | 5 |
| 26'-0" | 32'-0" | 6 |



AET RAMP TOLL PLAZA PLAN

PROVIDE ATTENUATOR, BARRIER WALL OR GUARDRAIL TERMINAL AS SHOWN ON PLANS

PROVIDE ATTENUATOR, BARRIER WALL OR GUARDRAIL TERMINAL AS SHOWN ON PLANS



AET RAMP TOLL PLAZA ELEVATION

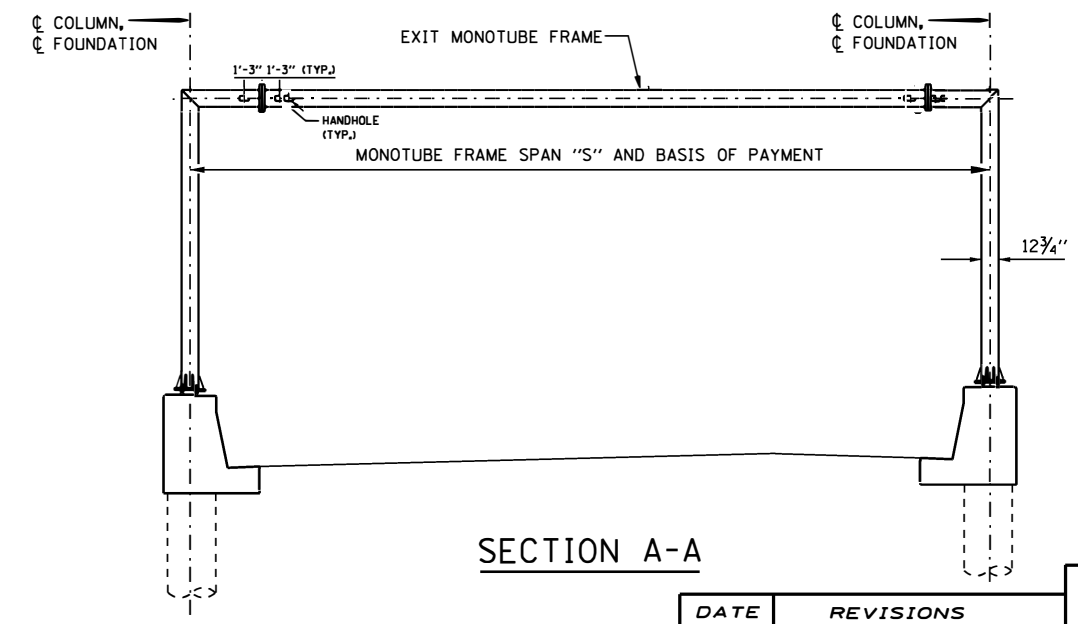
EXTERIOR JUNCTION BOX AS REQUIRED. COORDINATE INSTALLATION WITH ITS DRAWING.

SIGN TABLE

| SIGN | MAXIMUM AREA | MAXIMUM LENGTH |
|---------------------------|--------------|----------------|
| PLAZA SIGN | 24 S.F. | 8'-0" |
| I-PASS OR PAY ONLINE SIGN | 60 S.F. | 20'-0" |

NOTE:

1. SEE CONTRACT PLANS FOR SIGN SIZE AND LOCATION.
2. PROVIDE ENTRANCE AND EXIT MONOTUBE FRAME STATIONS IN CONTRACT PLANS.




SECTION A-A

APPROVED.....

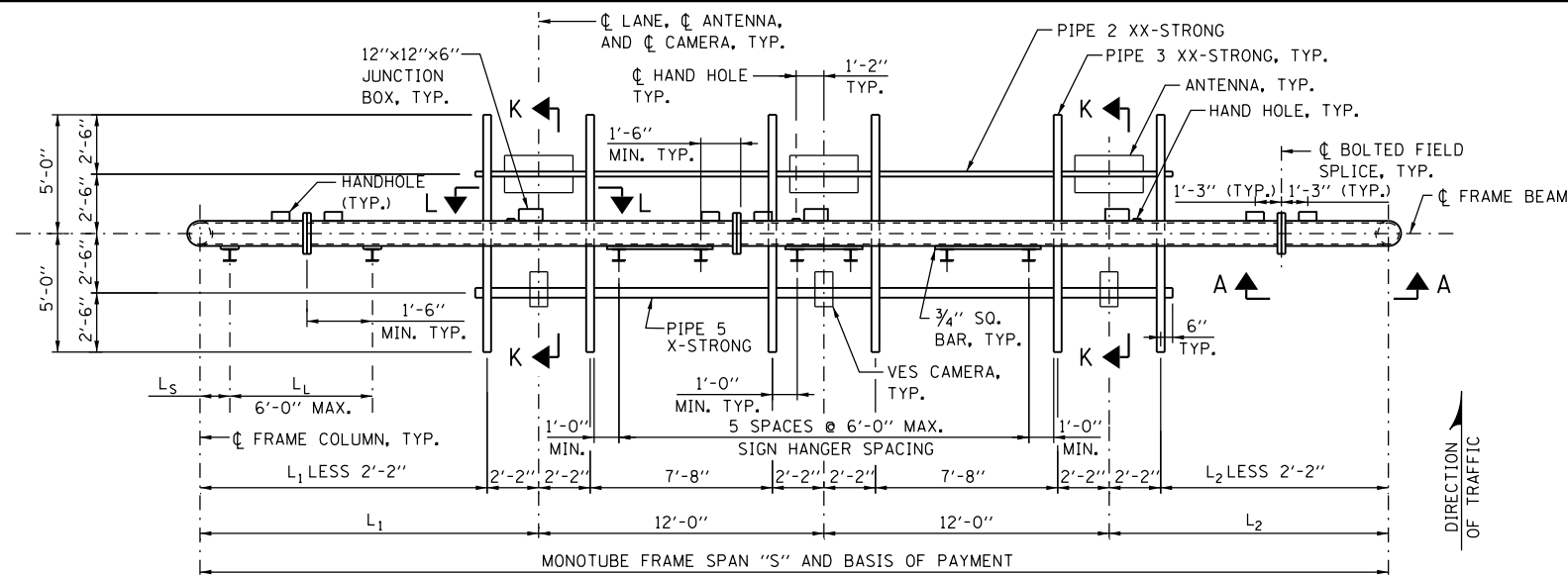
 CHIEF ENGINEER DATE 10-14-2014

| DATE | REVISIONS |
|-----------|--------------------------------------------------------------------|
| 3-31-2016 | REVISED FOUNDATION NOTE |
| 3-31-2017 | REVISED I-PASS ONLY SIGN |
| 3-01-2019 | UPDATED CONSTANT SLOPE BARRIER, REINFORCING DETAILS AND QUANTITIES |
| 5-24-2019 | UPDATED SHOULDER BARRIER DETAILS AND QUANTITIES |
| 2-13-2020 | ADDED HANDHOLE, INSTALLATION & INSPECTION OF SPLICE & ANCHORS |

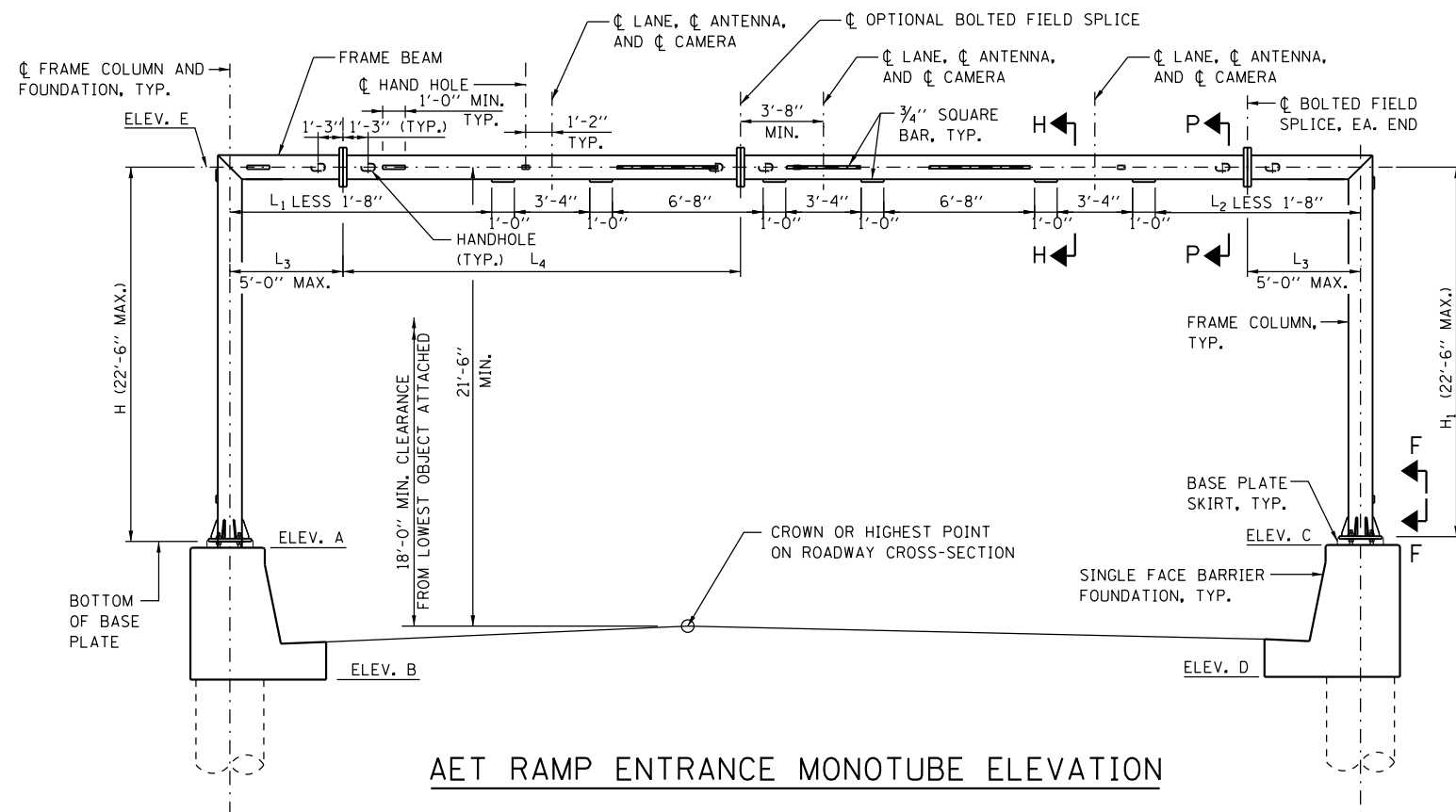


OVERHEAD SIGN STRUCTURE
 MONOTUBE TYPE (STEEL)
 STRUCTURE DETAILS
 FOR AET RAMP

STANDARD F15-05



AET RAMP ENTRANCE MONOTUBE PLAN



AET RAMP ENTRANCE MONOTUBE ELEVATION

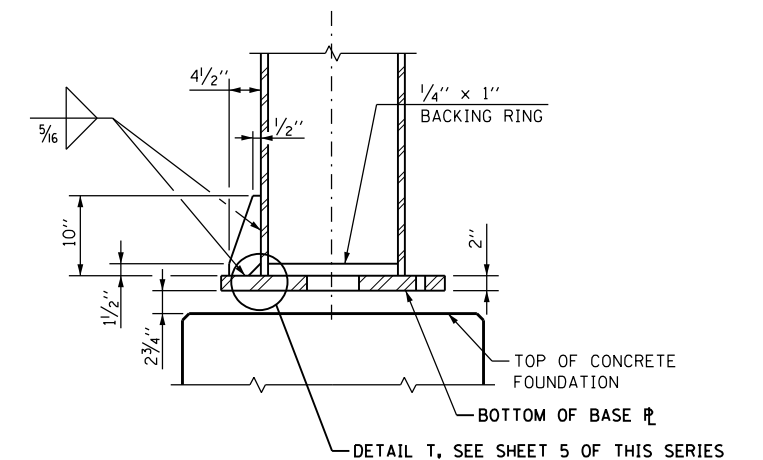
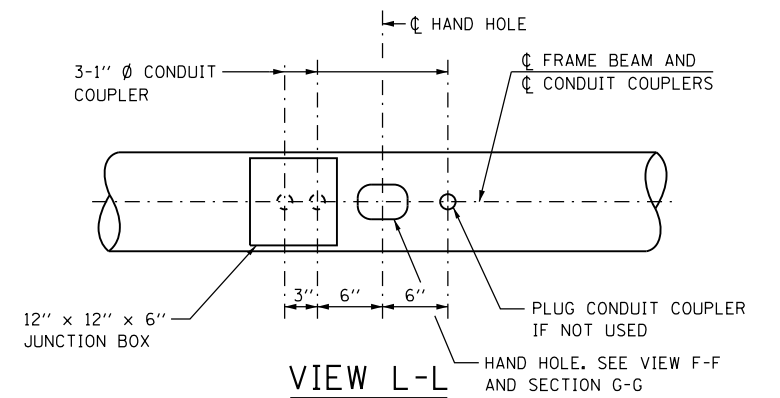
NOTES:

1. FOUNDATIONS FOR MONOTUBE FRAMES ARE SHOWN ON SHEET 6 OF THIS SERIES.
2. SEE SHEET 5 OF THIS SERIES FOR SECTIONS A-A, G-G, H-H, K-K, VIEW F-F AND BASE PLATE SKIRT.
3. SEE SHEET 4 OF THIS SERIES FOR SECTION P-P.
4. PROVIDE CAMBER AT MIDSPAN OF STRUCTURE.
5. LOCATE OPTIONAL BOLTED FIELD SPLICE NEAR MIDSPAN.
6. WORK THIS SHEET WITH, OVERHEAD SIGN STRUCTURE ENTRANCE MONOTUBE TYPE (STEEL) AET RAMP SUMMARY AND TOTAL BILL OF MATERIAL SHEET.

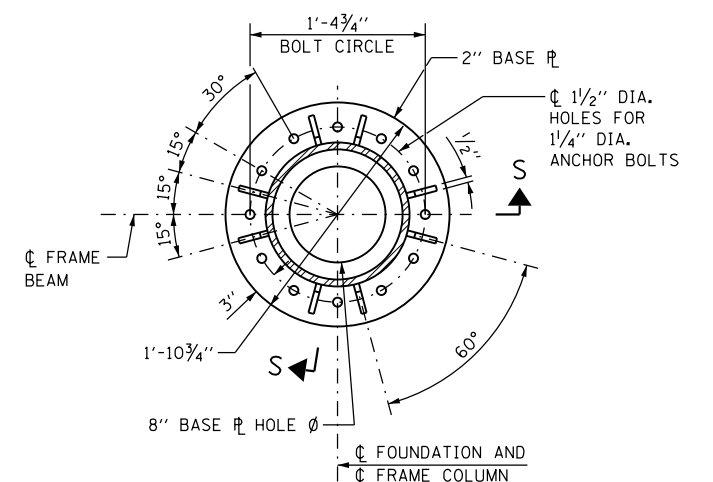
ENTRANCE MONOTUBE FRAME TABLE

| SPAN "S" | FRAME COLUMN | FRAME BEAM | CAMBER |
|-------------|-----------------|-----------------|--------|
| 50' MAX. | HSS 12.75x0.500 | HSS 12.75x0.500 | 1 3/4" |

SEE ILLINOIS TOLLWAY STANDARD DRAWING
F13 FOR SPANS GREATER THAN 50'.



SECTION S-S

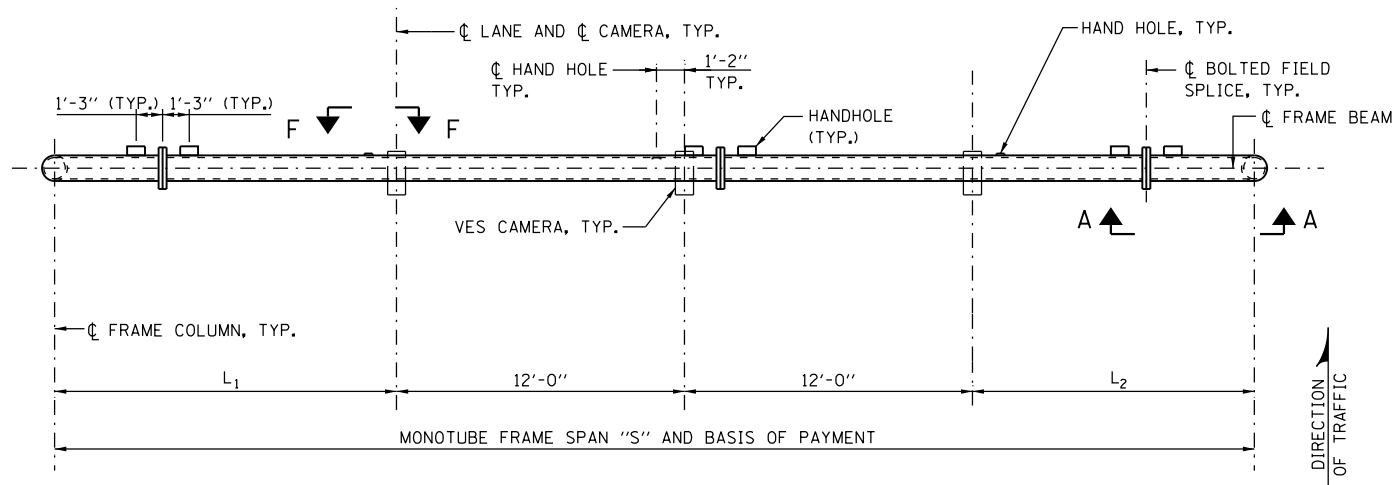


BASE PLATE PLAN
ENTRANCE AND EXIT MONOTUBE

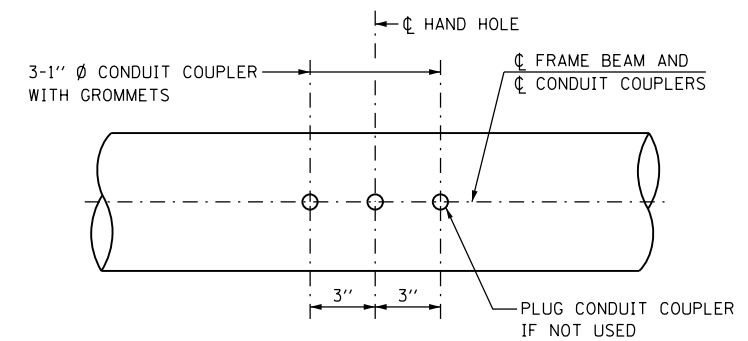


OVERHEAD SIGN STRUCTURE
MONOTUBE TYPE (STEEL)
STRUCTURE DETAILS
FOR AET RAMP

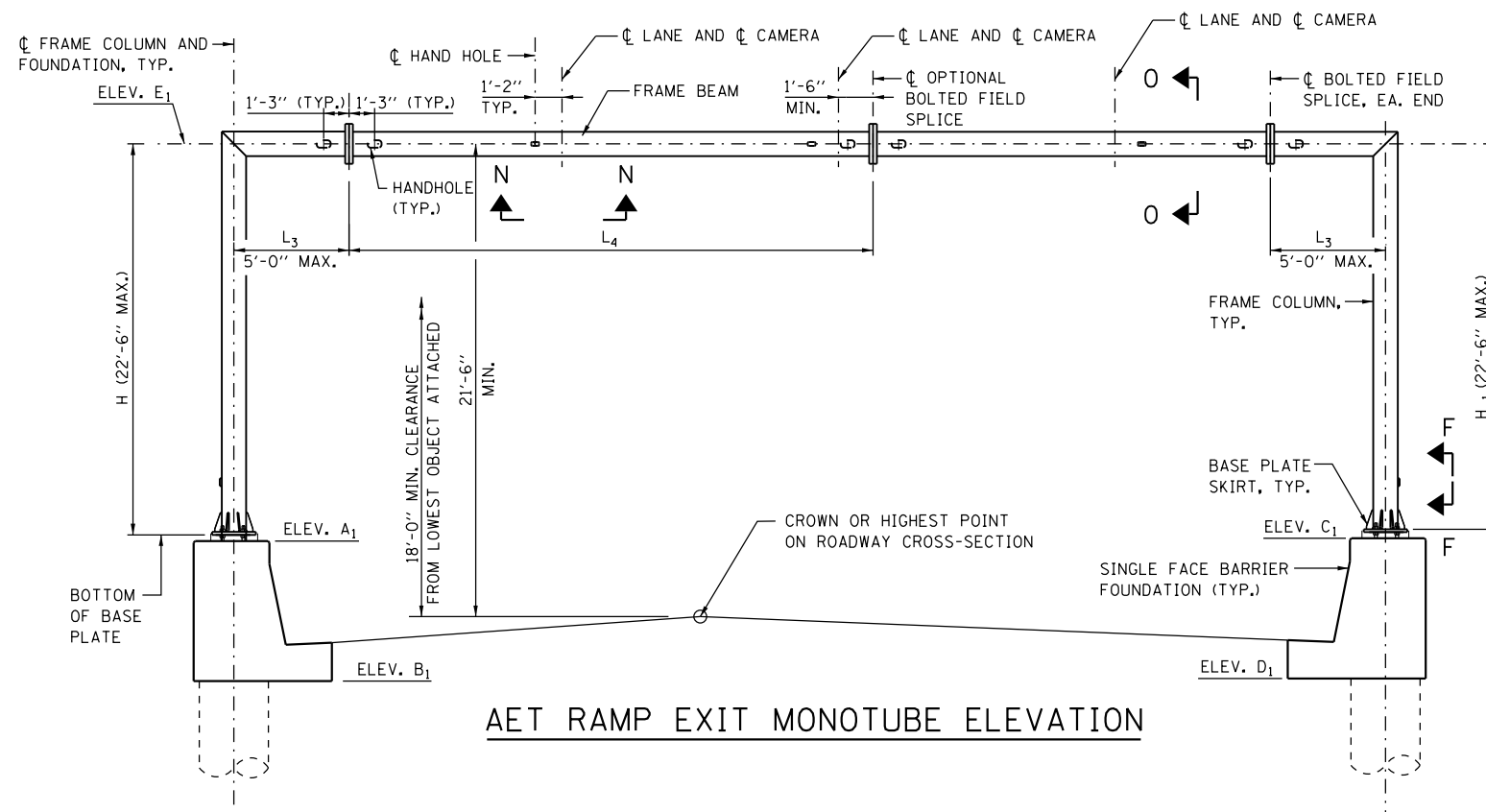
STANDARD F15-05



AET RAMP EXIT MONOTUBE PLAN



VIEW N-N (CONDUIT COUPLER DETAIL)



AET RAMP EXIT MONOTUBE ELEVATION

EXIT MONOTUBE FRAME TABLE

| SPAN "S" | FRAME COLUMN | FRAME BEAM | CAMBER |
|----------|-----------------|-----------------|--------|
| 50' MAX. | HSS 12.75x0.500 | HSS 12.75x0.500 | 1 3/4" |

SEE STANDARD F13 FOR SPANS GREATER THAN 50'.

NOTES:

1. SEE SHEET 2 OF THIS SERIES FOR SECTION S-S, BASE PL PLAN AND ADDITIONAL NOTES.
2. SEE SHEET 4 OF THIS SERIES FOR SECTION O-O.
3. SEE SHEET 5 OF THIS SERIES FOR SECTIONS A-A AND G-G, AND BASE PLATE SKIRT.
4. WORK THIS SHEET WITH, OVERHEAD SIGN STRUCTURE EXIT MONOTUBE TYPE (STEEL) AET RAMP SUMMARY AND TOTAL BILL OF MATERIAL SHEET.

APPROVED.....
Paul Kovacs
 CHIEF ENGINEER
 DATE 10-14-2014



GENERAL NOTES:

- 1. AFTER ADJUSTMENTS TO LEVEL FRAME BEAM AND ENSURE ADEQUATE VERTICAL CLEARANCE, TIGHTEN ALL TOP AND LEVELING NUTS AGAINST THE BASE PLATE WITH A MINIMUM TORQUE OF 200 LB.-FT. THEN PLACE STAINLESS STEEL MESH AROUND THE PERIMETER OF THE BASE PLATE. SECURE TO BASE PLATE WITH STAINLESS STEEL BANDING.
- 2. REINFORCEMENT BARS DESIGNATED "(E)" SHALL BE EPOXY COATED.

STRUCTURAL STEEL:

- 1. MATERIAL FOR THE MONOTUBE FRAME SHALL CONFORM TO THE REQUIREMENTS OF ASTM A500 GRADE B. BASE PLATE AND STIFFENER PLATE SHALL CONFORM TO ASTM A709 GRADE 50. OTHER STRUCTURAL STEEL SHAPES AND PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A36, UNLESS NOTED OTHERWISE.
- 2. PIPES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A53 GRADE B.
- 3. ANCHOR BOLTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM F1554 (AASHTO M314) GRADE 55, WITH A MINIMUM TENSILE STRENGTH OF 75,000 PSI. INSTALLATION AND INSPECTION OF ANCHOR BOLTS SHALL COMPLY WITH ILLINOIS TOLLWAY SPECIAL PROVISION "INTELLIGENT TRANSPORTATION SYSTEMS GANTRY FRAME "STEEL". ANCHORS SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM A153 (AASHTO M232). SEE SHEET 6 OF THIS SERIES FOR GALVANIZED LENGTH.
- 4. U-BOLTS SHALL BE STAINLESS STEEL. PROVIDE STAINLESS STEEL WASHERS AND NUTS FOR U-BOLTS.
- 5. BOLTS (EXCLUDING ANCHOR BOLTS AND U-BOLTS) SHALL BE HIGH STRENGTH STEEL BOLTS.
- 6. TUBES FOR MONOTUBE FRAME, PIPES, STRUCTURAL STEEL SHAPES AND PLATES SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM A123 AFTER FABRICATION.
- 7. THE MONOTUBE FRAME BEAM, COLUMNS, BASE PLATE MATERIAL, AND SPLICES ARE CONSIDERED TENSION MEMBERS AND SHALL CONFORM TO THE IMPACT TESTING REQUIREMENT, ZONE 2.

DESIGN LOADING:

| | |
|--------------------|-----------|
| WIND LOAD CRITERIA | |
| SIGN PANEL | 35 P.S.F. |
| COLUMN/BEAM | 35 P.S.F. |

EQUIPMENT LOADS:

| | |
|-----------------|--------|
| CAMERA ASSEMBLY | 8 LB. |
| ANTENNA | 20 LB. |

DESIGN STRESSES FOR REINFORCED CONCRETE:

| | |
|------------------------------------------------------|-----------------|
| f'c = COMPRESSIVE STRENGTH OF CONCRETE (CLASS SI) | = 3,500 P.S.I. |
| f'c = COMPRESSIVE STRENGTH OF CONCRETE (CLASS DS) | = 4,000 P.S.I. |
| fy = YIELD STRENGTH OF REINFORCEMENT BARS (GRADE 60) | = 60,000 P.S.I. |

FOUNDATION:

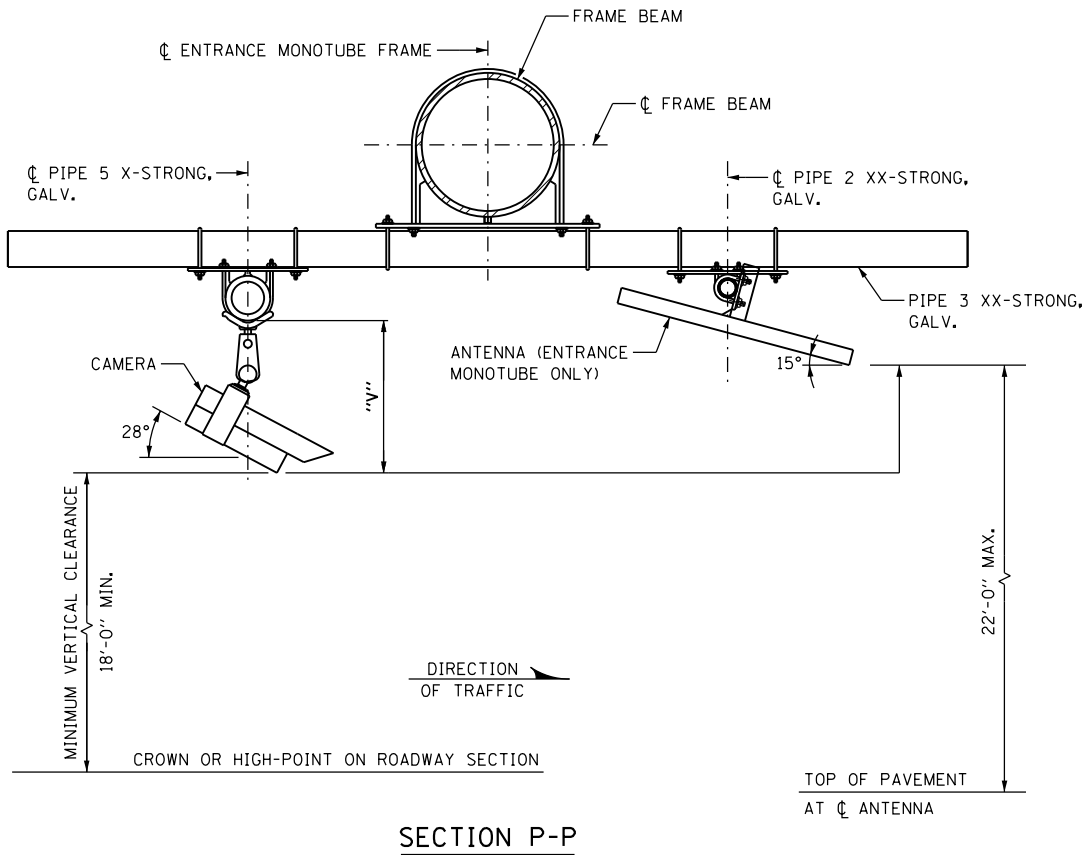
MINIMUM UNCONFINED COMPRESSIVE STRENGTH, Qu FOR ALL LAYERS OF COHESIVE SOILS (CLAYS) SHALL BE 1.25 TON/SQ.FT. AT MONOTUBE FRAMES.

DESIGN SPECIFICATIONS:

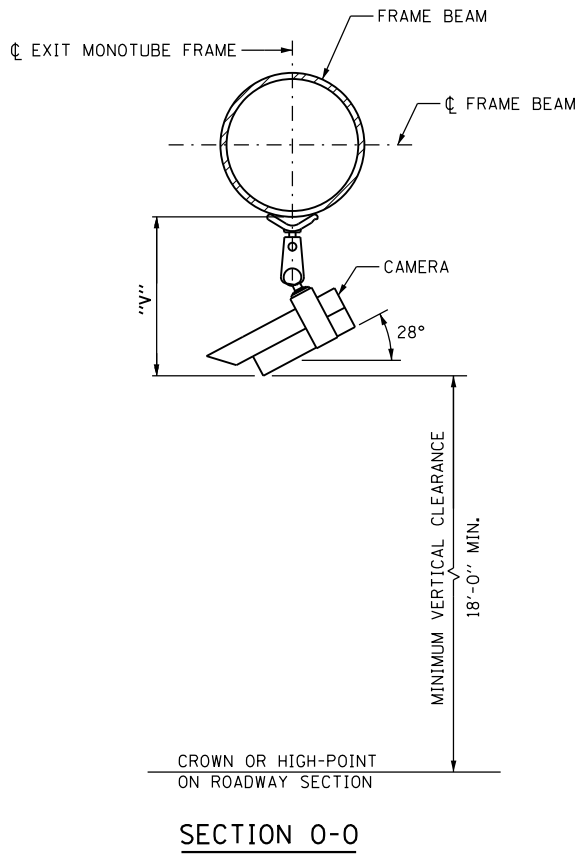
- 1. ILLINOIS TOLLWAY STRUCTURE DESIGN MANUAL, LATEST EDITION.
- 2. AASHTO STANDARD SPECIFICATION FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 6TH EDITION.
- 3. AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 6TH EDITION DATED FEBRUARY 2012.
- 4. ILLINOIS DEPARTMENT OF TRANSPORTATION BRIDGE MANUAL, JANUARY 2012

CONSTRUCTION SPECIFICATIONS:

- 1. ILLINOIS TOLLWAY SUPPLEMENTAL SPECIFICATIONS TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.
- 2. ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.

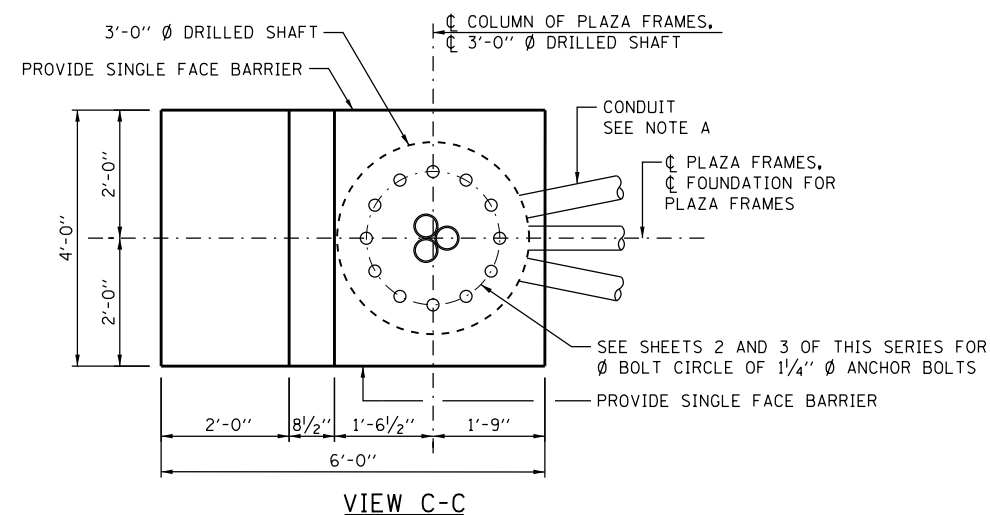
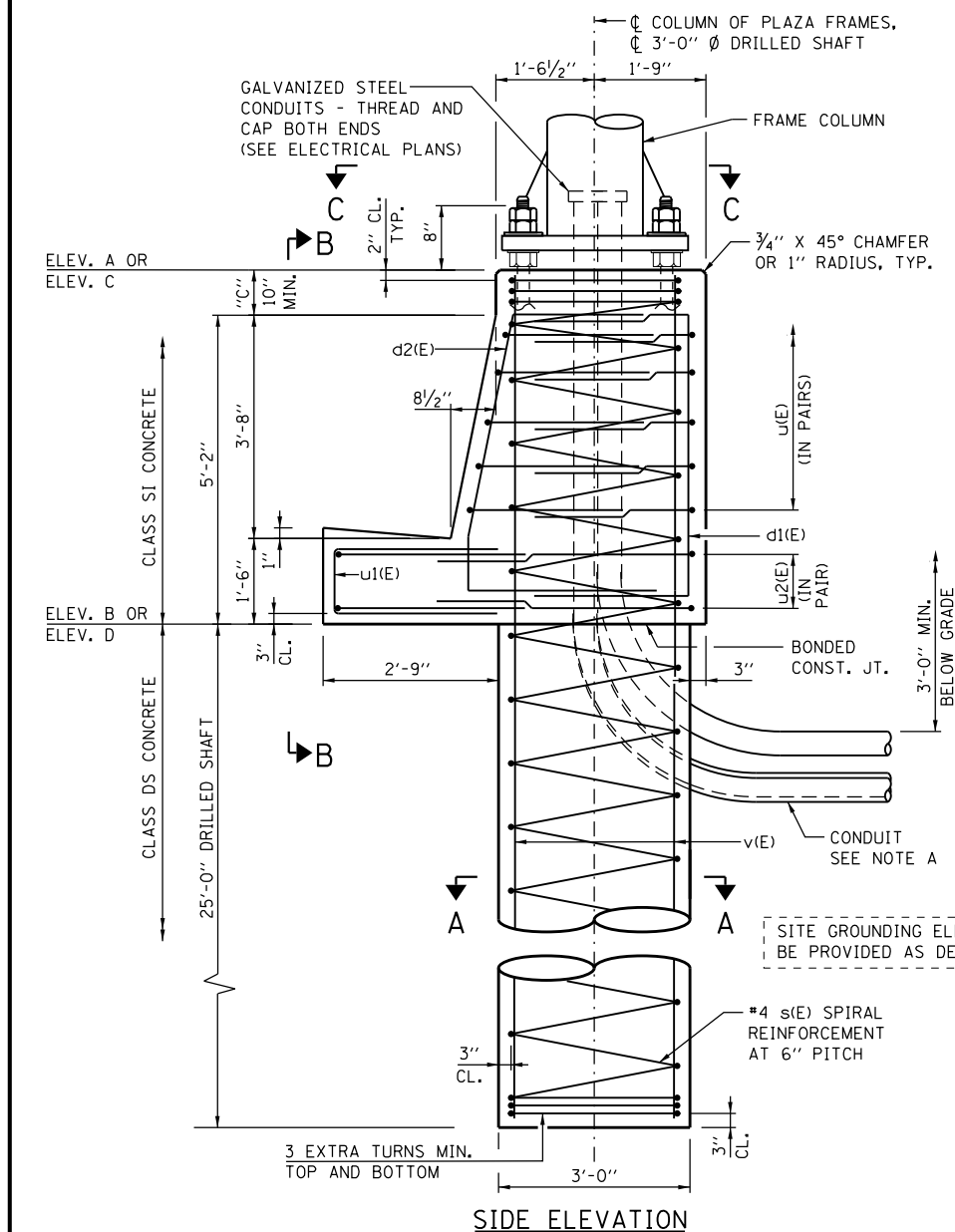


NOTE:
VERIFY DIMENSION "V" WITH
CAMERA MANUFACTURER.

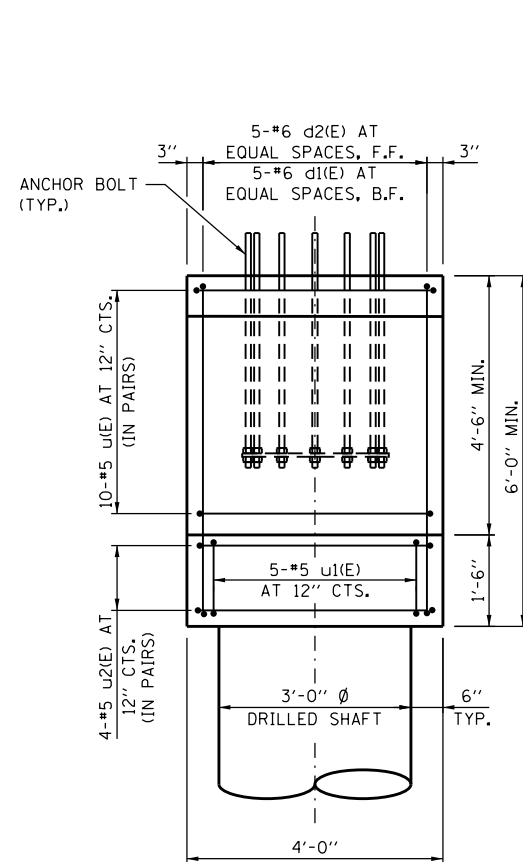


APPROVED.....
CHIEF ENGINEER

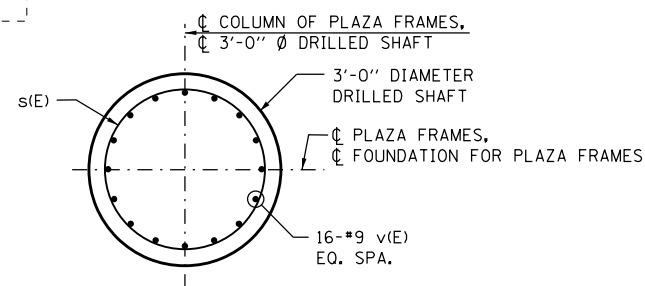
DATE 10-14-2014



SINGLE FACE BARRIER FOUNDATION FOR PLAZA FRAMES



VIEW B-B



SECTION A-A

NOTE A:

1. COORDINATE CONDUIT SIZE, LOCATION AND QUANTITY WITH ELECTRICAL PLANS. PROVIDE CONDUIT COUPLERS AS REQUIRED.
2. CONDUITS SHALL BE PLACED TO MISS REINFORCEMENT. CUTTING OF REINFORCEMENT SHALL NOT BE ALLOWED.

NOTE B:

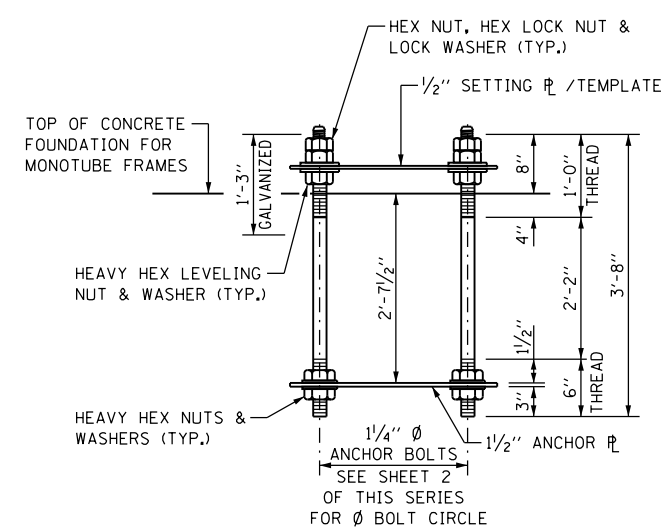
PROTECTIVE COAT SHALL BE APPLIED TO THE TRAFFIC AND TOP FACES OF THE BARRIER AND TOP OF GUTTER

FOUNDATION NOTE:

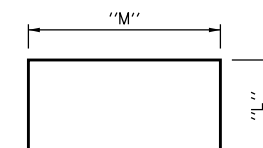
THE FOUNDATION DETAILS SHOWN ARE BASED ON THE PRESENCE OF MOSTLY COMMON COHESIVE SOIL CONDITIONS (SILTY OR SANDY CLAY), WITH AN AVERAGE UNCONFINED COMPRESSIVE STRENGTH (QU) > 1.25 TON/SQ. FT. WHICH MUST BE DETERMINED BY PREVIOUS SOIL INVESTIGATIONS AT THE JOBSITE. WHEN OTHER CONDITIONS ARE INDICATED, THE BORING DATA SHALL BE INCLUDED IN THE PLANS AND THE FOUNDATION DIMENSIONS SHOWN SHALL BE THE RESULT OF SITE SPECIFIC DESIGNS. IF CONDITIONS ENCOUNTERED IN THE FIELD ARE DIFFERENT THAN THOSE INDICATED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER TO DETERMINE IF THE FOUNDATION DIMENSIONS NEED TO BE MODIFIED.

LEGEND:

F.F. - FRONT FACE
B.F. - BACK FACE
CTS. - CENTERS

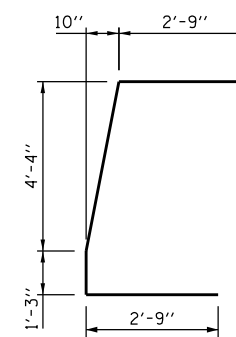


ANCHOR BOLT ASSEMBLY

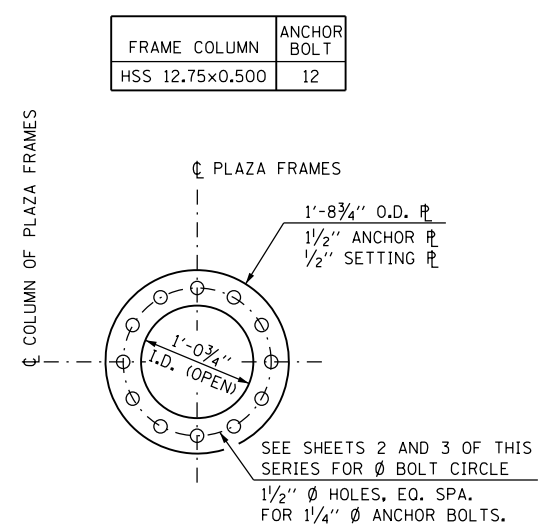


| BAR | "L" | "M" |
|-------|--------|-------|
| d1(E) | 2'-9" | 5'-7" |
| u(E) | 2'-9" | 3'-8" |
| u1(E) | 3'-3" | 1'-1" |
| u2(E) | 3'-10" | 3'-8" |

BARS d1(E), u(E),
u1(E) AND u2(E)



BAR d2(E)



ANCHOR PL / SETTING PL

BAR LIST-ONE FOUNDATION

| BAR | NO. | SIZE | LENGTH | SHAPE |
|-------|-----|------|--------|-------|
| d1(E) | 5 | #6 | 11'-1" | |
| d2(E) | 5 | #6 | 11'-2" | |
| s(E) | 1 | #4 | 30'-7" | |
| v(E) | 16 | #9 | 30'-7" | |
| u(E) | 10 | #5 | 9'-2" | |
| u1(E) | 5 | #5 | 7'-7" | |
| u2(E) | 4 | #5 | 11'-4" | |

* THE LENGTH OF SPIRAL SHOWN IS THE HEIGHT OF SPIRAL, COMPUTED USING "C" = 10". ADJUST LENGTH ACCORDINGLY IF "C" IS GREATER THAN 10".

** BAR LENGTH IS COMPUTED USING "C" = 10". ADJUST BAR LENGTH ACCORDINGLY IF "C" IS GREATER THAN 10".

ESTIMATED QUANTITY

| ITEM | UNIT | SINGLE FACE BARRIER FDN. |
|--------------------------------|---------|--------------------------|
| CLASS SI CONCRETE | CU. YD. | 3.8 |
| CLASS DS CONCRETE | CU. YD. | 6.6 |
| REINFORCEMENT BARS, EPOXY COAT | POUND | 2,360 |
| PROTECTIVE COAT | SQ. YD. | 4.4 |

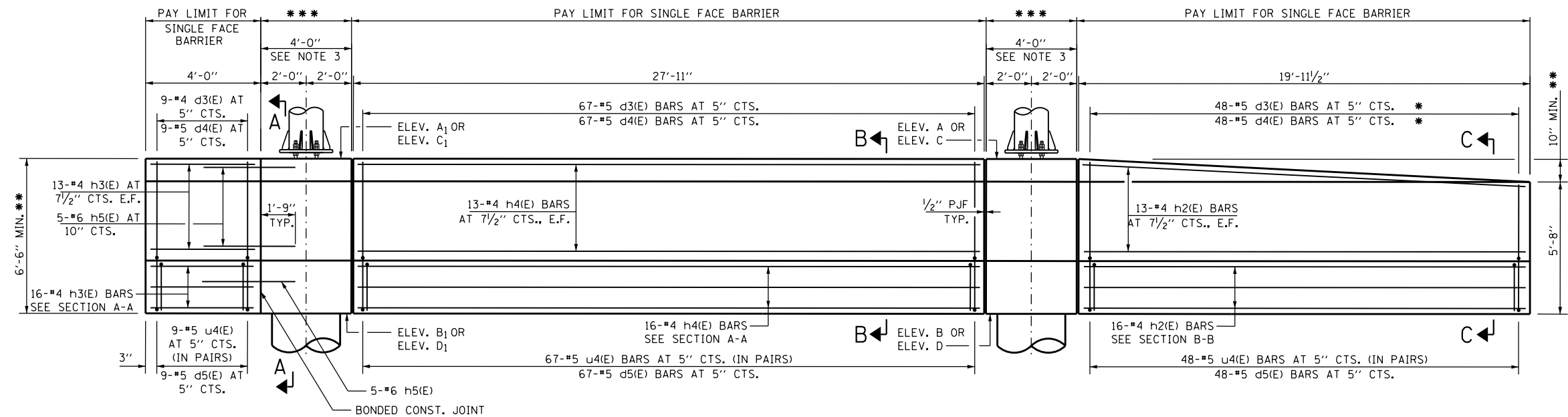
NOTE:

QUANTITIES FOR SINGLE FACE BARRIER FOUNDATION ARE DETERMINED USING "C" = 10". IF DIMENSION "C" IS GREATER THAN 10", ADJUST QUANTITIES ACCORDINGLY.



OVERHEAD SIGN STRUCTURE
MONOTUBE TYPE (STEEL)
STRUCTURE DETAILS
FOR AET RAMP

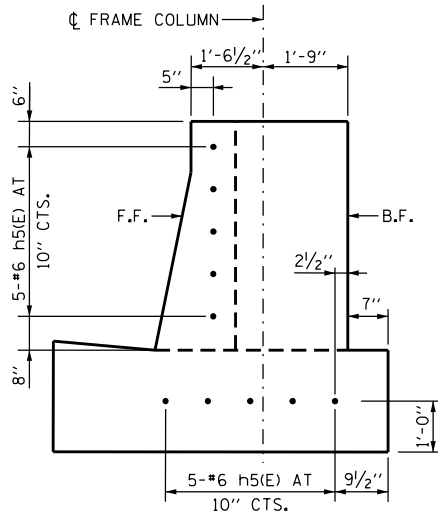
STANDARD F15-05



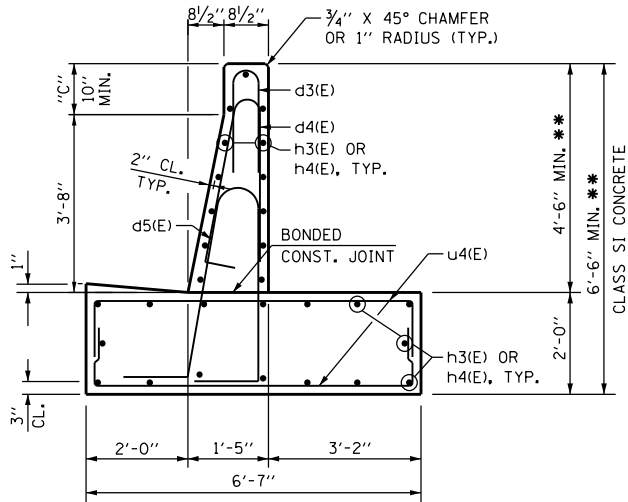
SINGLE FACE BARRIER AND BARRIER BASE ELEVATION

INSIDE FACE OF RIGHT BARRIER IS SHOWN
(MIRROR ELEVATION OF LEFT BARRIER)

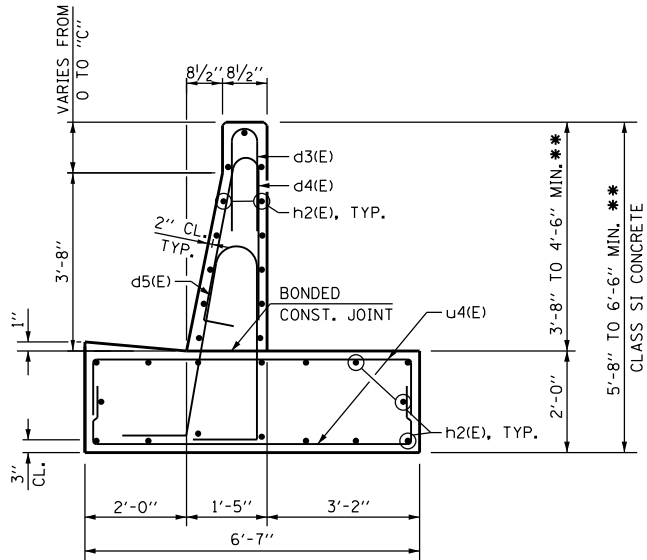
- * CUT IN FIELD AS REQUIRED TO FIT TAPER
- ** BASED ON DIMENSION "C" = 10"
- *** PAY LIMIT FOR FOUNDATION FOR OVERHEAD SIGN STRUCTURE



SECTION A-A



SECTION B-B



SECTION C-C

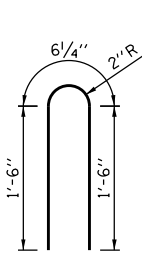
ESTIMATED QUANTITY

(FOR ONE SINGLE FACE BARRIER)

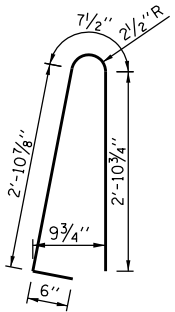
| ITEM | UNIT | TOTAL |
|----------------------------------|---------|-------|
| CONCRETE STRUCTURES | CU. YD. | 33.9 |
| REINFORCEMENT BARS, EPOXY COATED | POUND | 5,910 |
| PROTECTIVE COAT | SQ. YD. | 41.1 |

BAR LIST - FOR ONE BARRIER

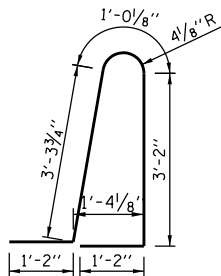
| BAR | NO. | SIZE | LENGTH | SHAPE |
|-------|-----|------|--------|-------|
| d3(E) | 124 | #4 | 3'-7" | U |
| d4(E) | 124 | #5 | 7'-0" | U |
| d5(E) | 124 | #5 | 9'-10" | U |
| h2(E) | 29 | #4 | 19'-7" | I |
| h3(E) | 29 | #4 | 3'-8" | I |
| h4(E) | 29 | #4 | 27'-7" | I |
| h5(E) | 10 | #6 | 3'-9" | I |
| u4(E) | 248 | #5 | 9'-3" | U |



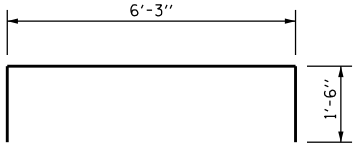
BAR d3(E)



BAR d4(E)



BAR d5(E)



BAR u4(E)

NOTES:

- PROTECTIVE COAT SHALL BE APPLIED TO THE TRAFFIC AND TOP FACES OF THE BARRIER, GUTTER AND TO THE ENTRANCE SIDE FACE (AT THE BEGINNING OF THE RAMP PLAZA PAVEMENT) FOR THE FULL HEIGHT OF THE BARRIER.
- ELECTRICAL JUNCTION BOXES SHALL BE EXTERIOR MOUNTED ON THE BACK FACE OF BARRIER.
- FOR SINGLE FACE BARRIER FOUNDATION DETAILS FOR MONOTUBE FRAMES, SEE SHEET 6 OF THIS SERIES.
- QUANTITIES FOR SINGLE FACE BARRIER ARE DETERMINED USING "C" = 10". IF DIMENSION "C" IS GREATER THAN 10", ADJUST QUANTITIES ACCORDINGLY.
- SEE OVERHEAD SIGN STRUCTURE ENTRANCE MONOTUBE TYPE (STEEL) AET RAMP SUMMARY AND TOTAL BILL OF MATERIAL IN CONTACT PLANS FOR COMPLETE BILL OF MATERIAL.



OVERHEAD SIGN STRUCTURE
MONOTUBE TYPE (STEEL)
STRUCTURE DETAILS
FOR AET RAMP

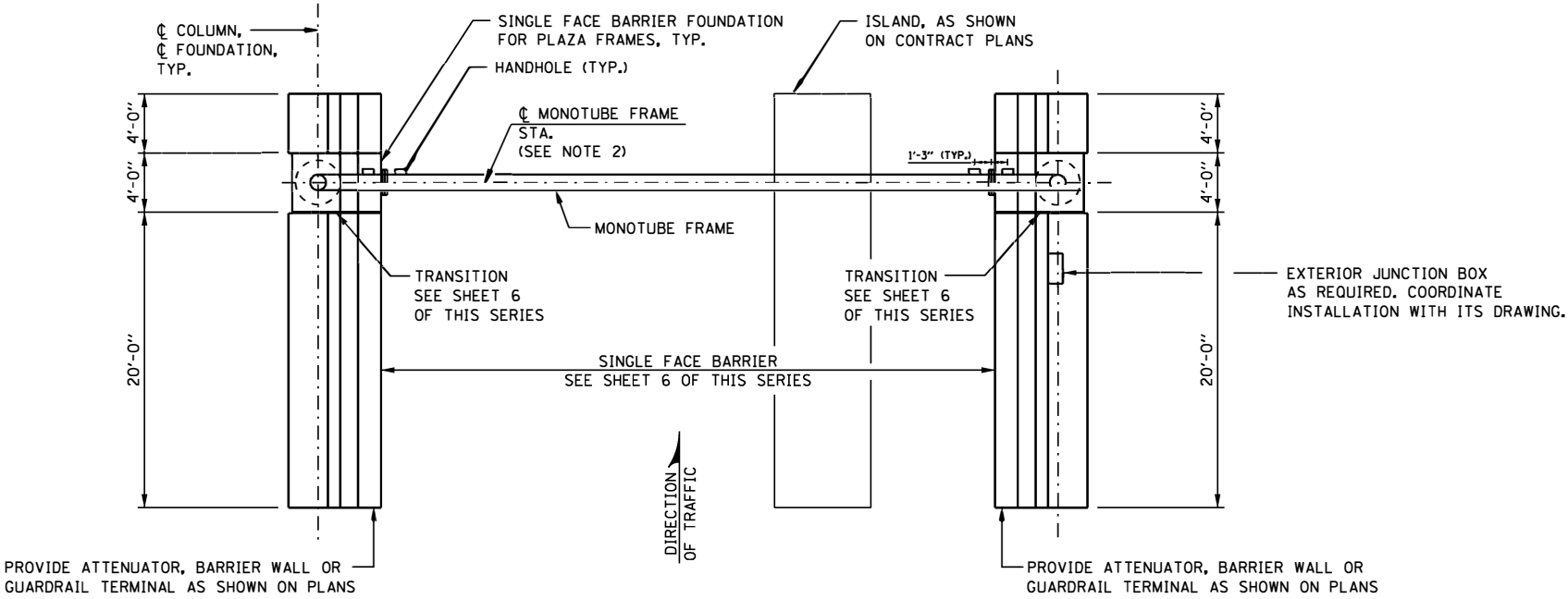
STANDARD F15-05

SIGN TABLE

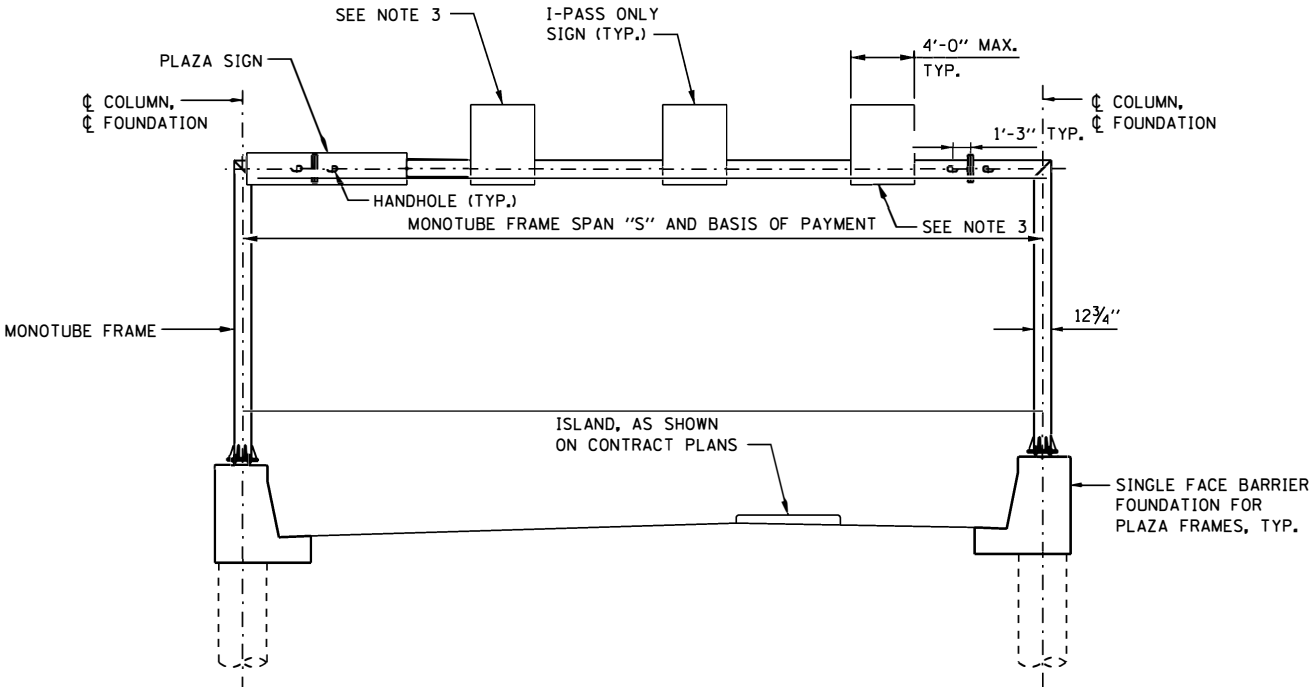
| SIGN | MAXIMUM AREA | MAXIMUM LENGTH |
|------------------|--------------|----------------|
| PLAZA SIGN | 24 S.F. | 8'-0" |
| I-PASS ONLY SIGN | 20 S.F. | 4'-0" |
| CASH ONLY SIGN | 20 S.F. | 4'-0" |

NOTE:

1. SEE CONTRACT PLANS FOR SIGN SIZE AND LOCATION.
2. PROVIDE MONOTUBE FRAME STATION IN CONTRACT PLANS.
3. CASH ONLY SIGN OR I-PASS ONLY SIGN. SEE CONTRACT PLANS FOR SIGN PLACEMENT.



CASH-IPO RAMP TOLL PLAZA PLAN



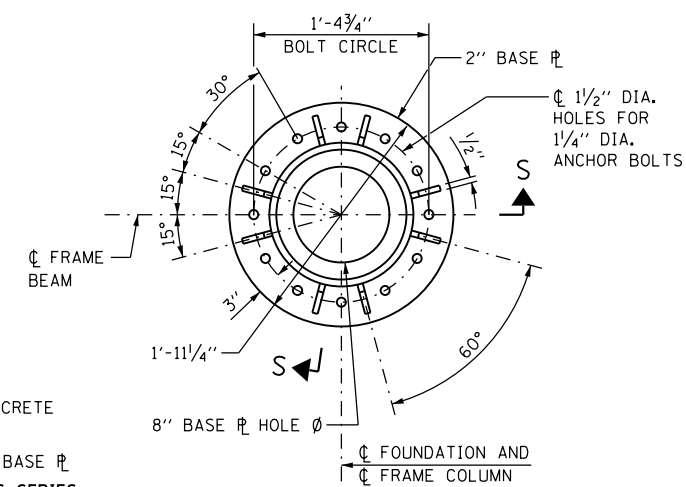
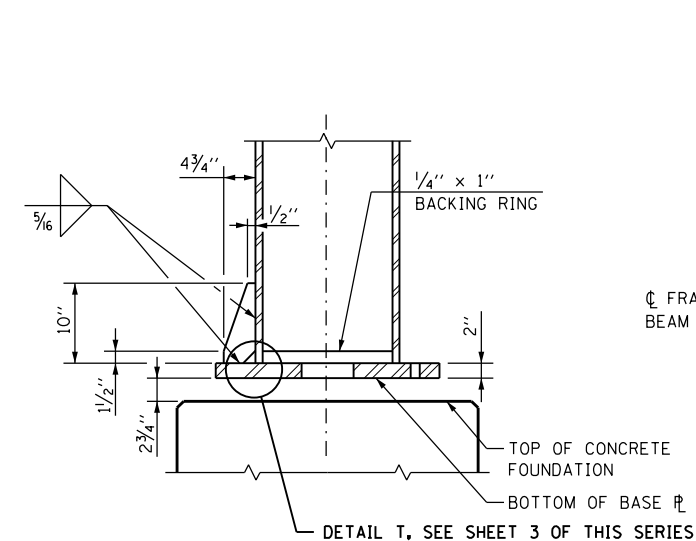
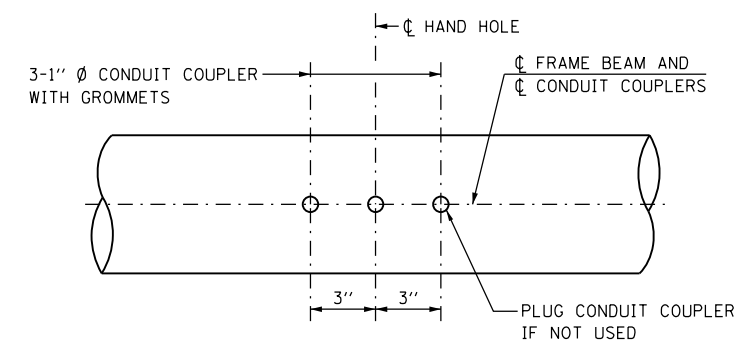
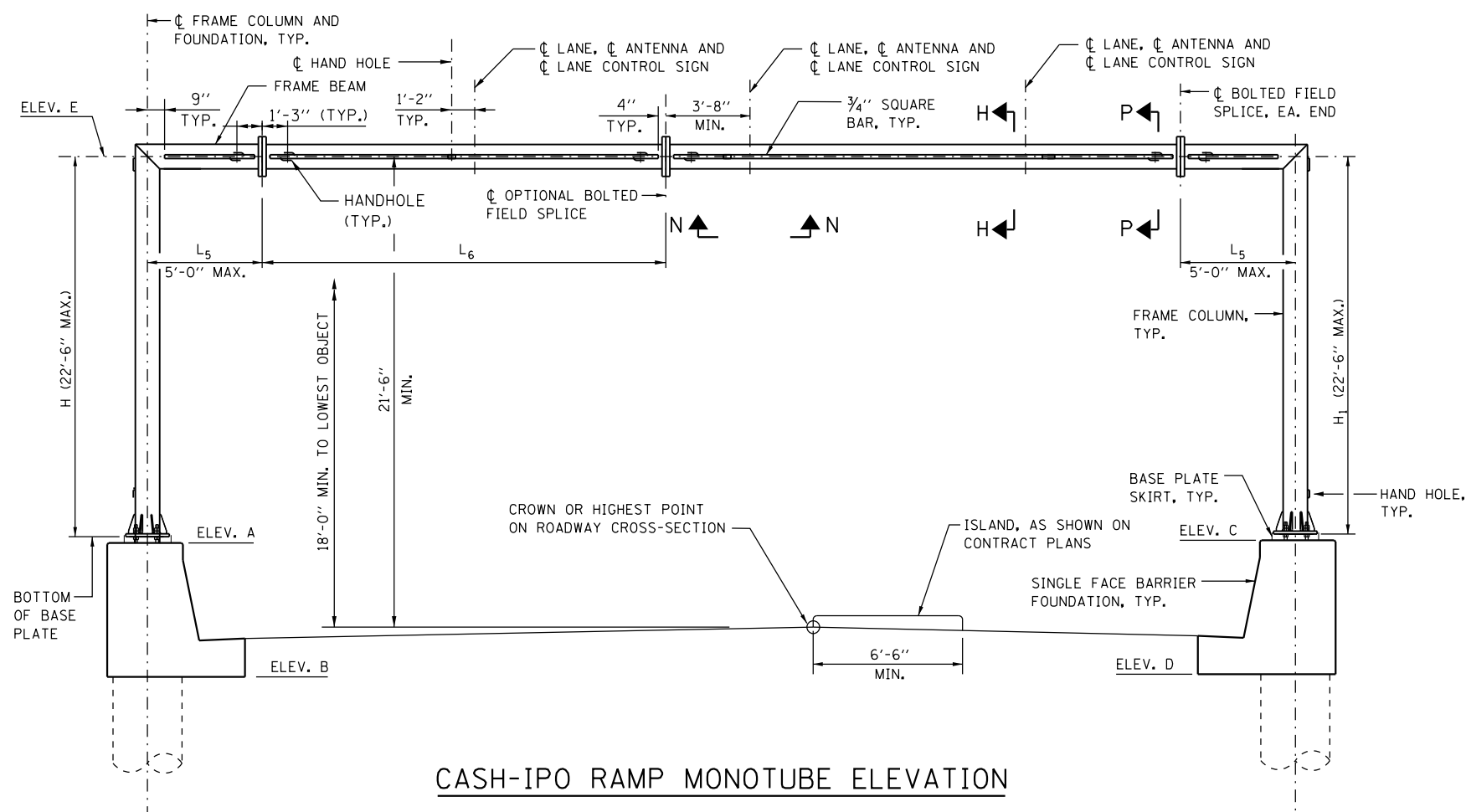
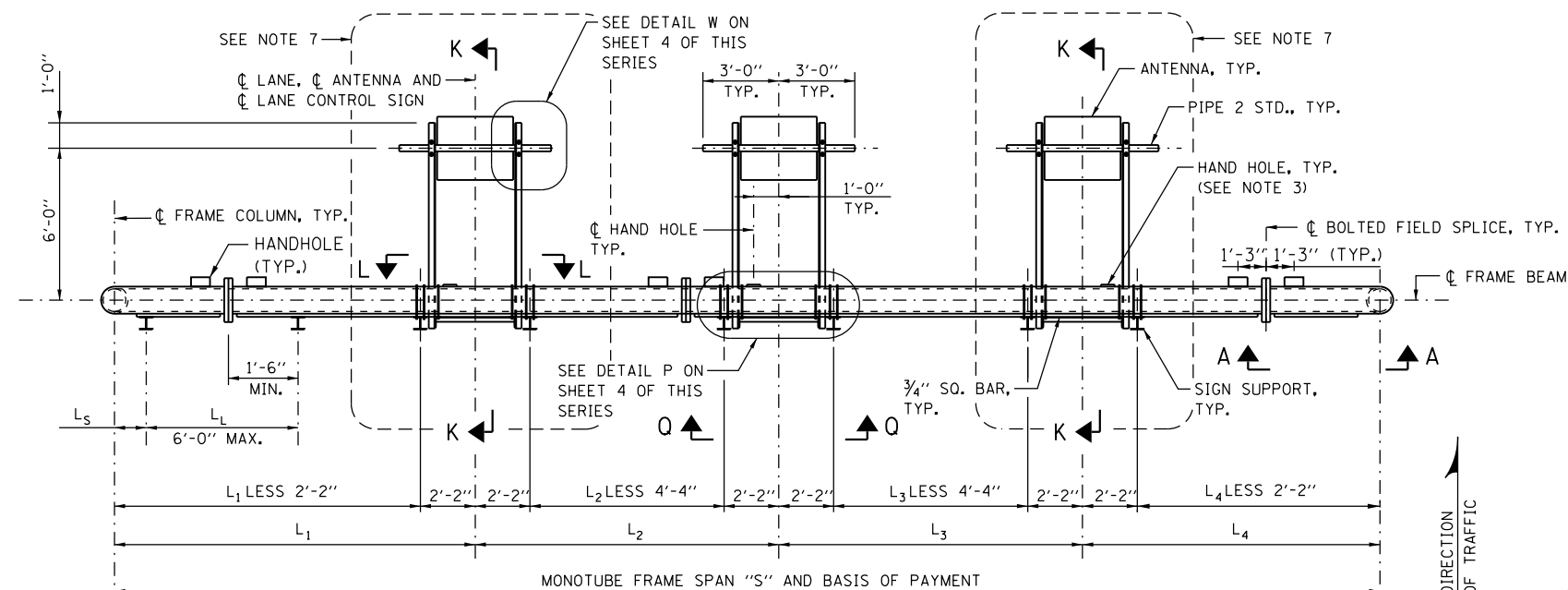
CASH-IPO RAMP TOLL PLAZA ELEVATION

APPROVED, *Paul Kovacs* DATE 10-14-2014
CHIEF ENGINEER

| DATE | REVISIONS |
|-----------|---------------------------------|
| 3-31-2016 | REVISED FOUNDATION NOTE. |
| 3-01-2019 | UPDATED CONSTANT SLOPE |
| | BARRIER, REINFORCING |
| | DETAILS AND QUANTITIES |
| 5-24-2019 | UPDATED SHOULDER BARRIER |
| | DETAILS AND QUANTITIES |
| | FOR 3'-8" |
| 2-13-2020 | ADDED HANDHOLES, INSTALLATION & |
| | INSPECTION OF SPLICE & ANCHORS |
| | UPDATED BARRIER DETAILS |



OVERHEAD SIGN STRUCTURE
MONOTUBE TYPE (STEEL)
STRUCTURE DETAILS
FOR CASH-IPO RAMP
STANDARD F16-04



MONOTUBE FRAME TABLE

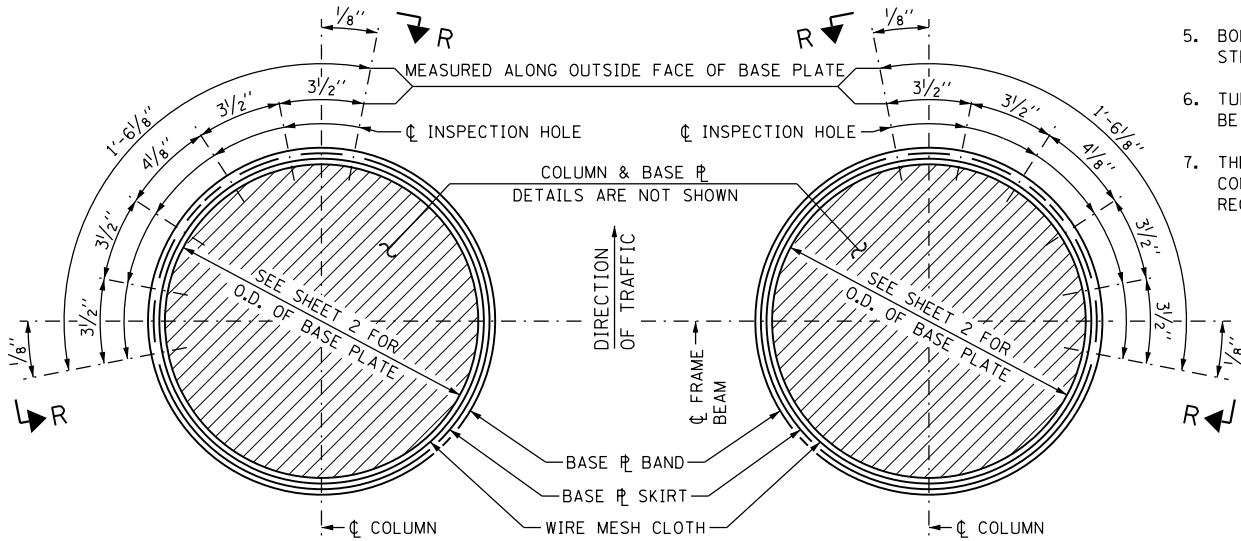
| SPAN "S" | FRAME COLUMN | FRAME BEAM | CAMBER |
|-------------|-----------------|-----------------|--------|
| 60' MAX. | HSS 12.75x0.500 | HSS 12.75x0.500 | 2 1/2" |

NOTES:

1. WORK THIS SHEET WITH OVERHEAD SIGN STRUCTURE MONOTUBE TYPE (STEEL) CASH-IPO RAMP, SUMMARY AND TOTAL BILL OF MATERIAL SHEET.
2. FOUNDATION FOR MONOTUBE FRAME IS SHOWN ON SHEET 5 OF THIS SERIES.
3. SEE SHEET 4 OF THIS SERIES FOR SECTIONS G-G, H-H AND K-K, VIEWS A-A AND Q-Q, AND HAND HOLE DETAILS.
4. SEE SHEET 3 OF THIS SERIES FOR SECTION P-P AND BASE PLATE SKIRT.
5. PROVIDE CAMBER AT MIDSPAN OF STRUCTURE.
6. LOCATE OPTIONAL BOLTED FIELD SPLICE NEAR MIDSPAN.
7. OMIT ANTENNA AND ANTENNA MOUNTING ASSEMBLY ABOVE CASH ONLY LANE.

GENERAL NOTES:

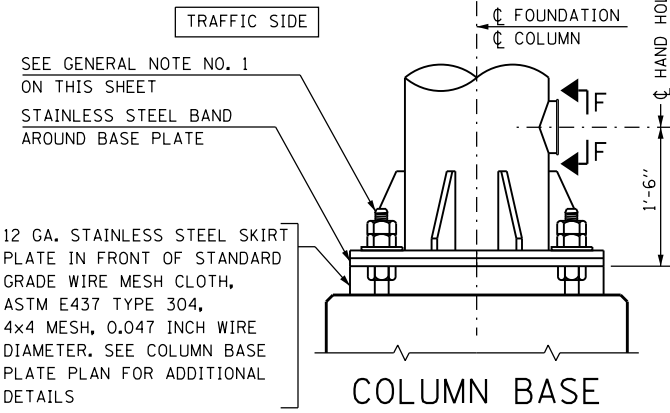
1. AFTER ADJUSTMENTS TO LEVEL FRAME BEAM AND ENSURE ADEQUATE VERTICAL CLEARANCE, TIGHTEN ALL TOP AND LEVELING NUTS AGAINST THE BASE PLATE WITH A MINIMUM TORQUE OF 200 LB.-FT. THEN PLACE STAINLESS STEEL MESH AROUND THE PERIMETER OF THE BASE PLATE. SECURE TO BASE PLATE WITH STAINLESS STEEL BANDING.
2. REINFORCEMENT BARS DESIGNATED "E" SHALL BE EPOXY COATED.
3. FINAL LOCATION OF I-PASS ANTENNAE SHALL BE AS DIRECTED BY THE ILLINOIS TOLLWAY.



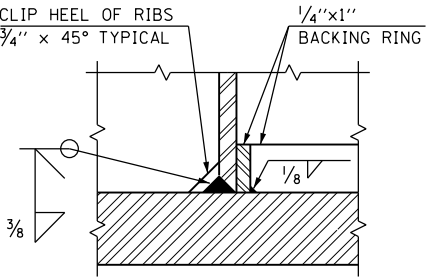
LEFT BASE PLATE
RIGHT BASE PLATE
COLUMN BASE PLATE PLAN

NOTE:

SEE SHEET 4 OF THIS SERIES FOR VIEW F-F.



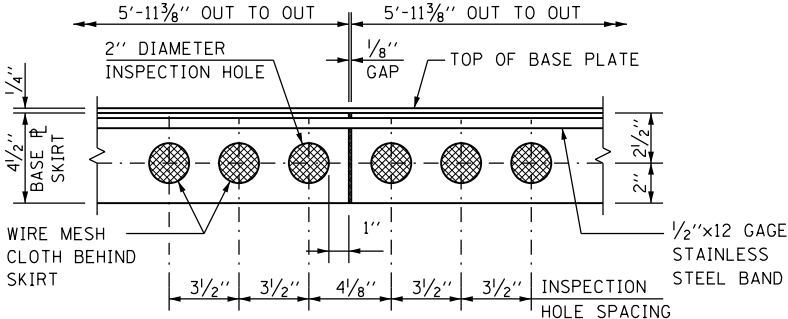
COLUMN BASE



DETAIL T

STRUCTURAL STEEL:

1. MATERIAL FOR THE MONOTUBE FRAME AND RECTANGULAR HSS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A500 GRADE B. BASE PLATE AND STIFFENER PLATE SHALL CONFORM TO ASTM A709 GRADE 50. OTHER STRUCTURAL STEEL SHAPES AND PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A36, UNLESS NOTED OTHERWISE.
2. PIPES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A53 GRADE B.
3. ANCHOR BOLTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM F1554 (AASHTO M314) GRADE 55, WITH A MINIMUM TENSILE STRENGTH OF 75,000 PSI. INSTALLATION AND INSPECTION OF ANCHOR BOLTS SHALL COMPLY WITH ILLINOIS TOLLWAY SPECIAL PROVISION "INTELLIGENT TRANSPORTATION SYSTEMS GANTRY FRAME "STEEL". ANCHORS SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM A153 (AASHTO M232). SEE SHEET 6 OF THIS SERIES FOR GALVANIZED LENGTH.
4. U-BOLTS SHALL BE STAINLESS STEEL. PROVIDE STAINLESS STEEL WASHERS AND NUTS FOR U-BOLTS.
5. BOLTS (EXCLUDING ANCHOR BOLTS AND U-BOLTS) SHALL BE HIGH STRENGTH STEEL BOLTS.
6. TUBES FOR MONOTUBE FRAME, PIPES, STRUCTURAL STEEL SHAPES AND PLATES SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM A123 AFTER FABRICATION.
7. THE MONOTUBE FRAME BEAM, COLUMNS, BASE PLATE MATERIAL, AND SPLICES ARE CONSIDERED TENSION MEMBERS AND SHALL CONFORM TO THE IMPACT TESTING REQUIREMENT, ZONE 2.



VIEW R-R (BASE PLATE SKIRT)

DESIGN LOADING:

| | |
|--------------------|-----------|
| WIND LOAD CRITERIA | |
| SIGN PANEL | 35 P.S.F. |
| COLUMN/BEAM | 35 P.S.F. |

EQUIPMENT LOADS:

| | |
|-----------------------|--------|
| LED LANE CONTROL SIGN | 50 LB. |
| ANTENNA | 28 LB. |

DESIGN STRESSES FOR REINFORCED CONCRETE:

| | |
|------------------------------------------------------|-----------------|
| f'c = COMPRESSIVE STRENGTH OF CONCRETE (CLASS SI) | = 3,500 P.S.I. |
| f'c = COMPRESSIVE STRENGTH OF CONCRETE (CLASS DS) | = 4,000 P.S.I. |
| fy = YIELD STRENGTH OF REINFORCEMENT BARS (GRADE 60) | = 60,000 P.S.I. |

FOUNDATION:

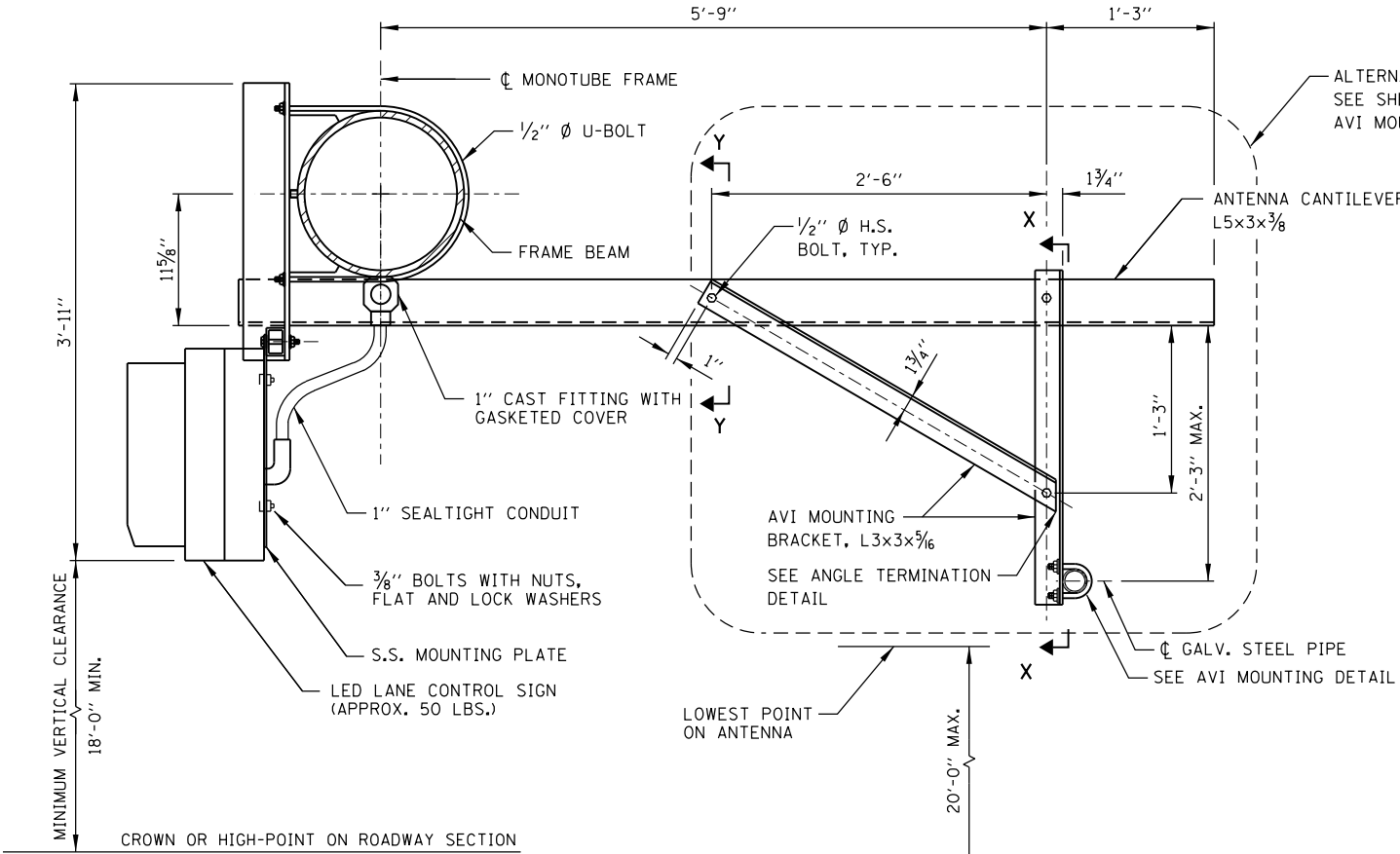
MINIMUM UNCONFINED COMPRESSIVE STRENGTH, QU FOR ALL LAYERS OF COHESIVE SOILS (CLAYS) SHALL BE 1.25 TON/SQ.FT. AT RAMP FRAMES.

DESIGN SPECIFICATIONS:

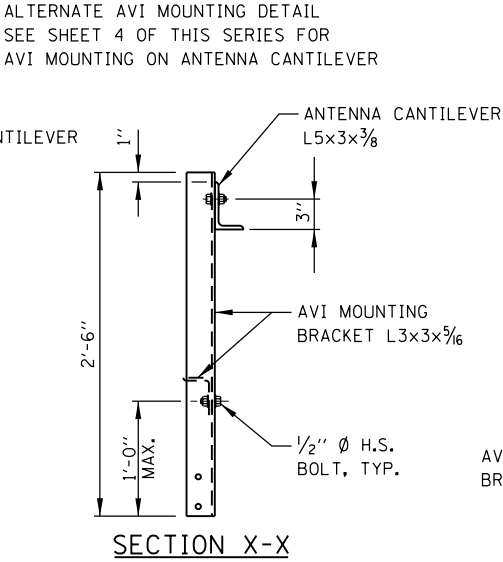
1. ILLINOIS TOLLWAY STRUCTURE DESIGN MANUAL, LATEST EDITION.
2. AASHTO STANDARD SPECIFICATION FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 6TH EDITION.
3. AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 6TH EDITION DATED FEBRUARY 2012.
4. ILLINOIS DEPARTMENT OF TRANSPORTATION BRIDGE MANUAL, JANUARY 2012

CONSTRUCTION SPECIFICATIONS:

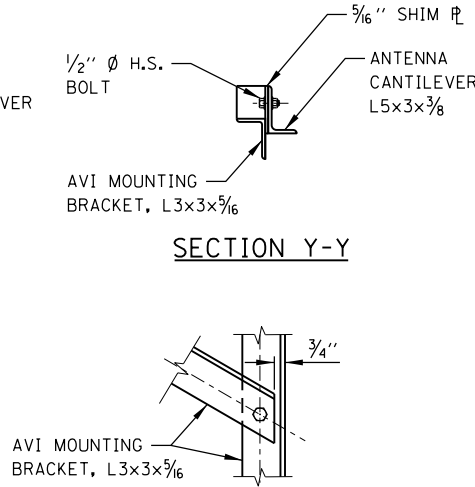
1. ILLINOIS TOLLWAY SUPPLEMENTAL SPECIFICATIONS TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.
2. ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.



SECTION P-P
(LED LANE CONTROL SIGNAL MOUNTING DETAIL)

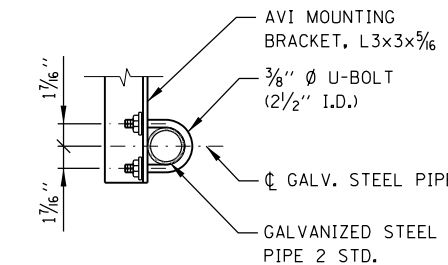


SECTION X-X



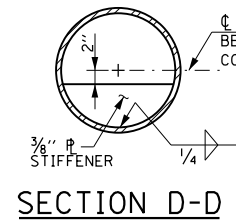
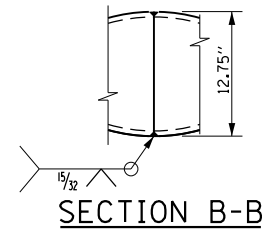
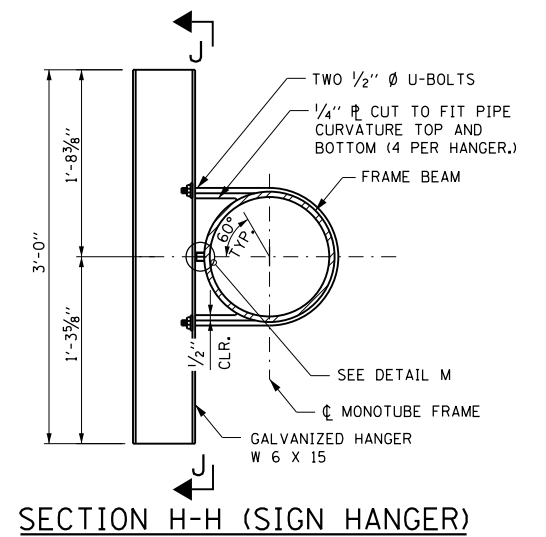
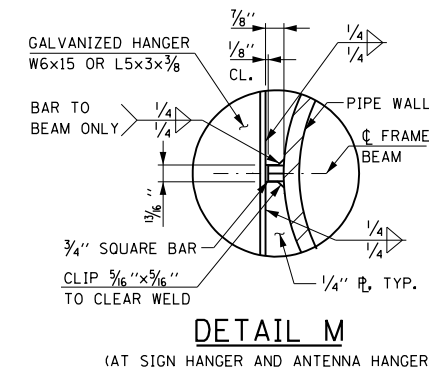
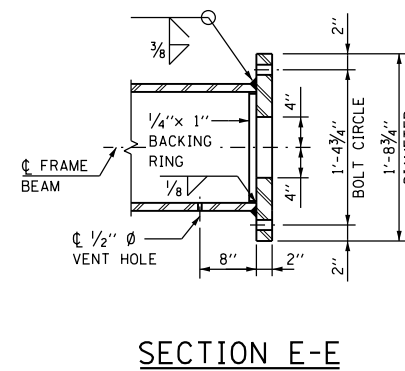
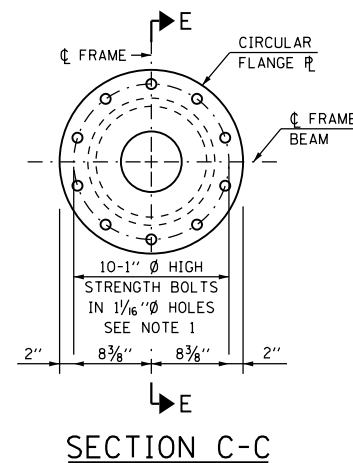
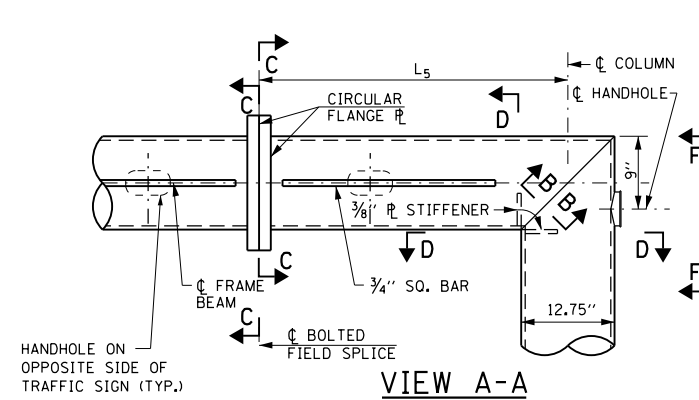
SECTION Y-Y

ANGLE TERMINATION

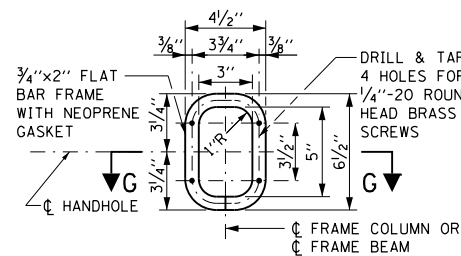


AVI MOUNTING DETAIL

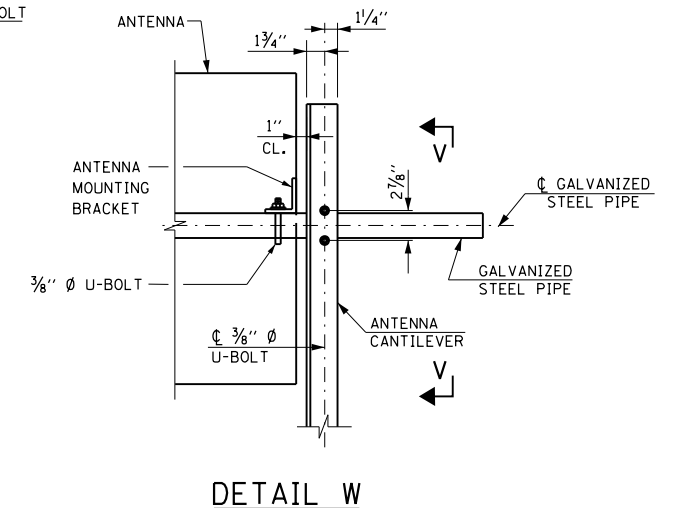
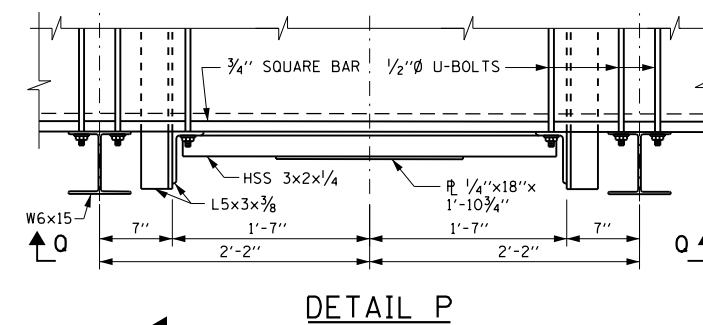
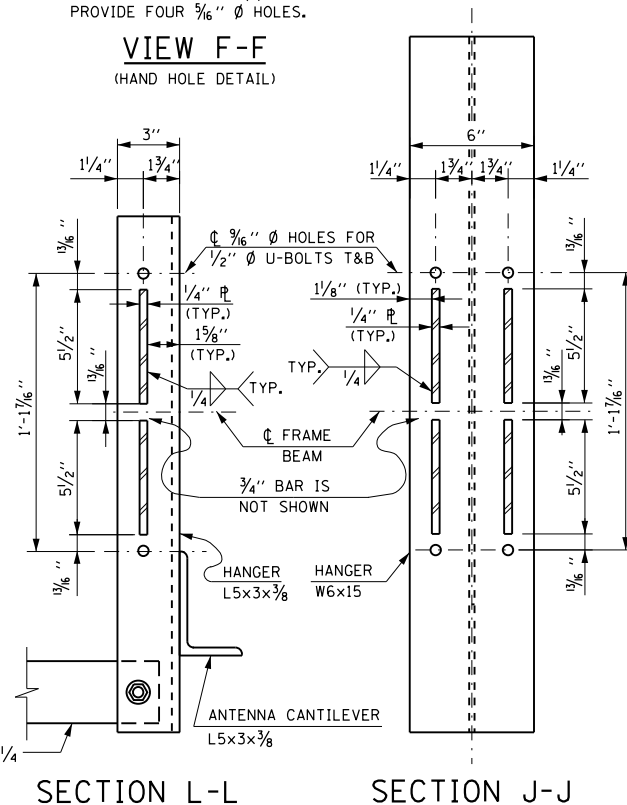
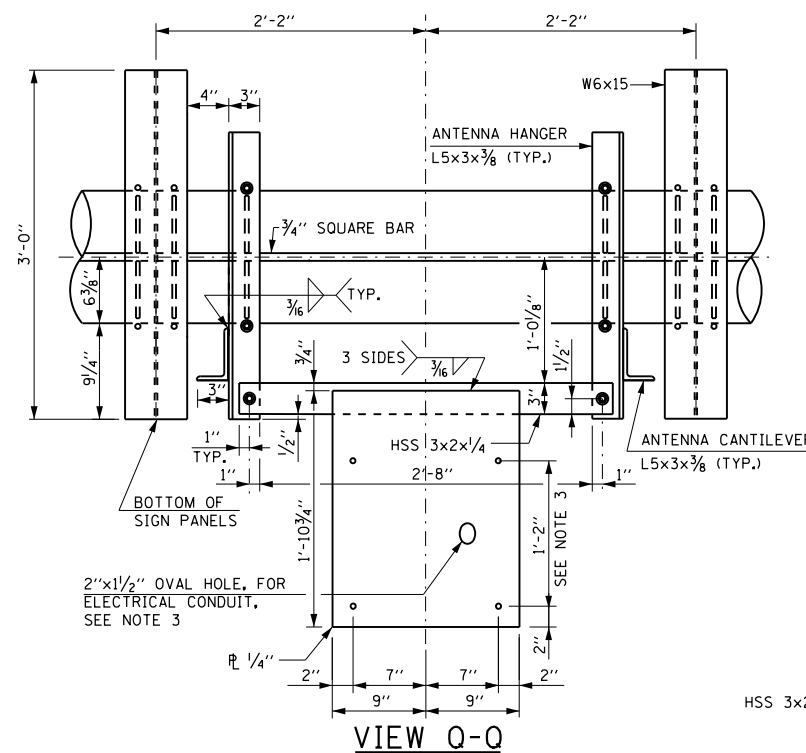
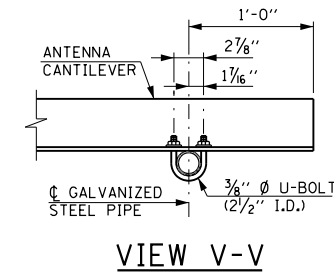
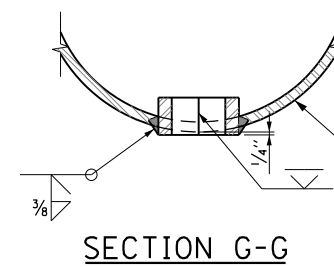




NOTE 1: INSTALLATION AND INSPECTION OF SPLICE BOLTS SHALL COMPLY WITH ILLINOIS TOLLWAY SPECIAL PROVISION "INTELLIGENT TRANSPORTATION SYSTEMS GANTRY FRAME (STEEL)".

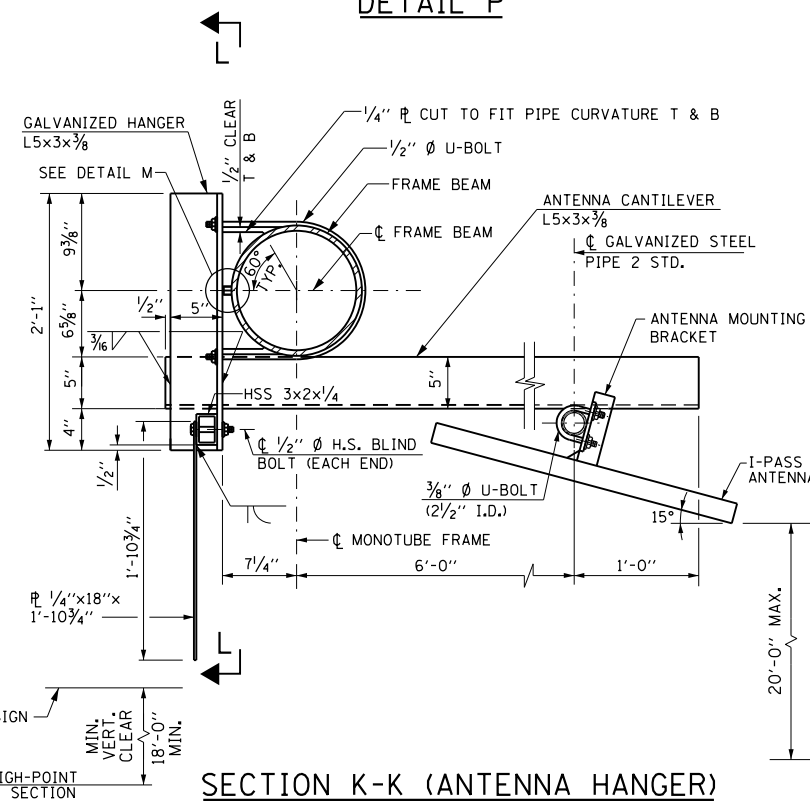


PROVIDE 6 1/2" x 4 1/2" #10 GA. COVER. ROUND CORNERS TO 1 3/4" RADIUS. PROVIDE FOUR 3/8" DIAMETER HOLES.



NOTES:

- SIGN AND SIGN HANGER ARE OMITTED FROM VIEW A-A FOR CLARITY.
- FOR DETAILS OF ATTACHMENT BETWEEN HANGER AND SIGN PANELS, SEE ILLINOIS TOLLWAY STANDARD DRAWING F10.
- CONTRACTOR SHALL VERIFY LOCATION AND SIZE OF HOLES WITH LANE CONTROL SIGNAL PRIOR TO FABRICATION OF 1/4" PLATE.
- T&B DENOTE TOP AND BOTTOM.
- PROVIDE ANTENNA MOUNTING BRACKET ACCORDING TO ANTENNA MANUFACTURER'S RECOMMENDATION.
- SEE SHEET 2 OF THIS SERIES FOR HANDHOLE LOCATIONS.

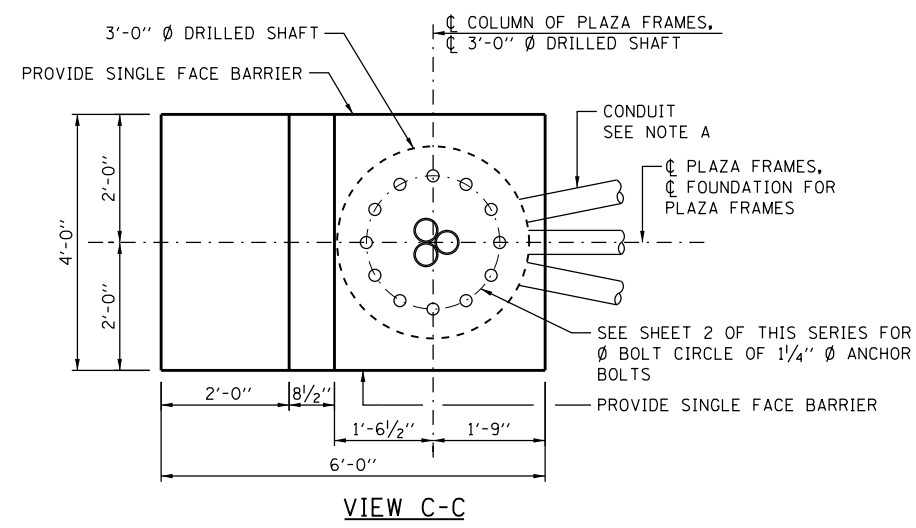
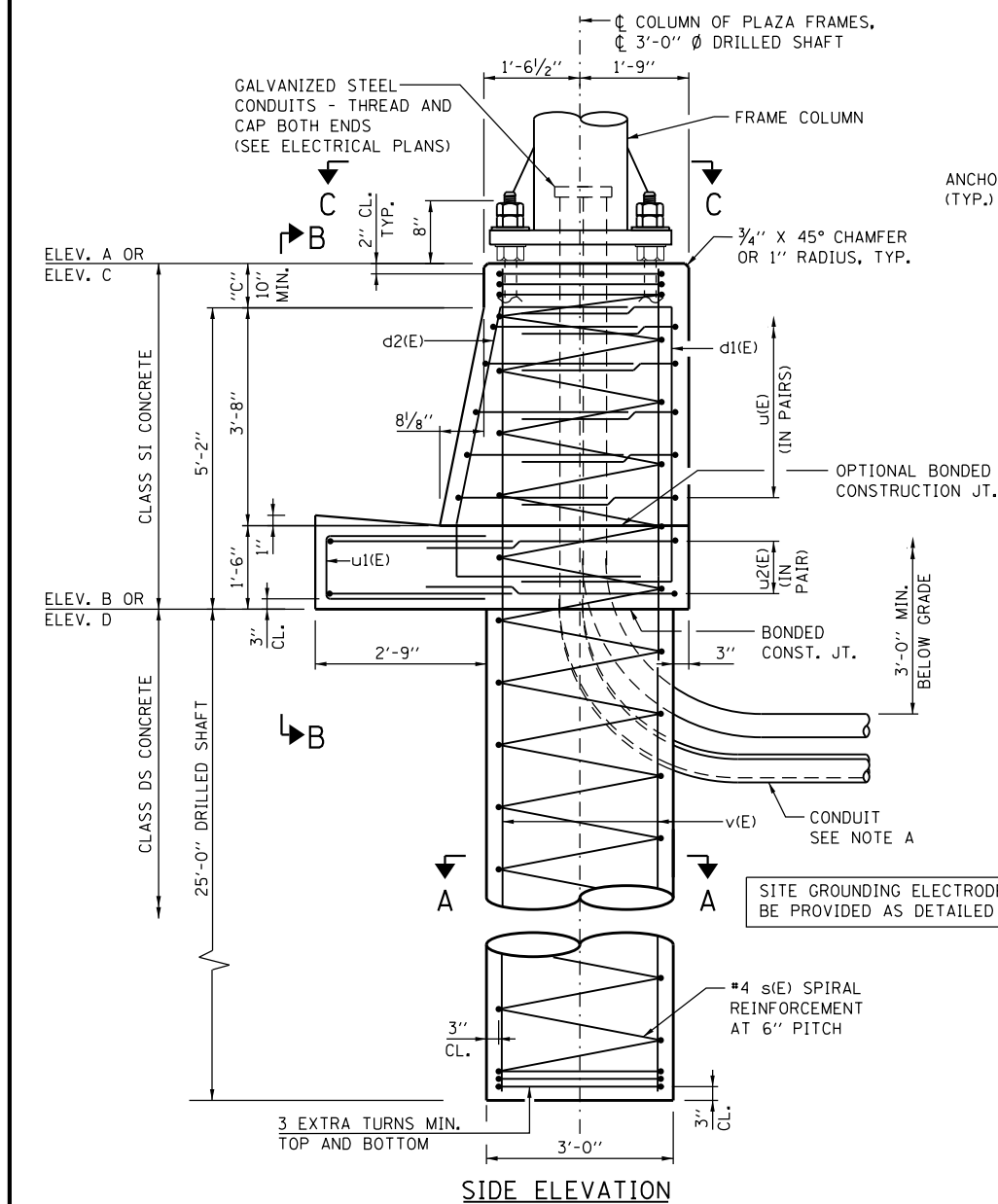


Paul Kovacs
APPROVED..... CHIEF ENGINEER DATE 10-14-2014

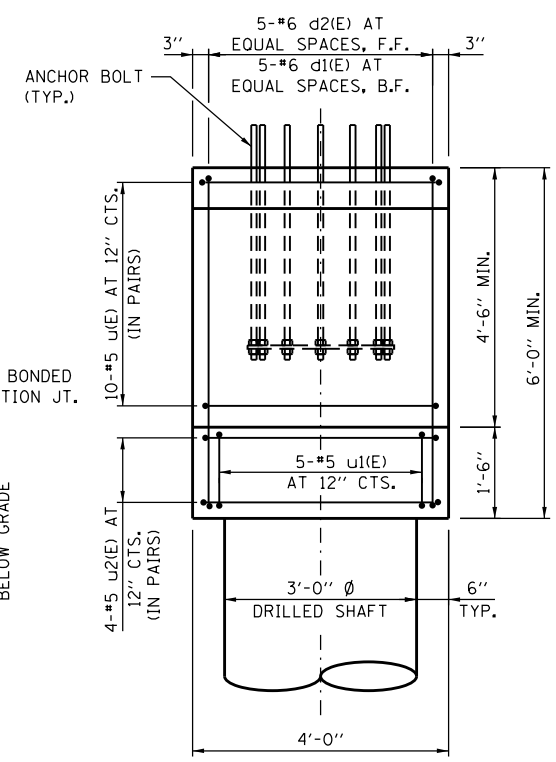
SHEET 4 OF 6



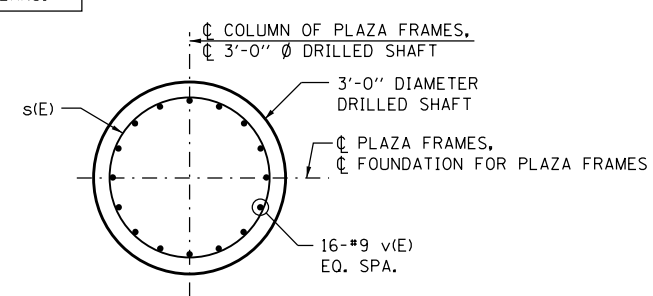
OVERHEAD SIGN STRUCTURE
MONOTUBE TYPE (STEEL)
STRUCTURE DETAILS
FOR CASH-IPO RAMP
STANDARD F16-04



SINGLE FACE BARRIER FOUNDATION FOR PLAZA FRAMES



VIEW B-B



SECTION A-A

NOTE A:

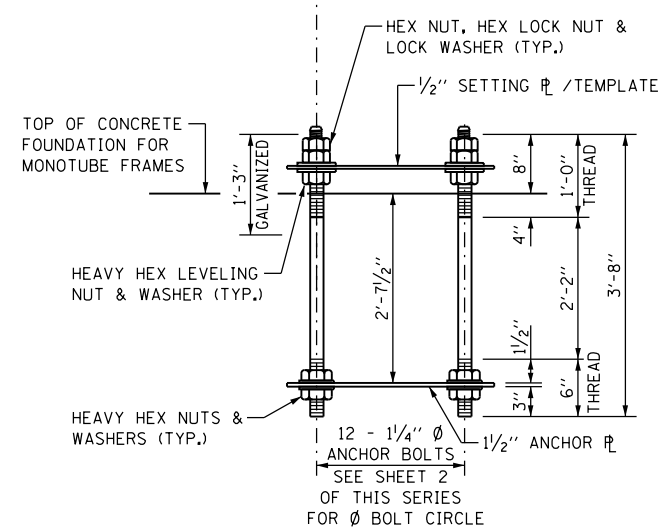
1. COORDINATE CONDUIT SIZE, LOCATION AND QUANTITY WITH ELECTRICAL PLANS. PROVIDE CONDUIT COUPLERS AS REQUIRED.
2. CONDUITS SHALL BE PLACED TO MISS REINFORCEMENT. CUTTING OF REINFORCEMENT SHALL NOT BE ALLOWED.
3. COST INCLUDED IN FOUNDATION FOR OVERHEAD SIGN STRUCTURE, RAMP MONOTUBE TYPE.
4. PROTECTIVE COAT SHALL BE APPLIED TO THE TRAFFIC AND TOP FACES OF BARRIER AND TOP OF GUTTER.

FOUNDATIONS:

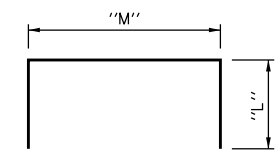
THE FOUNDATION DETAILS SHOWN ARE BASED ON THE PRESENCE OF MOSTLY COHESIVE SOIL CONDITIONS (SILTY OR SANDY CLAY), WITH AN AVERAGE UNCONFINED COMPRESSIVE STRENGTH (QU) > 1.25 TON/SQ. FT. WHICH SHALL BE DETERMINED BY PREVIOUS SOIL INVESTIGATIONS AT THE JOBSITE. WHEN OTHER CONDITIONS ARE INDICATED, THE BORING DATA SHALL BE INCLUDED IN THE PLANS AND THE FOUNDATION DIMENSIONS SHOWN SHALL BE THE RESULT OF SITE SPECIFIC DESIGNS. IF CONDITIONS ENCOUNTERED IN THE FIELD ARE DIFFERENT THAN THOSE INDICATED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER TO DETERMINE IF THE FOUNDATION DIMENSIONS NEED TO BE MODIFIED.

LEGEND:

F.F. - FRONT FACE
B.F. - BACK FACE
CTS. - CENTERS

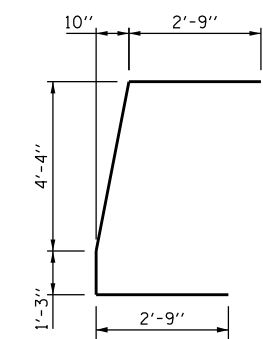


ANCHOR BOLT ASSEMBLY

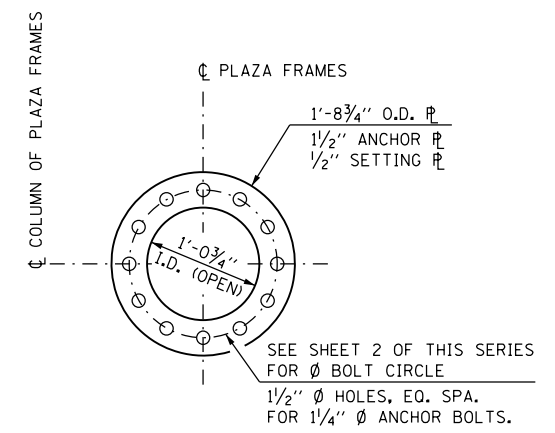


| BAR | "L" | "M" |
|-------|--------|-------|
| d1(E) | 2'-9" | 5'-7" |
| u(E) | 2'-9" | 3'-8" |
| u1(E) | 3'-3" | 1'-1" |
| u2(E) | 3'-10" | 3'-8" |

BARS d1(E), u(E), u1(E) AND u2(E)



BAR d2(E)



ANCHOR BOLT / SETTING BOLT

REINFORCEMENT BAR SCHEDULE FOR ONE FOUNDATION

| BAR | NO. | SIZE | LENGTH | SHAPE |
|-------|-----|------|--------|-------|
| d1(E) | 5 | #6 | 11'-1" | |
| d2(E) | 5 | #6 | 11'-2" | |
| s(E) | 1 | #4 | 30'-7" | |
| v(E) | 16 | #9 | 30'-7" | |
| u(E) | 10 | #5 | 9'-2" | |
| u1(E) | 5 | #5 | 7'-7" | |
| u2(E) | 4 | #5 | 11'-4" | |

* THE LENGTH OF SPIRAL SHOWN IS THE HEIGHT OF SPIRAL, COMPUTED USING "C" = 10". ADJUST LENGTH ACCORDINGLY IF "C" IS GREATER THAN 10".

** BAR LENGTH IS COMPUTED USING "C" = 10". ADJUST BAR LENGTH ACCORDINGLY IF "C" IS GREATER THAN 10".

ESTIMATED QUANTITY

| ITEM | UNIT | SINGLE FACE BARRIER FDN. |
|----------------------------------|---------|--------------------------|
| CLASS S1 CONCRETE | CU. YD. | 3.8 |
| CLASS DS CONCRETE | CU. YD. | 6.6 |
| REINFORCEMENT BARS, EPOXY COATED | POUND | 2,360 |
| PROTECTIVE COAT | SQ. YD. | 4.4 |

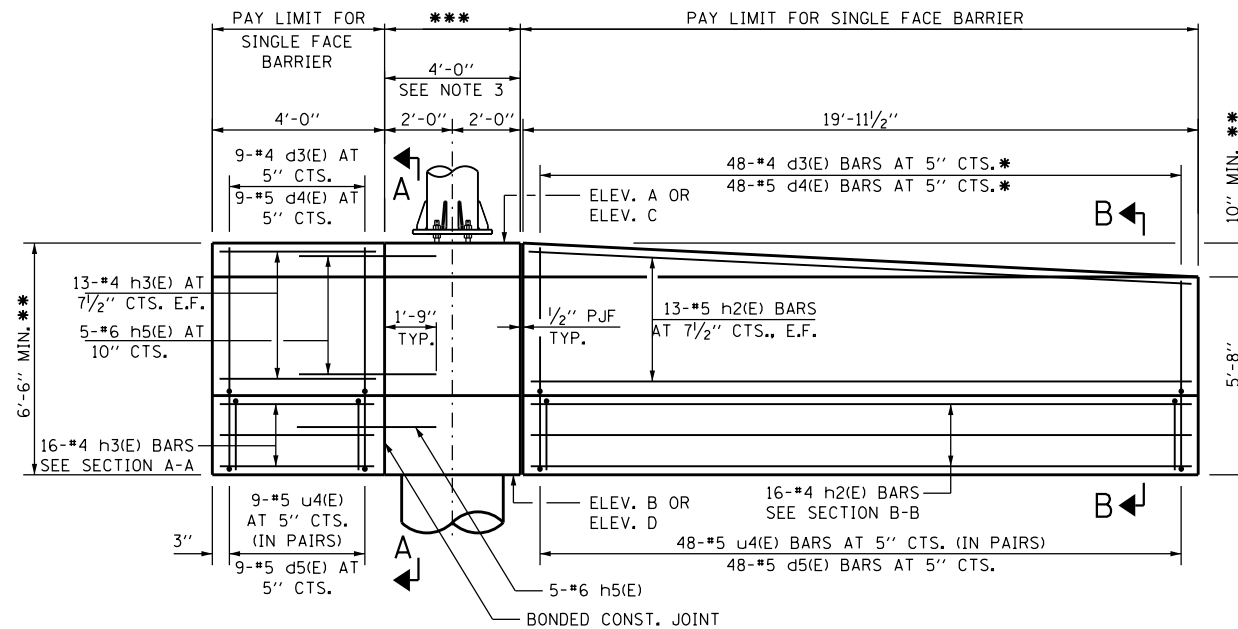
NOTE:

QUANTITIES FOR SINGLE FACE BARRIER FOUNDATION ARE DETERMINED USING "C" = 10". IF DIMENSION "C" IS GREATER THAN 10", ADJUST QUANTITIES ACCORDINGLY.



OVERHEAD SIGN STRUCTURE
MONOTUBE TYPE (STEEL)
STRUCTURE DETAILS
FOR CASH-IPO RAMP

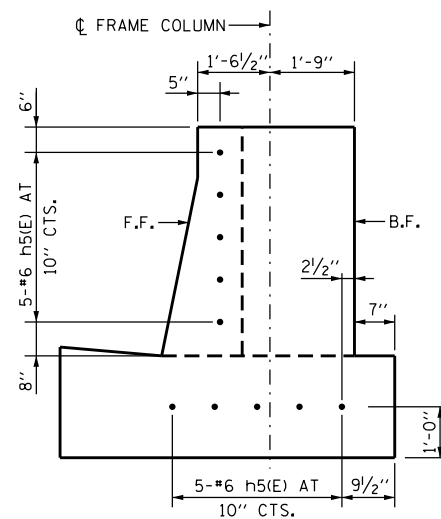
STANDARD F16-04



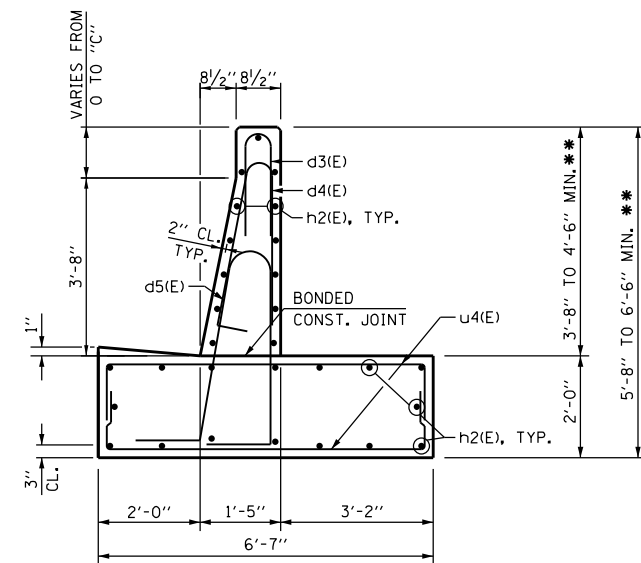
* CUT IN FIELD AS REQUIRED TO FIT TAPER
 ** BASED ON DIMENSION "C" = 10"
 *** PAY LIMIT FOR FOUNDATION FOR OVERHEAD SIGN STRUCTURE

SINGLE FACE BARRIER ELEVATION

INSIDE FACE OF RIGHT BARRIER IS SHOWN
 (MIRROR ELEVATION OF LEFT BARRIER)



SECTION A-A



SECTION B-B

ESTIMATED QUANTITY

(FOR ONE SINGLE FACE BARRIER)

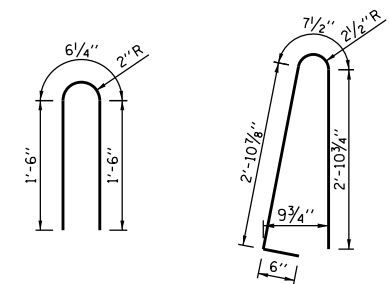
| ITEM | UNIT | TOTAL |
|----------------------------------|---------|-------|
| CONCRETE STRUCTURES | CU. YD. | 15.6 |
| REINFORCEMENT BARS, EPOXY COATED | POUND | 2,750 |
| PROTECTIVE COAT | SQ. YD. | 18.5 |

NOTES:

- PROTECTIVE COAT SHALL BE APPLIED TO THE TRAFFIC AND TOP FACES OF THE BARRIER, GUTTER AND TO THE ENTRANCE SIDE FACE (AT THE BEGINNING OF THE RAMP PLAZA PAVEMENT) FOR THE FULL HEIGHT OF THE BARRIER.
- ELECTRICAL JUNCTION BOXES SHALL BE EXTERIOR MOUNTED ON THE BACK FACE OF BARRIER.
- FOR SINGLE FACE BARRIER FOUNDATION DETAILS FOR MONOTUBE FRAMES, SEE SHEET 5 OF THIS SERIES.
- QUANTITIES FOR SINGLE FACE BARRIER ARE DETERMINED USING "C" = 10". IF DIMENSION "C" IS GREATER THAN 10", ADJUST QUANTITIES ACCORDINGLY.
- WORK THIS SHEET WITH, OVERHEAD SIGN STRUCTURE MONOTUBE TYPE (STEEL) CASH-IPO RAMP SUMMARY AND TOTAL BILL OF MATERIAL SHEET.

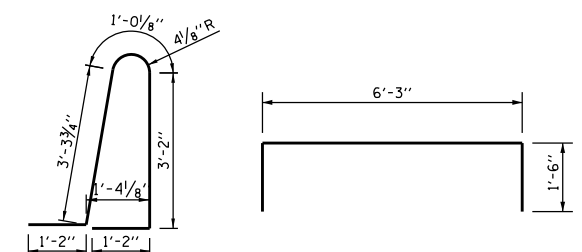
BAR LIST - ONE BARRIER

| BAR | NO. | SIZE | LENGTH | SHAPE |
|-------|-----|------|--------|-------|
| d3(E) | 57 | #4 | 3'-7" | U |
| d4(E) | 57 | #5 | 7'-0" | U |
| d5(E) | 57 | #5 | 9'-10" | U |
| h2(E) | 29 | #4 | 19'-7" | I |
| h3(E) | 29 | #4 | 3'-8" | I |
| h5(E) | 10 | #6 | 3'-9" | I |
| u4(E) | 114 | #5 | 9'-3" | U |



BAR d3(E)

BAR d4(E)



BAR d5(E)

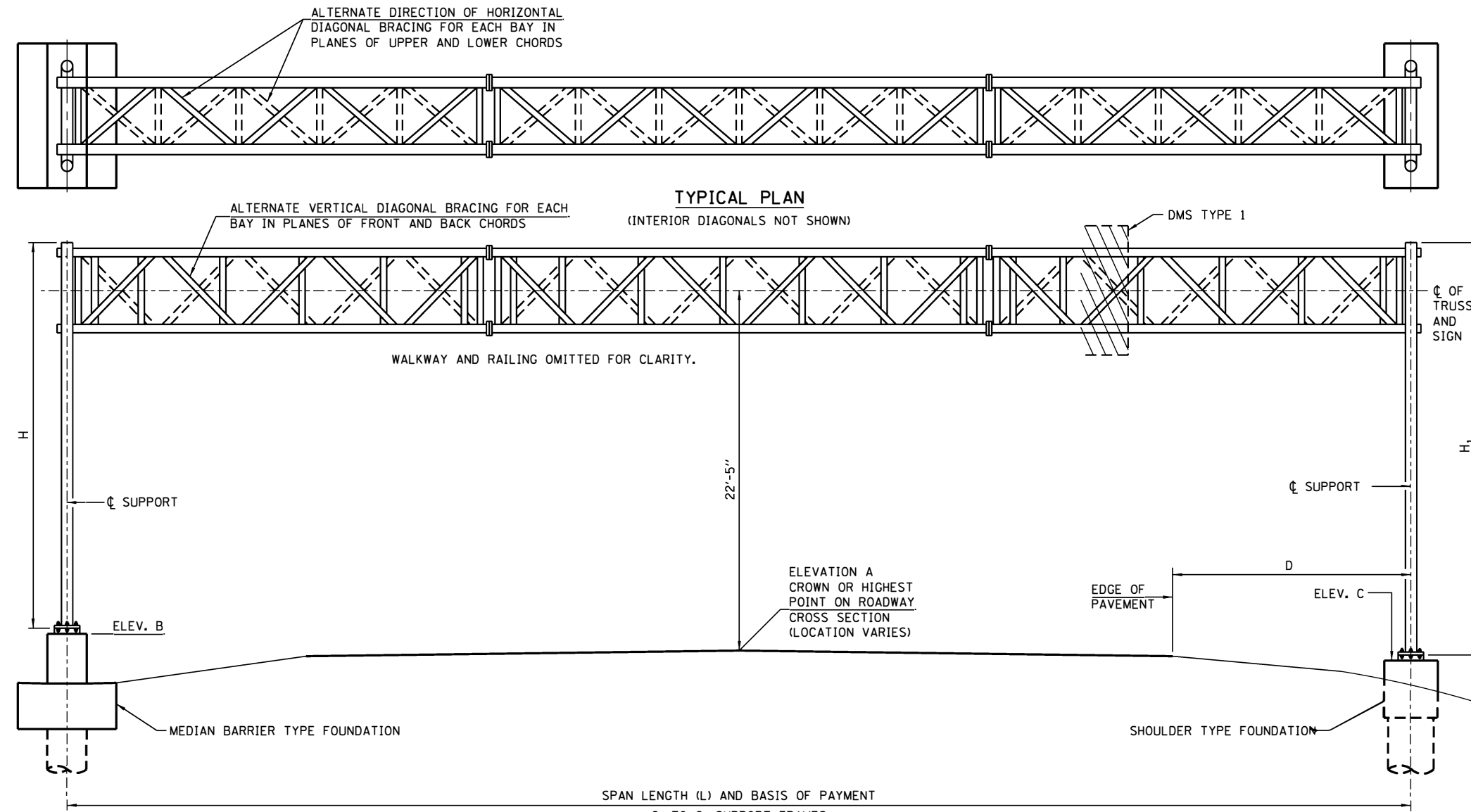
BAR u4(E)

APPROVED.....
 CHIEF ENGINEER
 DATE 10-14-2014



OVERHEAD SIGN STRUCTURE
 MONOTUBE TYPE (STEEL)
 STRUCTURE DETAILS
 FOR CASH-IPO RAMP

STANDARD F16-04



GENERAL NOTES:

1. WORK THIS SHEET WITH, OVERHEAD SIGN STRUCTURE SPAN TYPE (STEEL) SUMMARY AND BILL OF MATERIAL SHEET.
2. AFTER ADJUSTMENTS TO LEVEL TRUSS AND ENSURE ADEQUATE VERTICAL CLEARANCE, ALL TOP AND LEVELING NUTS SHALL BE TIGHTENED AGAINST THE BASE PLATE WITH A MINIMUM TORQUE OF 200 LB.-FT. STAINLESS STEEL MESH SHALL THEN BE PLACED AROUND THE PERIMETER OF THE BASE PLATE. SECURE TO BASE PLATE WITH STAINLESS STEEL BANDING.
3. SIGN SUPPORT STRUCTURES MAY BE SUBJECT TO DAMAGING VIBRATIONS AND OSCILLATIONS WHEN DMS IS NOT IN PLACE DURING ERECTION OR MAINTENANCE OF THE STRUCTURE. TO AVOID THESE, ATTACH TEMPORARY BLANK SIGN PANELS OR OTHER BRACING TO THE STRUCTURE UNTIL DMS IS INSTALLED.
4. TRUSS UNITS SHALL BE SHIPPED INDIVIDUALLY WITH ADEQUATE PROVISION TO PREVENT DETRIMENTAL MOTION DURING TRANSPORT. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE CONFIGURATION AND PROTECTION OF THE TRUSS UNITS.
5. ALL WELDS SHALL BE CONTINUOUS UNLESS OTHERWISE SHOWN. ALL WELDING SHALL BE DONE IN ACCORDANCE WITH CURRENT AWS D1.1 STRUCTURE WELDING CODE AND THE STANDARD SPECIFICATIONS.
6. INSTALLATIONS NOT WITHIN DIMENSIONAL LIMITS SHOWN REQUIRE SPECIAL ANALYSIS FOR ALL COMPONENTS.
7. ONE DMS TYPE 1 IS PERMITTED TO BE MOUNTED ON A SPAN TRUSS. DO NOT MOUNT SIGN PANELS ON THIS TRUSS.

FABRICATION NOTES:

1. MATERIALS: SEE MATERIAL SPECIFICATIONS TABLE FOR MATERIAL SPECIFICATIONS FOR OVERHEAD SIGN STRUCTURE SPAN TYPE (STEEL). STAINLESS STEEL FOR SHIMS, SLEEVES AND HANDHOLE COVERS SHALL BE ASTM A240, TYPE 302 OR 304 OR ANOTHER ALLOY SUITABLE FOR EXTERIOR EXPOSURE AND ACCEPTABLE TO THE ENGINEER. THE STEEL PIPE AND STIFFENING RIBS AT THE BASE PLATE FOR THE STEEL POST SHALL HAVE A MINIMUM LONGITUDINAL CHАРPY V-NOTCH (CVN) ENERGY OF 15 LB.-FT. AT 40°F (ZONE 2) BEFORE GALVANIZING.
2. WELDING: ALL MATERIALS, WELDING PROCEDURES AND INSPECTION USED FOR THE SPAN TYPE OVERHEAD SIGN STRUCTURE SHALL CONFORM TO AWS D1.1-10 FOR TUBULAR, CYCLICALLY LOADED STRUCTURES. ADDITIONALLY, ALL WELDED MATERIALS USED SHALL BE PREQUALIFIED FOR USE WITH WPS PER AWS D1.1-10, TABLE 3.1.
3. FASTENERS FOR STEEL TRUSSES: HIGH STRENGTH BOLTS SHALL SATISFY THE REQUIREMENTS OF AASHTO M164 (ASTM A325), OR APPROVED ALTERNATE, AND SHALL HAVE MATCHING LOCKNUTS. THREADED STUDS FOR SPLICES (IF MEMBERS INTERFERE) SHALL SATISFY THE REQUIREMENTS OF ASTM A449, ASTM A193 GRADE B7, OR APPROVED ALTERNATE, AND SHALL HAVE MATCHING LOCKNUTS. BOLTS AND LOCKNUTS NOT REQUIRED TO BE HIGH STRENGTH SHALL SATISFY THE REQUIREMENTS OF ASTM A307. ALL BOLTS AND LOCKNUTS SHALL BE HOT DIP GALVANIZED PER AASHTO M232, EXCEPT STAINLESS STEEL FASTENERS, NUTS AND WASHERS. THE LOCKNUTS SHALL HAVE NYLON OR STEEL INSERTS. A STAINLESS STEEL FLAT WASHER CONFORMING TO ASTM A240 TYPE 302 OR 304, IS REQUIRED UNDER BOTH HEAD AND NUT OR UNDER BOTH NUTS WHERE THREADED STUDS ARE USED. HIGH STRENGTH BOLT INSTALLATION SHALL CONFORM TO ARTICLE 505.04(f)(2)d OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. ROTATIONAL CAPACITY ("ROCAP") TESTING OF BOLTS WILL NOT BE REQUIRED.
4. U-BOLTS: U-BOLTS SHALL BE PRODUCED FROM ASTM A193 GRADE B8 OR B8M, OR AN EQUIVALENT MATERIAL ACCEPTABLE TO THE ENGINEER. ALL NUTS FOR U-BOLTS SHALL BE LOCKNUTS EQUIVALENT TO ASTM A307 WITH NYLON OR STEEL INSERTS AND HOT DIP GALVANIZED PER AASHTO M232. A STAINLESS STEEL FLAT WASHER CONFORMING TO ASTM A240, TYPE 302 OR 304, IS REQUIRED UNDER EACH U-BOLT LOCKNUT.
5. STEEL GRATING: STEEL BARS FOR GRATING ELEMENTS SHALL CONFORM TO ASTM A36 OR AN EQUIVALENT MATERIAL ACCEPTABLE TO THE ENGINEER.
6. GALVANIZING: ALL PLATES, SHAPES AND PIPE SHALL BE HOT DIP GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH AASHTO M111. PAINTING IS NOT PERMITTED. ALL FASTENERS SHALL BE HOT DIP GALVANIZED IN ACCORDANCE WITH AASHTO M111 OR M232 AS APPROPRIATE FOR THE PRODUCT (EXCEPT STAINLESS STEEL FASTENERS).

CONSTRUCTION SPECIFICATIONS:

1. ALL MATERIALS, EXCEPT AS SHOWN, FABRICATION, ERECTION AND CONSTRUCTION REQUIREMENTS SHALL BE IN ACCORDANCE WITH THE LATEST STANDARD SPECIFICATIONS.
2. SPAN TYPE (STEEL) TRUSS ARE DESIGNED FOR A 10'-0" DEEP DMS, WITH A MAXIMUM LENGTH OF 30'-0" AND A MAXIMUM THICKNESS OF 4'-2".
3. WALKWAY LOADING SHALL INCLUDE DEAD LOAD PLUS 500 LBS. CONCENTRATED LIVE LOAD.

LOADING:

1. SPAN TYPE (STEEL) TRUSS ARE DESIGNED FOR A 10'-0" DEEP DMS, WITH A MAXIMUM LENGTH OF 30'-0" AND A MAXIMUM THICKNESS OF 4'-2".
2. SPAN TYPE (STEEL) TRUSS ARE DESIGNED FOR 40 PSF WIND PRESSURE ON TRUSS MEMBERS AND 56 PSF ON DMS.
3. WALKWAY LOADING SHALL INCLUDE DEAD LOAD PLUS 500 LBS. CONCENTRATED LIVE LOAD.

DESIGN SPECIFICATIONS:

THESE STRUCTURES ARE DESIGNED TO SATISFY THE 2015 AASHTO LRFD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, FIRST EDITION WITH INTERIMS.

**MATERIAL SPECIFICATIONS TABLE
FOR STRUCTURAL STEEL AND FASTENERS**

| ELEMENT OF STRUCTURE | SPECIFICATION | MINIMUM YIELD STRENGTH (K.S.I.) | MINIMUM ULTIMATE STRENGTH (K.S.I.) |
|-----------------------------------------|------------------------------------------|---------------------------------|------------------------------------|
| HOLLOW STRUCTURAL SECTIONS (HSS) | ASTM A500 GRADE B | 42 | 58 |
| STRUCTURAL STEEL PIPE | ASTM A53, TYPE E OR S, GRADE B | 35 | 60 |
| STRUCTURAL STEEL BAR, PLATES AND SHAPES | ASTM A572 GRADE 50 | 50 | 65 |
| STAINLESS STEEL BOLTS | ASTM A193 GRADE B8 OR B8M | 30 | 75 |
| STRUCTURAL STEEL BOLTS | ASTM 325, TYPE 1 | -- | 105 |
| STAINLESS STEEL LOCKNUTS | ASTM A194 GRADE 8F ASTM A194 GRADE 2H | -- | -- |
| NUTS | ASTM A563 GRADE DH | -- | -- |
| STEEL WASHERS | ASTM F436 | -- | -- |
| STAINLESS STEEL WASHERS | ASTM A240, TYPE 302 | -- | -- |
| STEEL ANCHOR BOLTS | AASHTO M314 OR ASTM F1554 | 105 | 125 |

DESIGN WIND LOADING DIAGRAM

ETPA = EFFECTIVE TRUSS PROJECTED AREA.
MAXIMUM DMS WEIGHT = 5000 LBS.

SHEET 1 OF 12

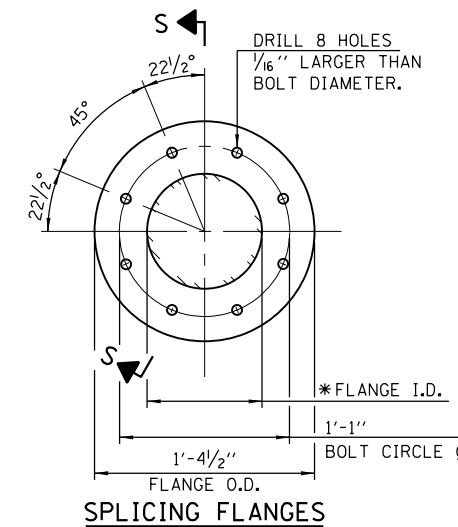
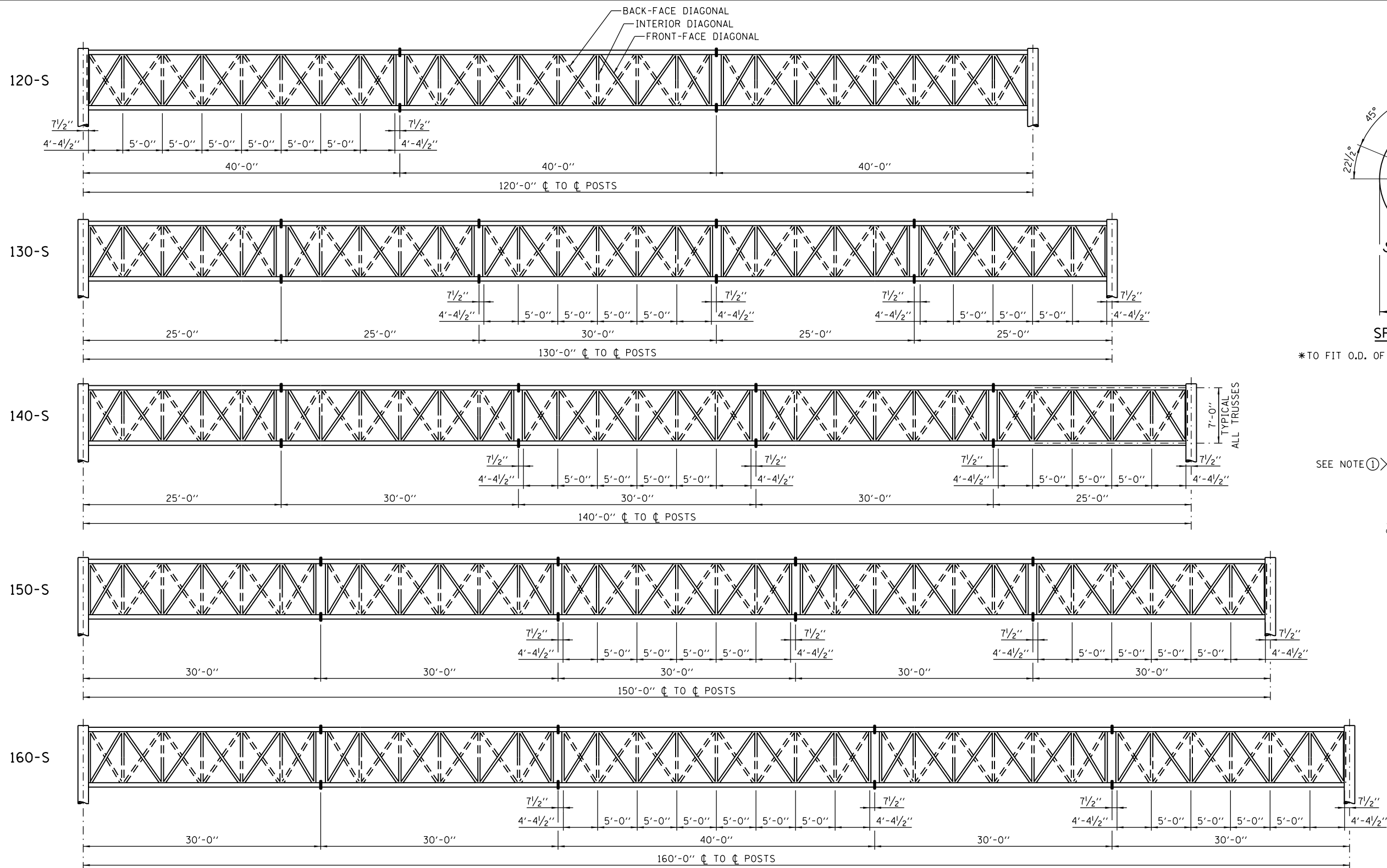


OVERHEAD SIGN STRUCTURE
SPAN TYPE (STEEL)
STRUCTURE DETAILS

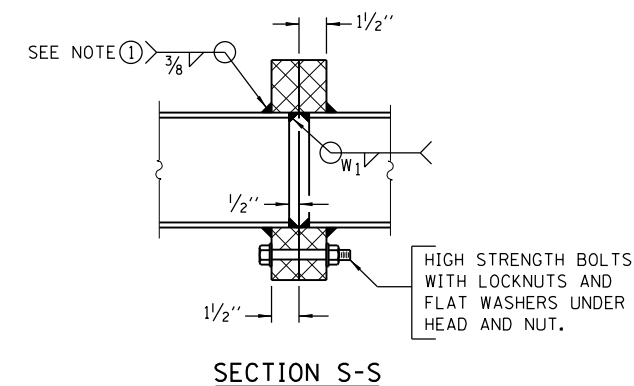
STANDARD F17-05

| DATE | REVISIONS |
|-----------|-----------------------------------------------------------|
| 3-31-2016 | REVISED FOUNDATION NOTE. |
| 3-31-2017 | FOUNDATION REINFORCEMENT UPDATE |
| 3-01-2018 | REVISED SIGN STRUCTURE |
| 3-01-2019 | UPDATE BARRIER SHAPE, HEIGHT AND TRANSITION LENGTH |
| 2-13-2020 | UPDATE CRASHWALL HEIGHT ADDED HEAVY HEX NUT TO ANCHORS |

APPROVED.....
DATE 5-20-2014.
CHIEF ENGINEERING OFFICER



*TO FIT O.D. OF CHORD WITH MAXIMUM GAP OF 1/16".

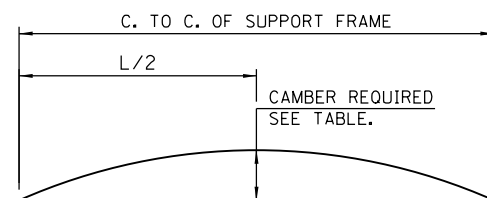


NOTE:

- ① SPLICING FLANGES SHALL BE ATTACHED TO EACH TRUSS UNIT WITH THE TRUSS SHOP ASSEMBLED TO CAMBER SHOWN. TRUSS UNITS SHALL BE IN PROPER ALIGNMENT AND FLANGE SURFACES SHALL BE SHOP BOLTED INTO FULL CONTACT BEFORE WELDING. SUFFICIENT EXTERNAL WELDS OR TACKS SHALL BE MADE TO SECURE FLANGES UNTIL REMAINING WELDS ARE MADE AFTER DISASSEMBLY. ADJACENT FLANGES SHALL BE "MATCH MARKED" TO INSURE PROPER FIELD ASSEMBLY.

PART ELEVATION VIEWS

| SPAN LENGTH (L) | CAMBER |
|-----------------|--------|
| 120' | 2 3/4" |
| 130' | 3 1/4" |
| 140' | 4" |
| 150' | 4 1/4" |
| 160' | 5" |



CAMBER DIAGRAM

- NOTE:
- FABRICATE TRUSS WITH CHORDS CURVED SMOOTHLY TO PROVIDE CAMBER.
 - DO NOT CAMBER BY SHIMMING AT TRUSS FIELD SPLICES OR CUTTING AND REWELDING CHORD.

TRUSS MEMBER SCHEDULE

| DESIGN TRUSS TYPE | SPAN | CHORDS | VERTICAL DIAGONALS, VERTICALS AND INTERIOR DIAGONALS | HORIZONTAL DIAGONALS | HORIZONTALS | SPLICING FLANGE | | |
|-------------------|------|-----------------|------------------------------------------------------|----------------------|-----------------|-----------------------|--------|--------------|
| | | | | | | H.S. BOLTS NO./SPLICE | DIA. | WELD SIZE W1 |
| 120-S | 120' | HSS 8.625x0.322 | PIPE 3 1/2" X-STRONG | PIPE 3 XX-STRONG | PIPE 3 X-STRONG | 8 | 1" | 1/4" |
| 130-S | 130' | HSS 8.625x0.375 | PIPE 3 1/2" X-STRONG | PIPE 3 XX-STRONG | PIPE 3 X-STRONG | 8 | 1" | 5/16" |
| 140-S | 140' | HSS 8.625x0.375 | PIPE 3 1/2" X-STRONG | PIPE 3 XX-STRONG | PIPE 3 X-STRONG | 8 | 1" | 5/16" |
| 150-S | 150' | HSS 8.625x0.500 | PIPE 3 1/2" X-STRONG | PIPE 3 XX-STRONG | PIPE 3 X-STRONG | 8 | 1" | 7/16" |
| 160-S | 160' | HSS 8.625x0.500 | PIPE 3 1/2" X-STRONG | PIPE 3 XX-STRONG | PIPE 3 X-STRONG | 8 | 1 1/4" | 7/16" |

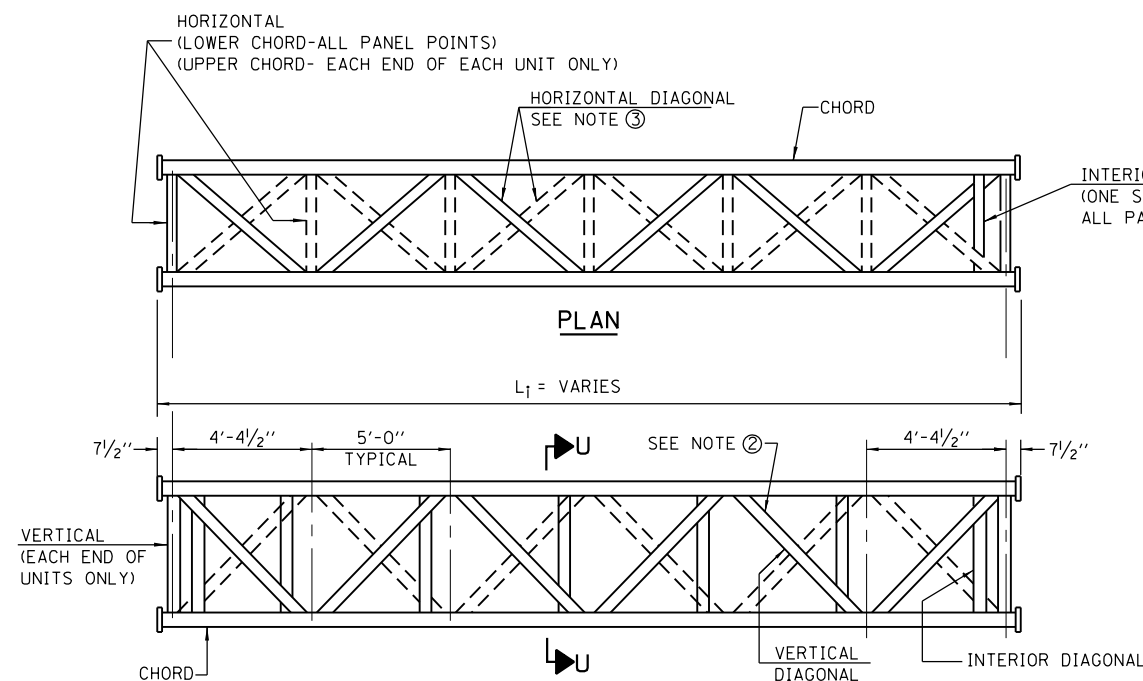
SHEET 2 OF 12



OVERHEAD SIGN STRUCTURE
SPAN TYPE (STEEL)
STRUCTURE DETAILS

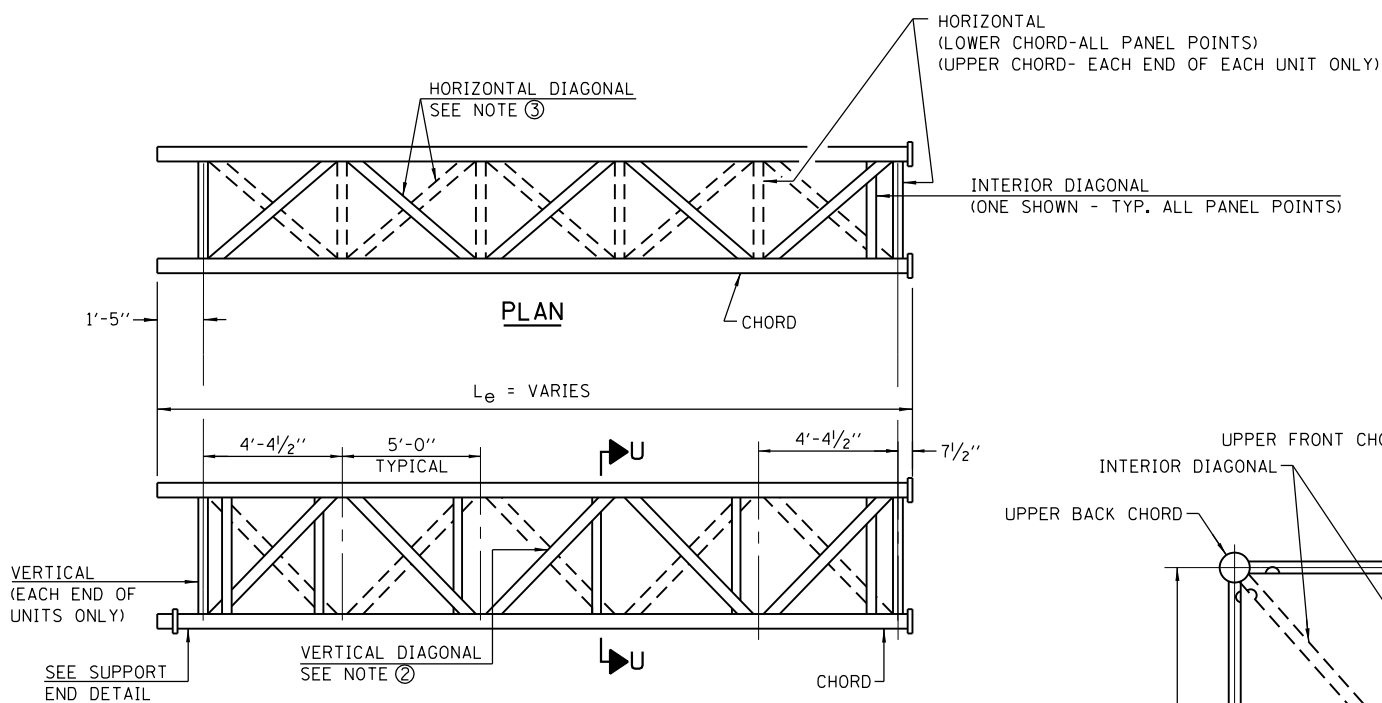
STANDARD F17-05

APPROVED: *Paul Kovacs* DATE 5-20-2014
CHIEF ENGINEERING OFFICER



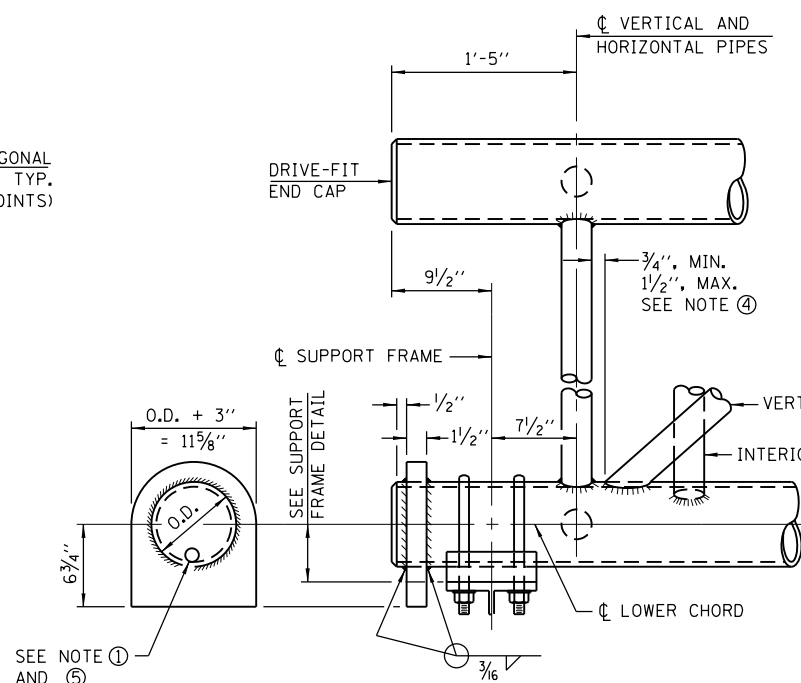
**ELEVATION
TYPICAL INTERIOR UNIT**

EVEN OR ODD NUMBER OF PANELS/EXTERIOR UNITS ALLOWED.

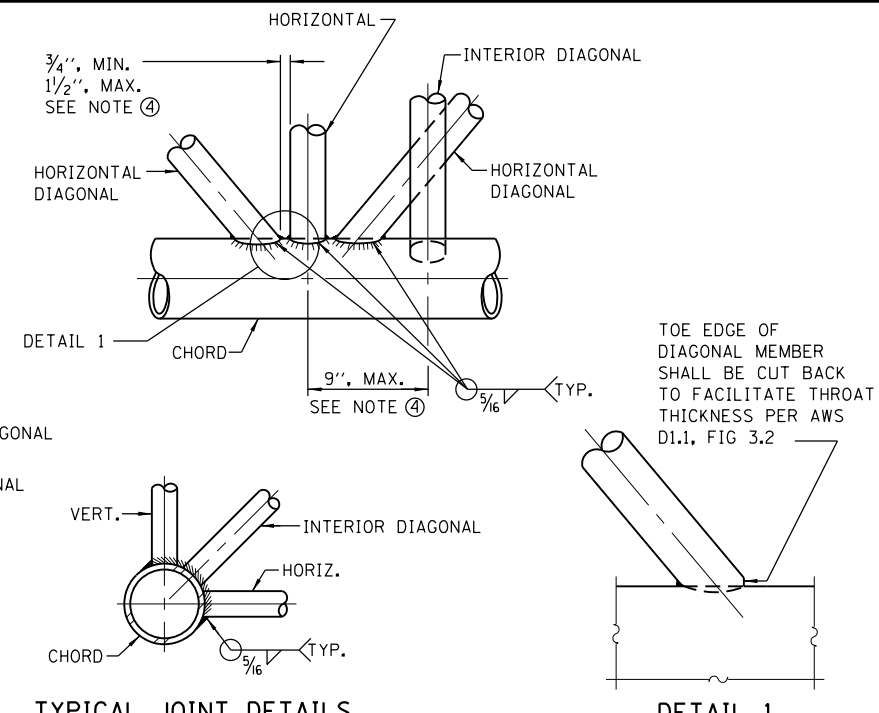


**ELEVATION
TYPICAL EXTERIOR UNIT**

EVEN OR ODD NUMBER OF PANELS/EXTERIOR UNITS ALLOWED.

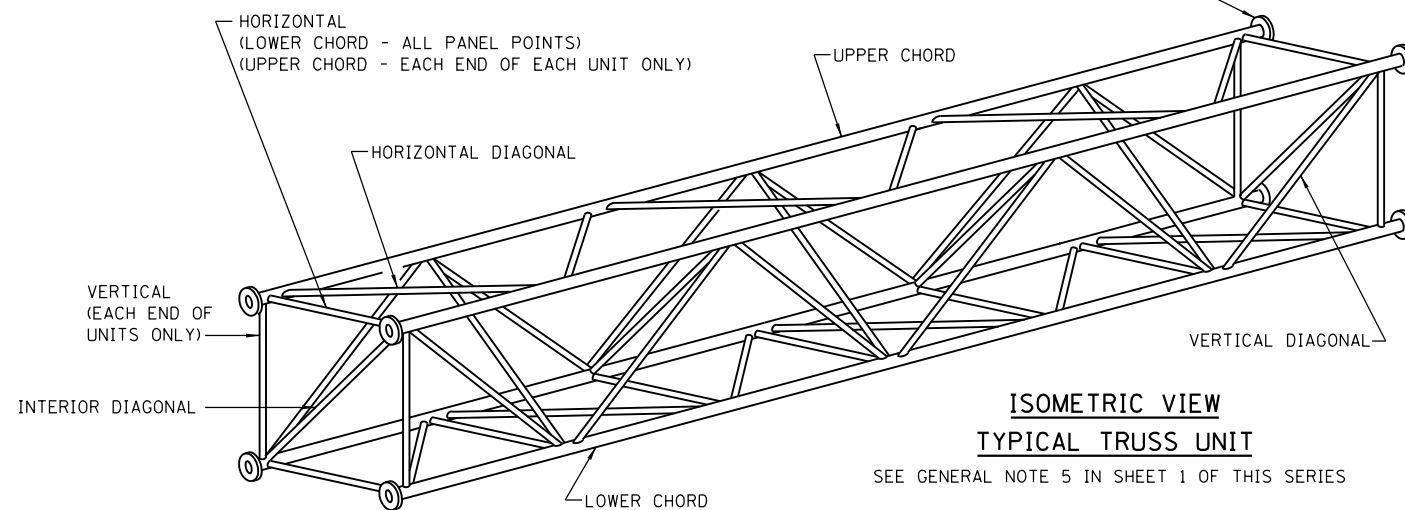


SUPPORT END DETAIL FOR EXTERIOR UNIT



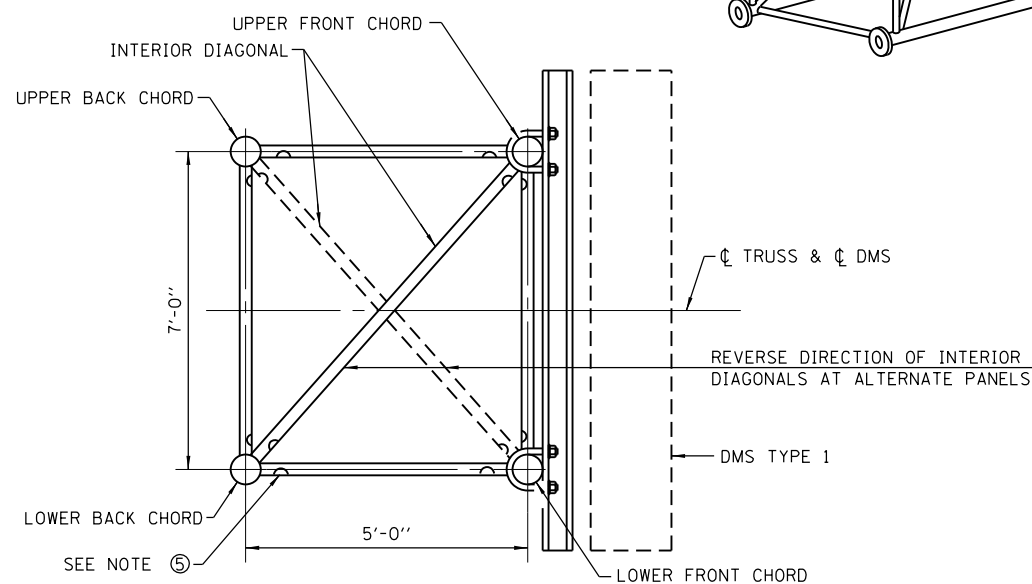
TYPICAL JOINT DETAILS

DETAIL 1



**ISOMETRIC VIEW
TYPICAL TRUSS UNIT**

SEE GENERAL NOTE 5 IN SHEET 1 OF THIS SERIES



**SECTION U-U
(VERTICAL AND HORIZONTAL DIAGONALS NOT SHOWN)**

NOTES

- CONTRACTOR SHALL USE STANDARD DRIVE-FIT CAP TO CLOSE END. 1/2" Ø DRAIN HOLE IN DRIVE-FIT CAP INSTALLED AFTER GALVANIZING. (TYP. AT NON-SPLICE ENDS OF CHORDS)
- VERTICAL DIAGONALS IN FRONT AND BACK FACE SHALL ALTERNATE INCLINATION.
- HIDDEN LINES SHOW WIND BRACING ALTERNATES DIRECTION BETWEEN PLANES OF TOP AND BOTTOM CHORDS.
- ALL DIAGONALS SHALL BE OFFSET FROM THE PANEL POINT BASED ON THE FOLLOWING: OFFSET SHALL PROVIDE A 3/4" MINIMUM TO 1 1/2" MAXIMUM CLEARANCE BETWEEN DIAGONAL AND ANY OTHER DIAGONAL, HORIZONTAL OR VERTICAL MEMBER, AND TO PROVIDE CLEARANCE FOR U-BOLT CONNECTIONS OF DMS TYPE 1 OR WALKWAY BRACKETS.
- GALVANIZING VENT HOLES OF ADEQUATE SIZE SHALL BE PROVIDED ON UNDERSIDE AT EACH END OF TRUSS MEMBERS EXCEPT CHORDS. ALTERNATELY, HOLES MAY BE PROVIDED IN WALL OF CHORDS. ALL VENT HOLES SHALL BE DRILLED AND DE-BURRED, TYP.

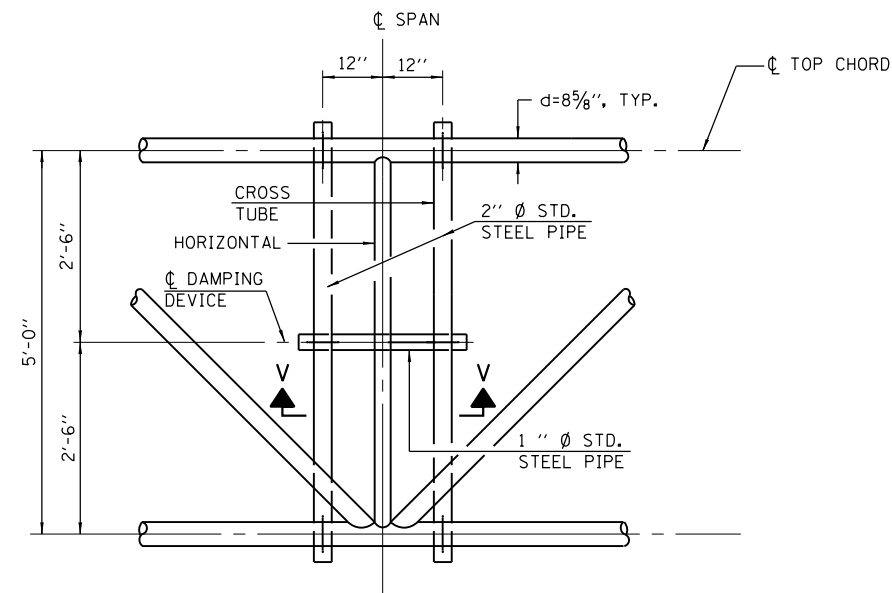
SHEET 3 OF 12



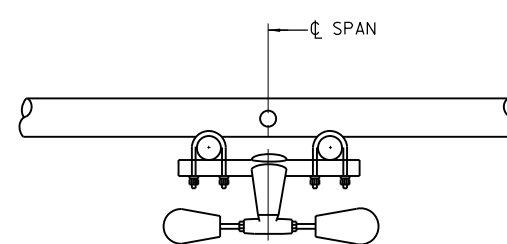
OVERHEAD SIGN STRUCTURE
SPAN TYPE (STEEL)
STRUCTURE DETAILS

STANDARD F17-05

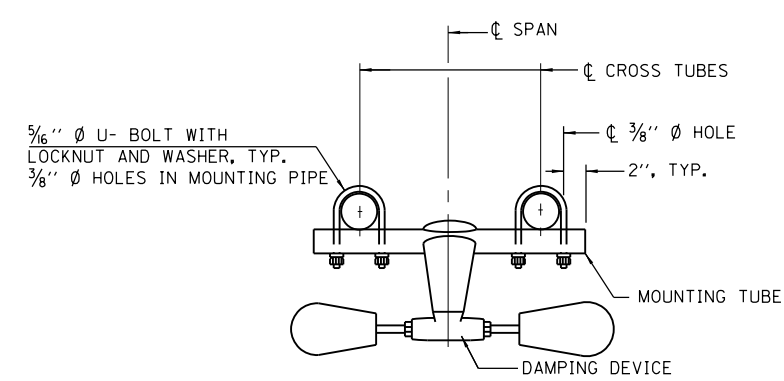
APPROVED..... DATE 5-20-2014
CHIEF ENGINEERING OFFICER



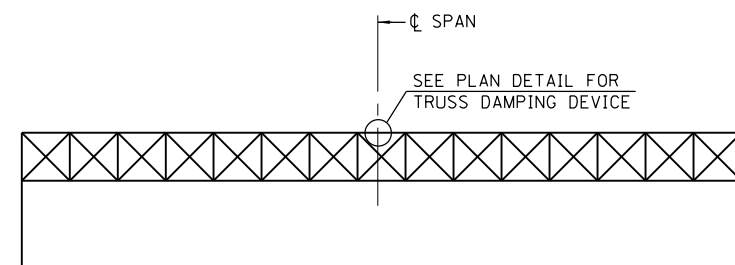
PLAN DETAIL
CL SPAN AT PANEL POINTS



SECTION V-V

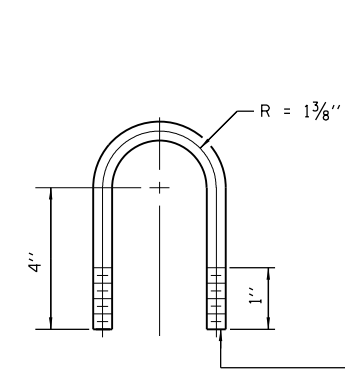


**TRUSS DAMPING
DEVICE CONNECTION DETAIL**
(TYPICAL)

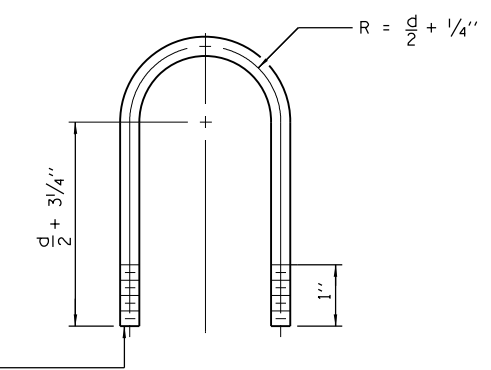


ELEVATION
STEEL OVERHEAD
SIGN TRUSS

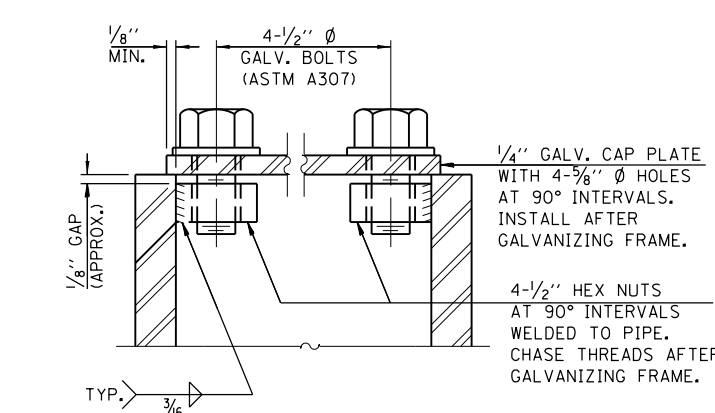
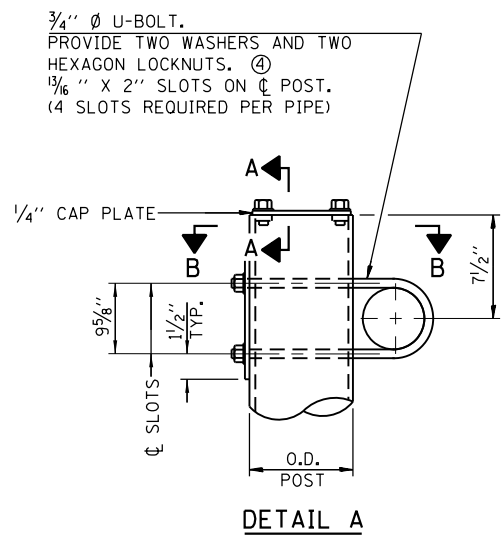
DAMPER NOTE:
ONE DAMPER PER TRUSS. (31 LBS. STOCKBRIDGE-TYPE - 29" MINIMUM BETWEEN ENDS OF WEIGHTS).



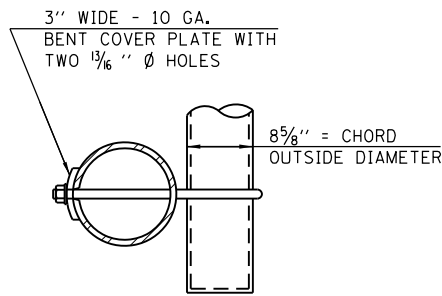
**DAMPING DEVICE MOUNTING
TUBE U-BOLT DETAIL**
(TYPICAL)



**TOP CHORD TO CROSS TUBE
U-BOLT DETAIL**
(TYPICAL)



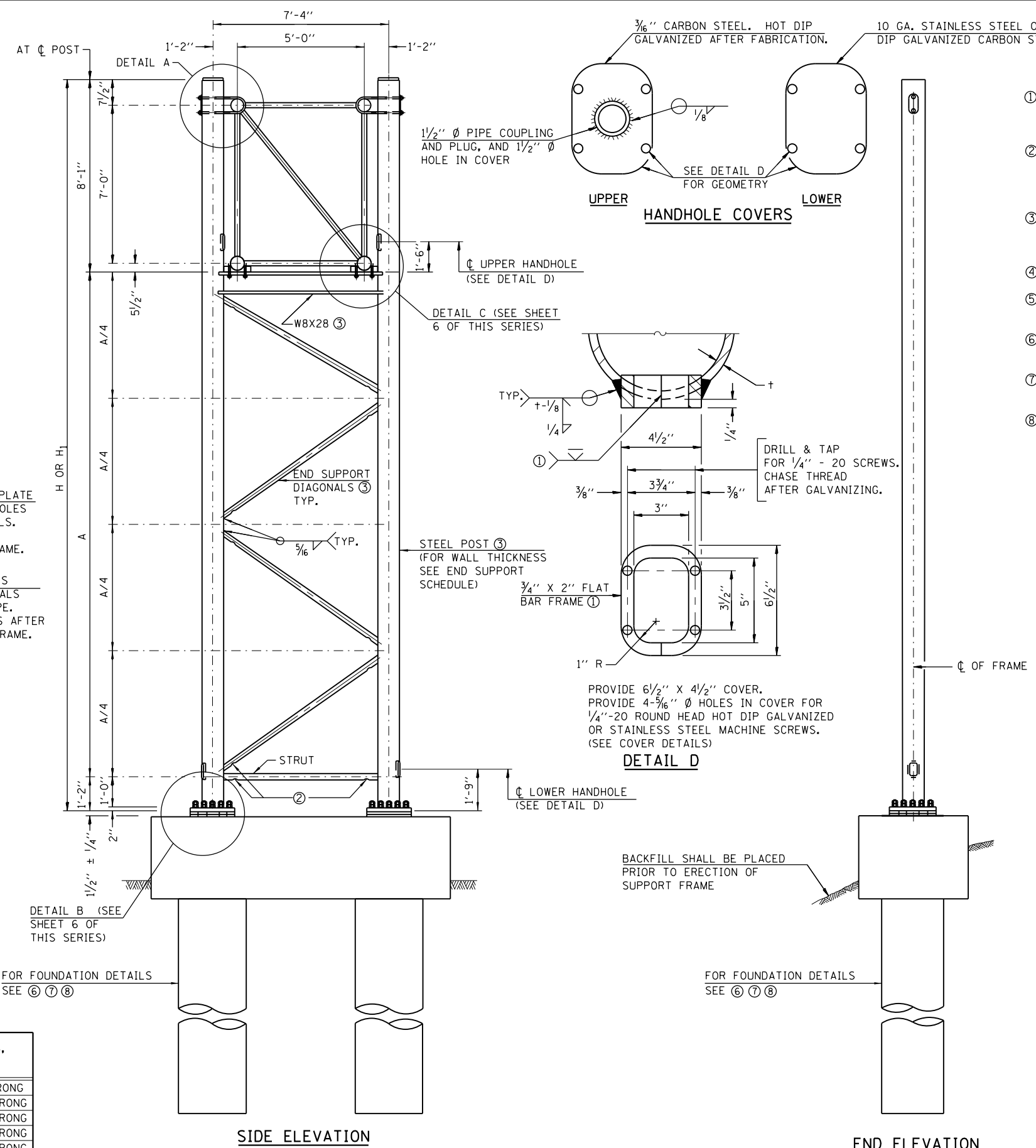
AS AN ALTERNATE TO BOLTS, MAY USE GALVANIZED
DRIVE-FIT CAPS INSTALLED AFTER GALVANIZING FRAME.



SECTION B-B

END SUPPORT SCHEDULE

| DESIGN TRUSS TYPE | H OR H ₁ | + | POSTS | DIAGONALS, STRUT |
|-------------------------|---------------------|------|-----------------|---------------------|
| 120-S | 34' MAX. | 1/2" | HSS 12.75x0.500 | PIPE 5 X-STRONG |
| 130-S | 34' MAX. | 1/2" | HSS 14x0.500 | PIPE 5 XX-STRONG |
| 140-S | 34' MAX. | 1/2" | HSS 14x0.500 | PIPE 5 XX-STRONG |
| 150-S | 36' MAX. | 1/2" | HSS 16x0.500 | PIPE 5 XX-STRONG |
| 160-S | 36' MAX. | 1/2" | HSS 16x0.500 | PIPE 5 XX-STRONG |



NOTES:

- IN LIEU OF FABRICATED HANDHOLE FRAME AS SHOWN, MAY CUT FROM 2" PLATE (ROLLING DIRECTION VERTICAL). ALL CUT FACES TO BE GROUND TO ANSI ROUGHNESS OF 500 µIN OR LESS.
- GALVANIZING VENT HOLES OF ADEQUATE SIZE SHALL BE PROVIDED ON UNDERSIDE AT EACH END OF BRACING PIPES. ALTERNATELY, HOLES MAY BE PROVIDED IN WALL OF PIPE COLUMN. ALL VENT HOLES SHALL BE DRILLED AND DE-BURRED, TYP.
- STEEL PIPE, PLATE, CARBON STEEL HANDHOLE COVERS AND ROLLED SECTIONS SHALL BE HOT DIP GALVANIZED AFTER FABRICATION. PAINTING IS NOT PERMITTED. SEE SHEET 1 OF THIS SERIES.
- SEE GENERAL NOTES FOR FASTENERS.
- NONSTANDARD APPLICATIONS SHALL HAVE DIMENSIONS VERIFIED OR AMENDED AS APPROPRIATE.
- SEE SHEET 7 OF THIS SERIES FOR SHOULDER TYPE FOUNDATION DETAILS.
- SEE SHEET 8 OF THIS SERIES FOR MEDIAN BARRIER TYPE FOUNDATION DETAILS.
- SEE SHEET 9 OF THIS SERIES FOR MEDIAN BARRIER TYPE FOUNDATION DETAILS WHEN EXISTING UTILITY IS PRESENT.

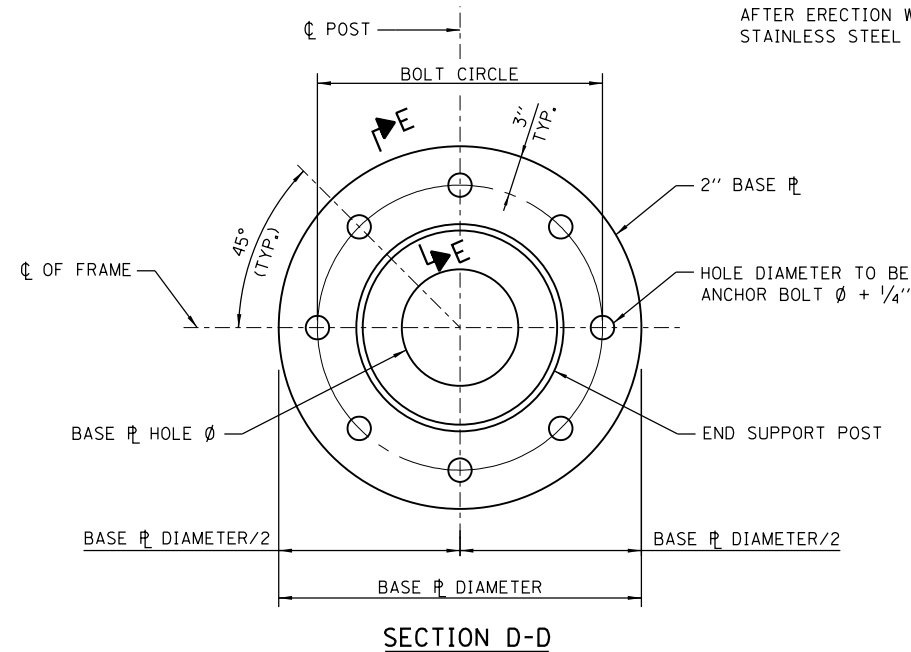
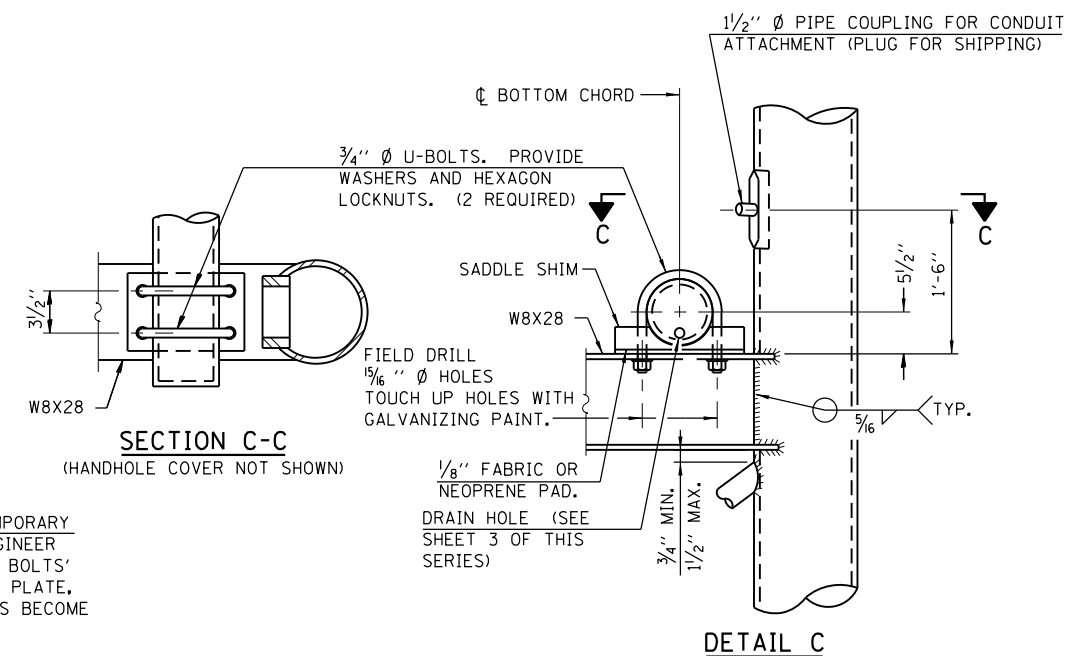
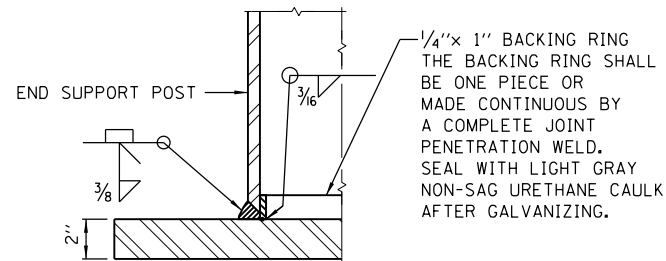
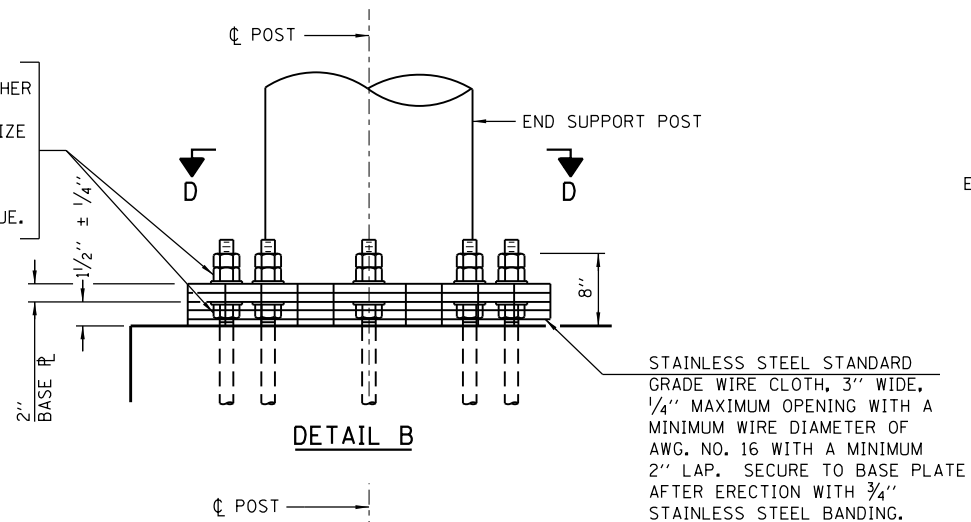
END SUPPORT DETAILS



OVERHEAD SIGN STRUCTURE
SPAN TYPE (STEEL)
STRUCTURE DETAILS

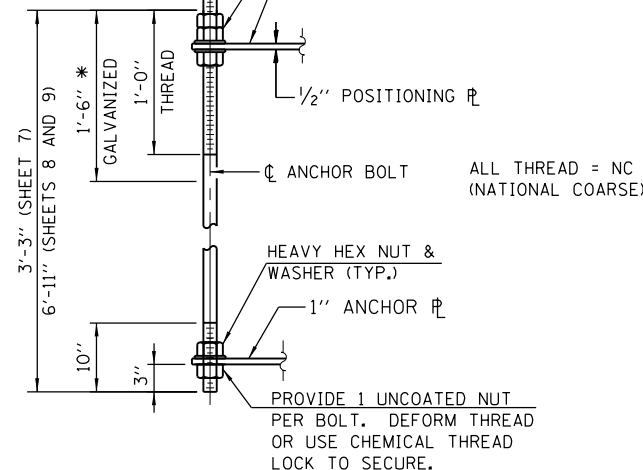
STANDARD F17-05

HEXAGON LOCKNUT AND WASHER (TOP), LEVELING NUT AND WASHER (BOTTOM). GALVANIZE PER AASHTO M232. NUTS SHALL EACH BE TIGHTENED AGAINST BASE PLATE WITH 200 LB.-FT. MINIMUM TORQUE.



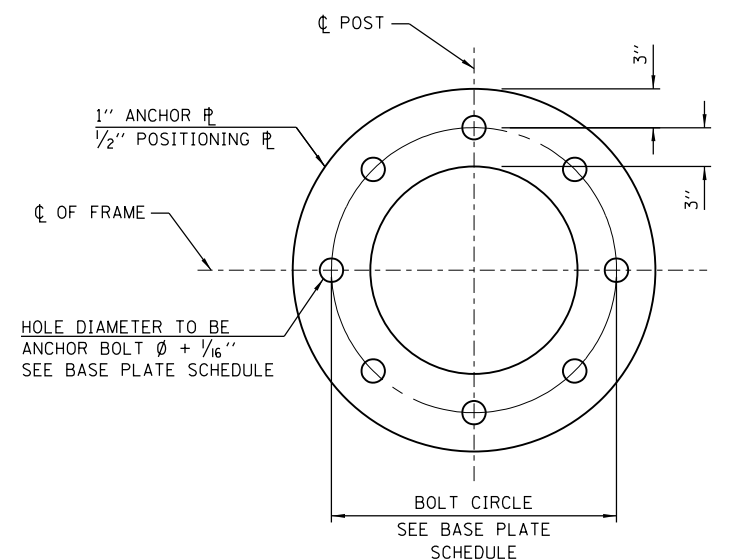
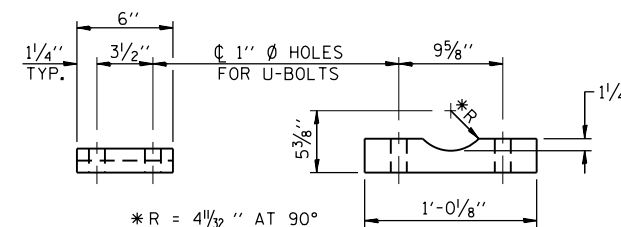
FOR UT, GRIND TOP OF BOLT SQUARE AND SMOOTH BEFORE GALVANIZING.

UTILIZE 1/2" POSITIONING PLATE AND TEMPORARY NUTS WITH LEVELING NUTS OR OTHER ENGINEER APPROVED METHODS TO MAINTAIN ANCHOR BOLTS' ALIGNMENT DURING CONCRETE PLACEMENT. PLATE, EXTRA NUTS AND OTHER POSITIONING AIDS BECOME CONTRACTOR'S PROPERTY.



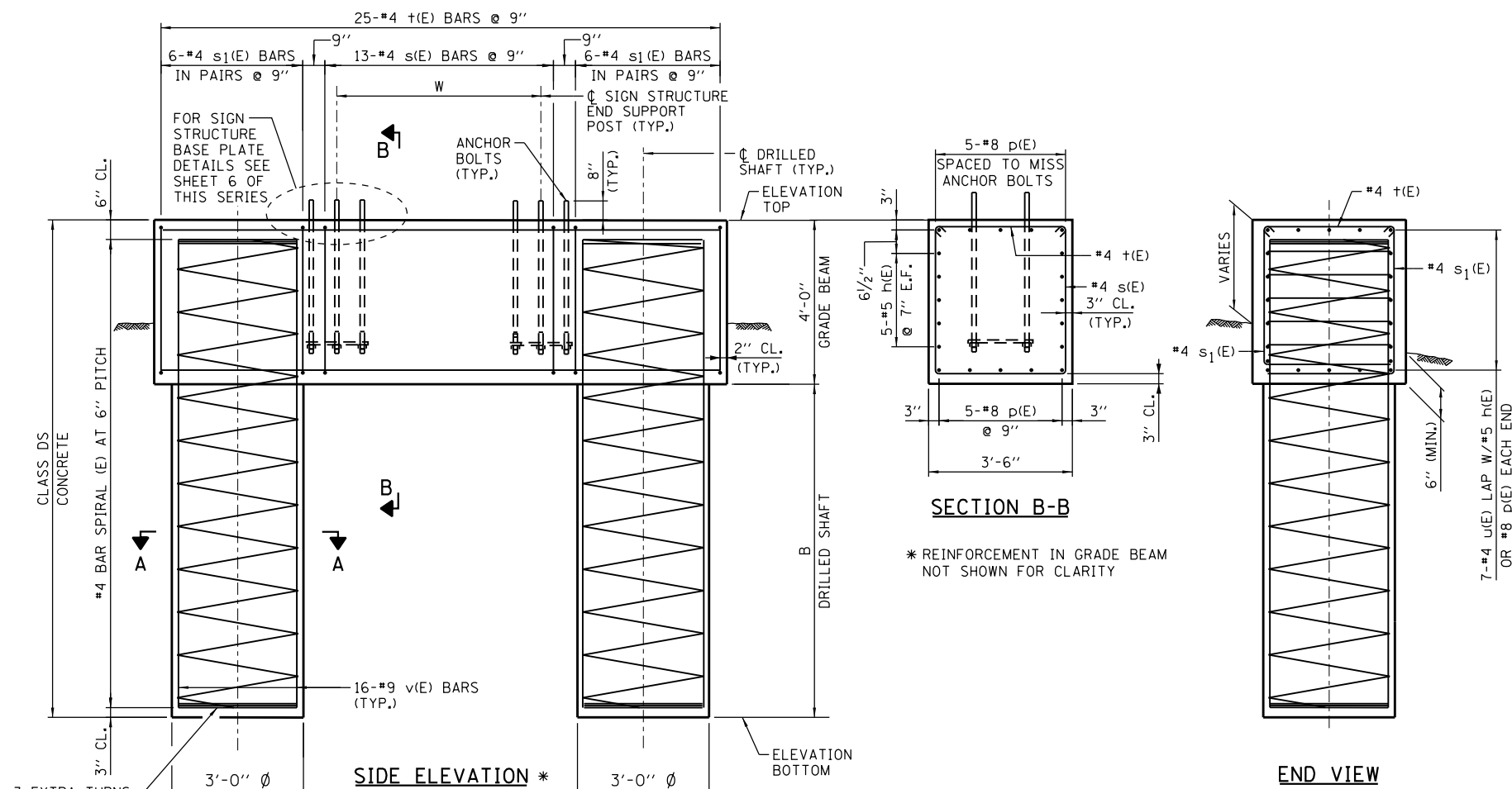
ANCHOR BOLTS SHALL CONFORM TO AASHTO M314 GRADE 105 AND MEET CHARPY V-NOTCH (CVN) ENERGY OF 15 LB.-FT. AT 40° F. GALVANIZE UPPER 18" PER AASHTO M232. NO WELDING SHALL BE PERMITTED ON BOLTS.

* 18" IS MINIMUM TO BE GALVANIZED. ENTIRE BOLT MAY BE GALVANIZED AT CONTRACTOR'S OPTION.



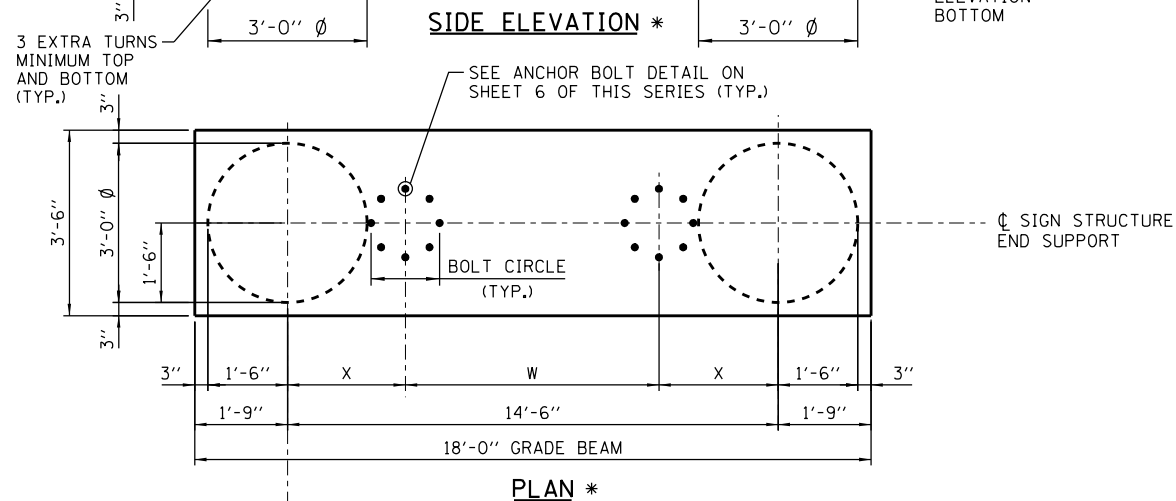
BASE PLATE SCHEDULE

| DESIGN TRUSS TYPE | END SUPPORT POST OUTSIDE DIAMETER | BASE PLATE | | BOLT CIRCLE | ANCHOR BOLT DIA. |
|-------------------|-----------------------------------|------------|--------|-------------|------------------|
| | | DIAMETER | HOLE Ø | | |
| 120-S | 1'-0 3/4" | 2'-0 3/4" | 6.75" | 1'-6 3/4" | 1 1/2" |
| 130-S | 14" | 2'-2" | 8" | 1'-8" | 1 1/2" |
| 140-S | 14" | 2'-2" | 8" | 1'-8" | 1 1/2" |
| 150-S | 16" | 2'-4" | 8" | 1'-10" | 1 1/2" |
| 160-S | 16" | 2'-4" | 8" | 1'-10" | 1 3/4" |



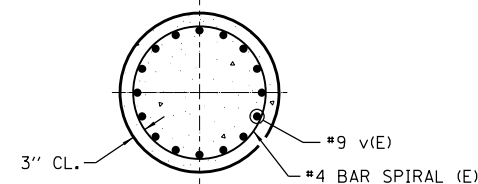
SECTION B-B

* REINFORCEMENT IN GRADE BEAM
NOT SHOWN FOR CLARITY



PLAN *

SITE GROUNDING ELECTRODE SYSTEM
TO BE PROVIDED AS DETAILED ON
PLANS.



SECTION A-A
(TYPICAL BOTH SHAFTS)

NOTES:

1. THE FOUNDATION DETAILS SHOWN ARE BASED ON THE PRESENCE OF MOSTLY COHESIVE SOIL CONDITIONS (SILTY OR SANDY CLAY), WITH AN AVERAGE UNCONFINED COMPRESSIVE STRENGTH (QU) > 1.25 TON/SQ. FT. WHICH SHALL BE DETERMINED BY PREVIOUS SOIL INVESTIGATIONS AT THE JOBSITE. WHEN OTHER CONDITIONS ARE INDICATED, THE BORING DATA SHALL BE INCLUDED IN THE PLANS AND THE FOUNDATION DIMENSIONS SHOWN SHALL BE THE RESULT OF SITE SPECIFIC DESIGNS. IF CONDITIONS ENCOUNTERED IN THE FIELD ARE DIFFERENT THAN THOSE INDICATED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER TO DETERMINE IF THE FOUNDATION DIMENSIONS NEED TO BE MODIFIED.
2. ALL MATERIAL, FABRICATION, AND CONSTRUCTION REQUIREMENTS SHALL BE IN ACCORDANCE WITH SECTION 734 OF THE ILLINOIS TOLLWAY SUPPLEMENTAL SPECIFICATIONS.
3. CONCRETE SHALL BE PLACED MONOLITHICALLY, WITHOUT CONSTRUCTION JOINTS UNLESS NOTED OTHERWISE.
4. BACKFILL SHALL BE PLACED PER SECTION 502 OF THE IDOT STANDARD SPECIFICATION AND PRIOR TO ERECTION OF END SUPPORT POST.
5. PROVIDE NORMAL SURFACE FINISH, FOLLOWED BY CONCRETE SEALER APPLICATION ON ALL CONCRETE SURFACES EXCEPT BOTTOM OF GRADE BEAM AND DRILLED SHAFTS.
6. ALL REBAR DESIGNATED (E) SHALL BE EPOXY COATED. REBAR SHALL BE POSITIONED SO THAT THERE WILL BE NO INTERFERENCE BETWEEN VERTICAL REINFORCEMENT AND ANCHOR BOLTS.
7. NO SONOTUBES OR DECOMPOSABLE FORMS SHALL BE USED 6" BELOW THE FINISHED GROUND LINE. PERMANENT METAL FORMS OR OTHER SHIELDING SHALL NOT BE LEFT IN PLACE BELOW THE ELEVATION WITHOUT THE ENGINEER'S WRITTEN PERMISSION. EXCAVATIONS SHALL BE DEWATERED BEFORE CONCRETE PLACEMENT IF DIRECTED BY THE ENGINEER AT NO ADDITIONAL COST.
8. IF NECESSARY TO INCREASE STEEL END SUPPORT HEIGHT ABOVE THE LIMITATIONS SHOWN IN SIGN STRUCTURE MEMBER SCHEDULE ON SHEET 5 OF THIS SERIES, GRADE BEAM DEPTH ON THIS SHEET SHALL BE INCREASED UP TO 6'-0" WITHOUT CHANGES TO THE DRILLED SHAFT DESIGN, GRADE BEAM REINFORCEMENT, CONCRETE VOLUME AND LENGTH OF ANCHOR BOLTS SHALL BE REVISED ACCORDINGLY.

BAR LIST - EACH FOUNDATION

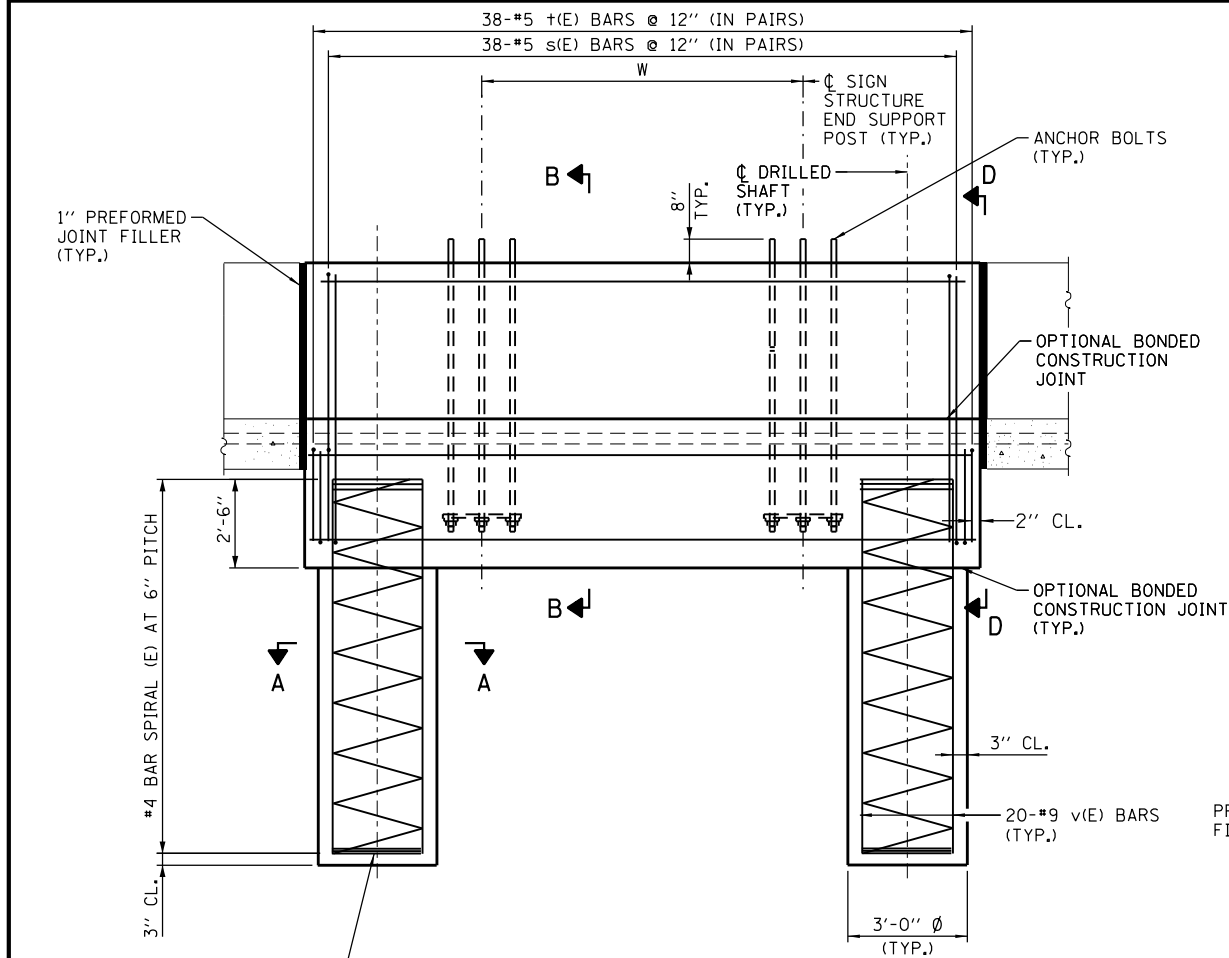
(2 SHAFT AND 1 GRADE BEAM)

| BAR | NUMBER | SIZE | LENGTH | SHAPE |
|--------------------|--------|------|--------------|-------|
| h(E) | 10 | #5 | 17'-8" | == |
| p(E) | 10 | #8 | 17'-8" | == |
| s(E) | 13 | #4 | 11'-1" | U |
| s ₁ (E) | 24 | #4 | 6'-11 1/2" | U |
| t(E) | 25 | #4 | 3'-11" | U |
| u(E) | 14 | #4 | 7'-0" | U |
| v(E) | 32 | #9 | B SIDE 3'-3" | == |

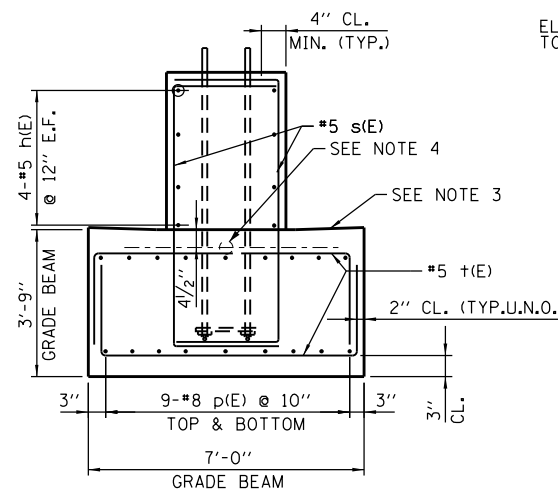
#4 BAR SPIRAL (E) - SEE SIDE ELEVATION

SHOULDER FOUNDATION SCHEDULE

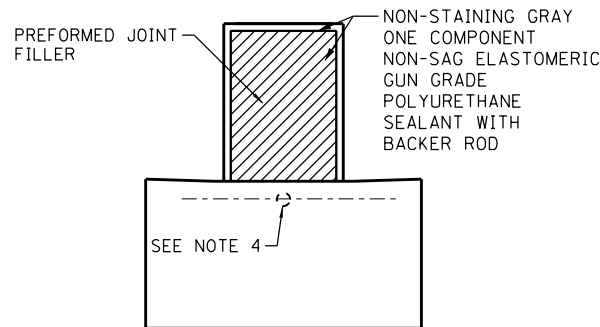
| DESIGN TRUSS TYPE | W | X | B | CLASS DS CONCRETE (CU YD) | REINFORCEMENT BARS (POUNDS) |
|-------------------|-------|-------|--------|---------------------------|-----------------------------|
| 120-S | 7'-4" | 3'-7" | 50'-0" | 35.5 | 7960 |
| 130-S | 7'-4" | 3'-7" | 55'-0" | 38.1 | 8600 |
| 140-S | 7'-4" | 3'-7" | 55'-0" | 38.1 | 8600 |
| 150-S | 7'-4" | 3'-7" | 55'-0" | 38.1 | 8600 |
| 160-S | 7'-4" | 3'-7" | 55'-0" | 38.1 | 8600 |



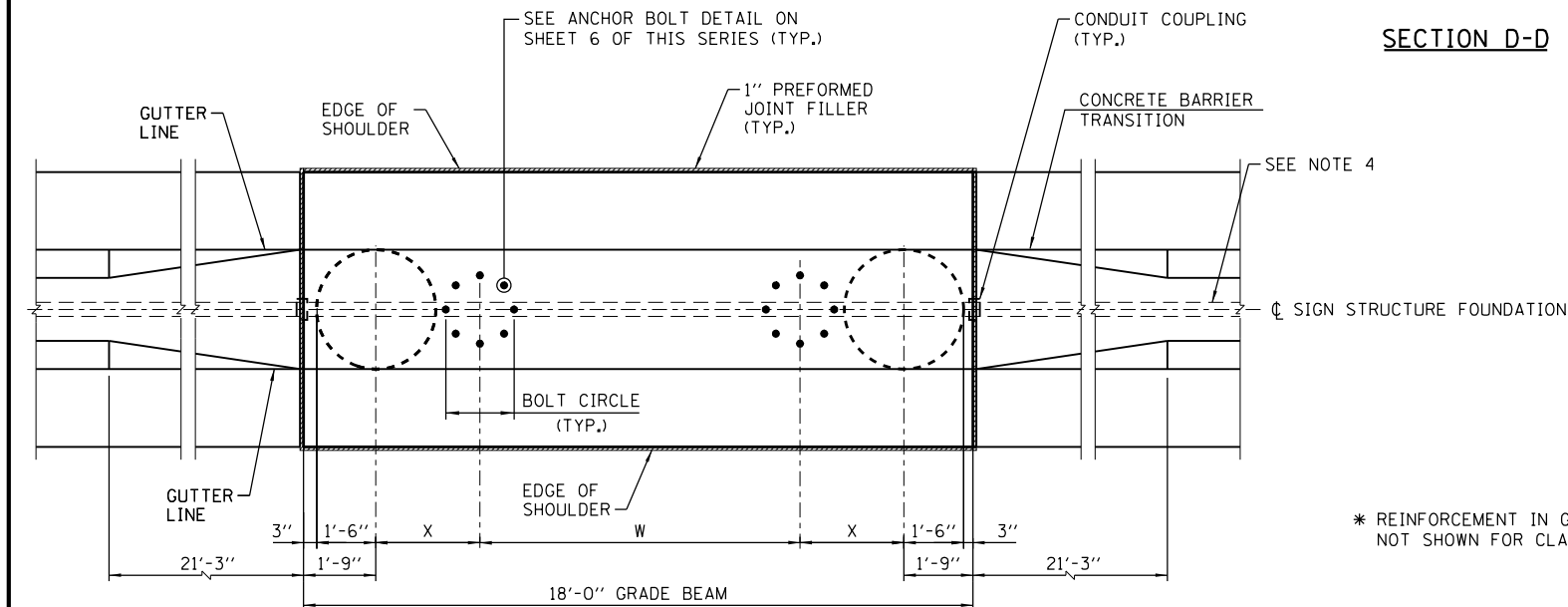
SIDE ELEVATION *



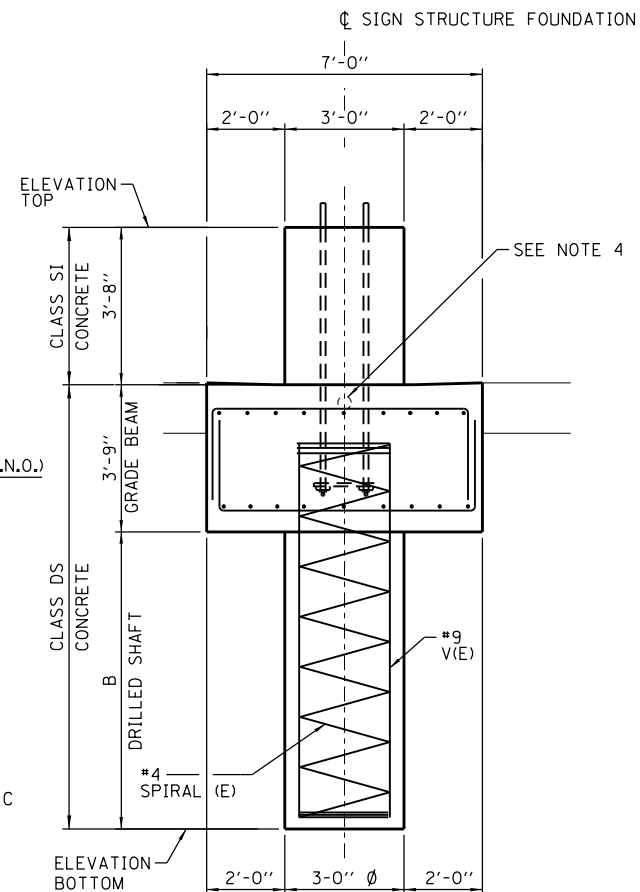
SECTION B-B



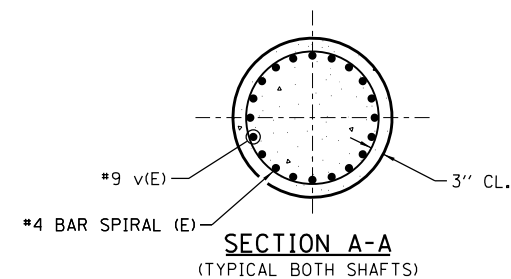
SECTION D-D



PLAN *



END VIEW

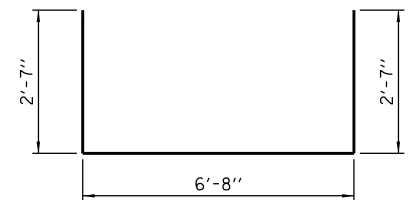


SECTION A-A
(TYPICAL BOTH SHAFTS)

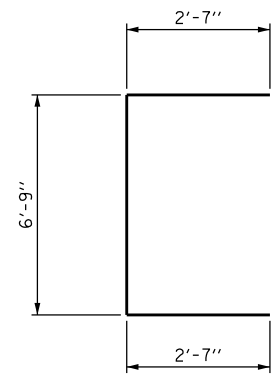
BAR LIST - EACH FOUNDATION

| BAR | NUMBER | SIZE | LENGTH | SHAPE |
|------|--------|------|-------------|-------|
| h(E) | 8 | #5 | 17'-8" | |
| p(E) | 18 | #8 | 17'-8" | |
| s(E) | 38 | #5 | 11'-3" | C |
| t(E) | 38 | #5 | 11'-10" | U |
| v(E) | 40 | #9 | B ADD 2'-3" | |

*4 BAR SPIRAL (E) - SEE SIDE ELEVATION



BAR t(E)



BAR s(E)

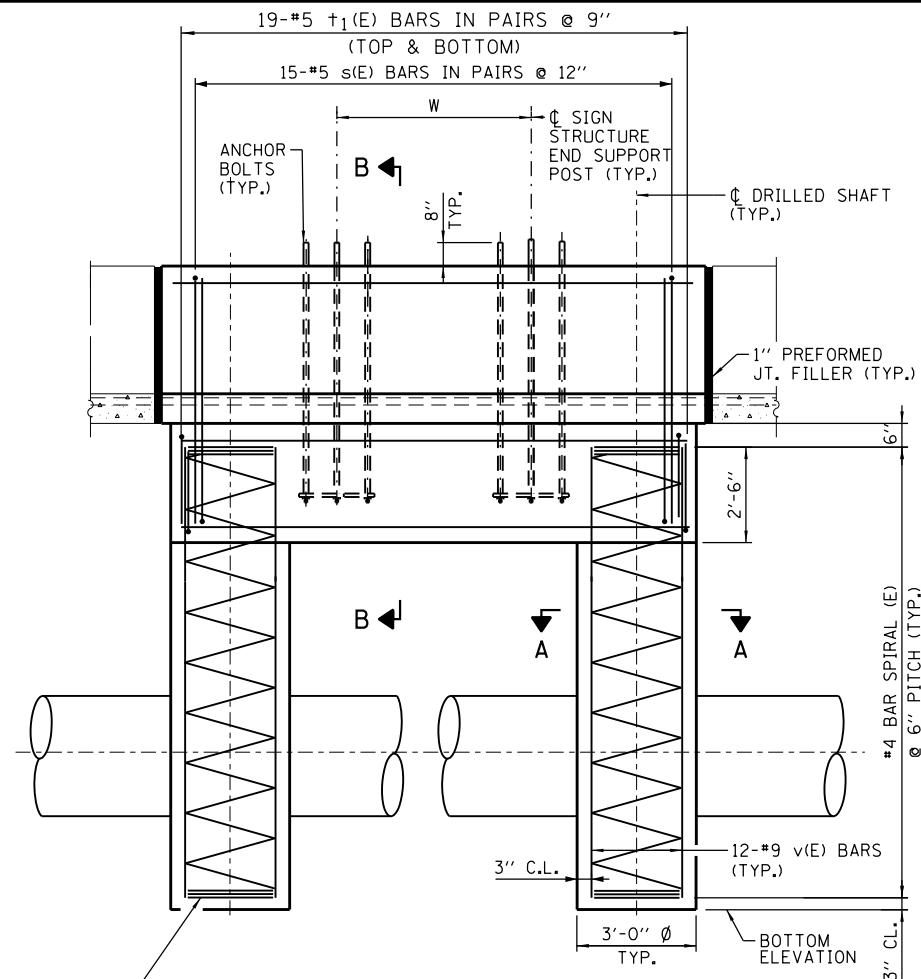
MEDIAN BARRIER FOUNDATION SCHEDULE

| DESIGN TRUSS TYPE | W | X | B | CLASS SI CONCRETE (CU YD) | CLASS DS CONCRETE (CU YD) | REINFORCEMENT BARS (POUNDS) | PROTECTIVE COAT (SQ YD) |
|-------------------|-------|-------|--------|---------------------------|---------------------------|-----------------------------|-------------------------|
| 120-S | 7'-4" | 3'-7" | 55'-0" | 7.4 | 46.3 | 10970 | 28.0 |
| 130-S | 7'-4" | 3'-7" | 55'-0" | 7.4 | 46.3 | 10970 | 28.0 |
| 140-S | 7'-4" | 3'-7" | 60'-0" | 7.4 | 48.9 | 12850 | 28.0 |
| 150-S | 7'-4" | 3'-7" | 65'-0" | 7.4 | 51.5 | 13630 | 28.0 |
| 160-S | 7'-4" | 3'-7" | 65'-0" | 7.4 | 51.5 | 13630 | 28.0 |

NOTES:

- SEE SHEET 7 OF THIS SERIES FOR FOUNDATION NOTES AND DESIGN CRITERIA.
- FOR SIGN STRUCTURE BASE PLATE DETAIL, SEE SHEET 6 OF THIS SERIES.
- REFERENCE ILLINOIS TOLLWAY STANDARD DRAWING C5 FOR GUTTER SLOPE.
- COORDINATE CONDUIT SIZE, LOCATION AND QUANTITY WITH ELECTRICAL PLANS. CONDUITS SHALL BE PLACED TO MISS REINFORCEMENT BARS. DO NOT CUT REINFORCEMENT BARS.
- PROTECTIVE COAT SHALL BE APPLIED TO THE TRAFFIC AND TOP FACES OF BARRIER AND TOP FACE OF GUTTER.

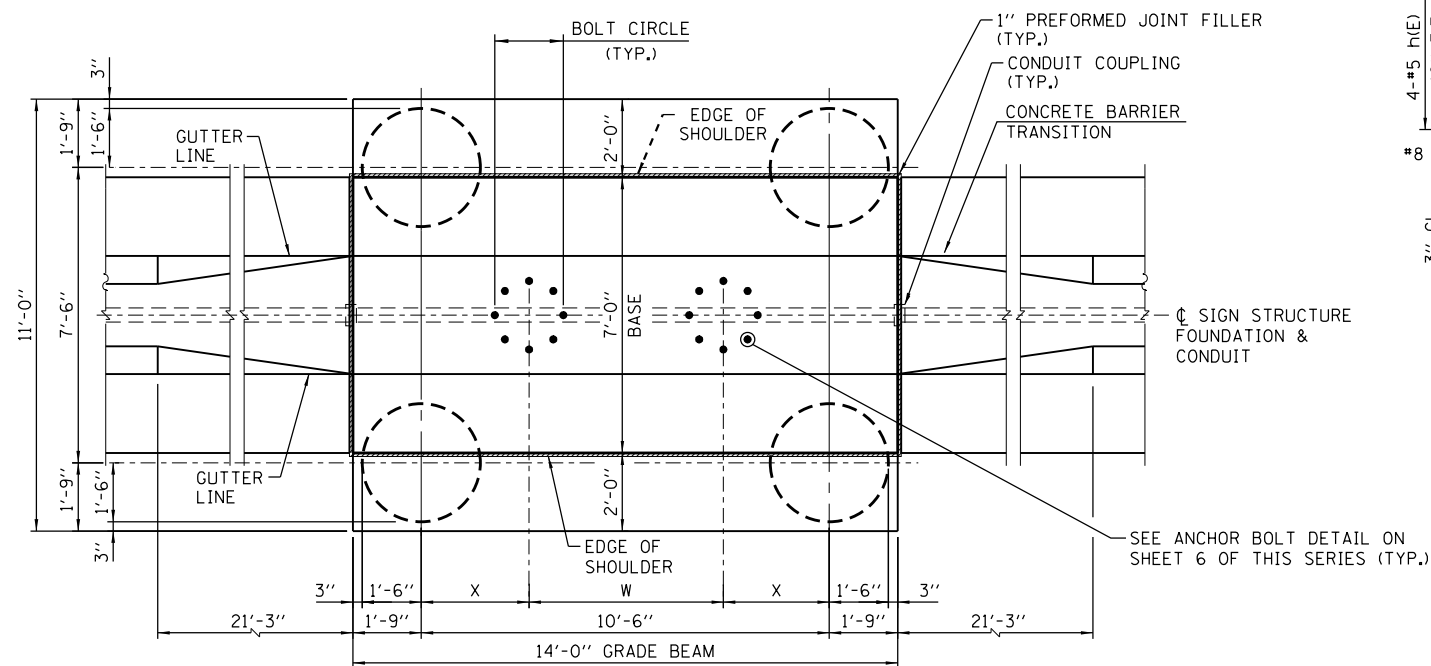




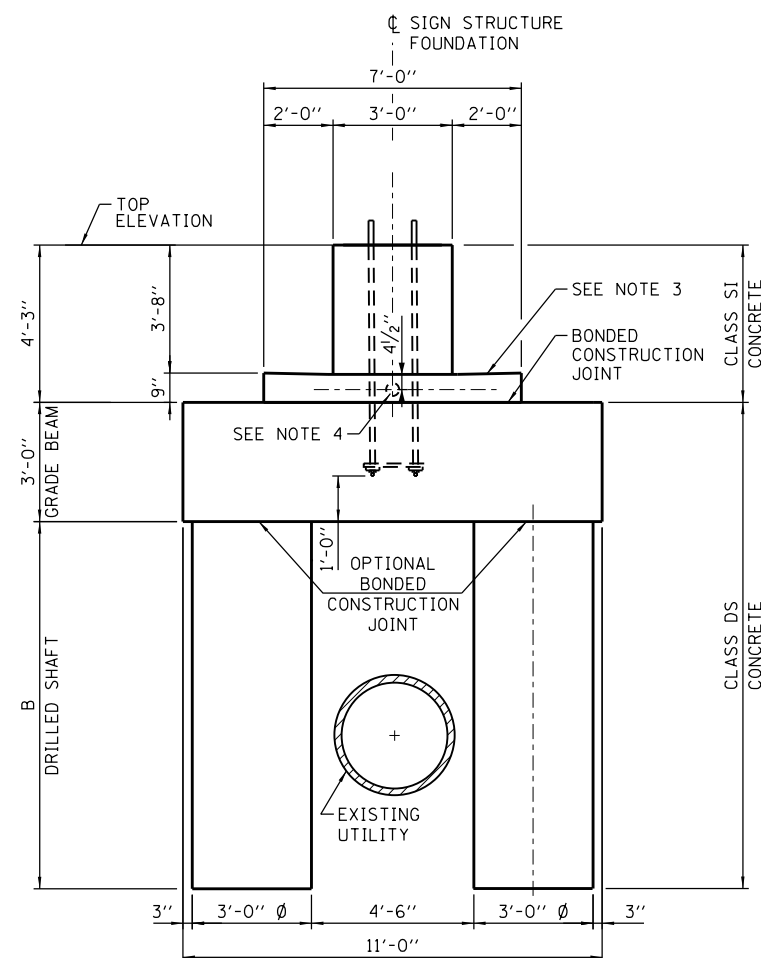
SIDE ELEVATION *

* REINFORCEMENT IN GRADE BEAM NOT SHOWN FOR CLARITY

SITE GROUNDING ELECTRODE SYSTEM TO BE PROVIDED AS DETAILED ON PLANS.



PLAN *



END VIEW

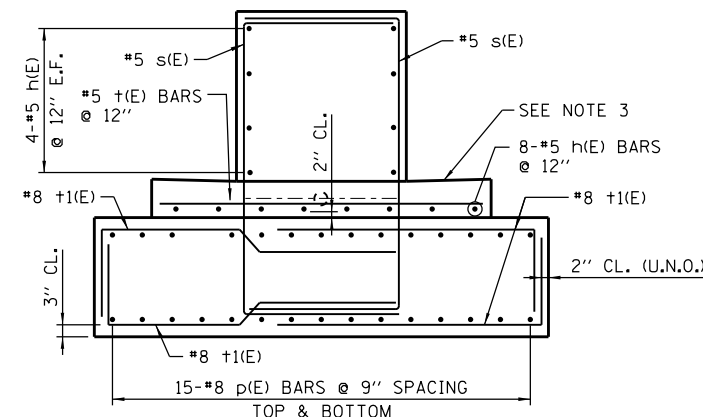
BAR LIST - EACH FOUNDATION

| BAR | NUMBER | SIZE | LENGTH | SHAPE |
|-------|--------|------|-------------|-------|
| h(E) | 16 | #5 | 13'-8" | — |
| p(E) | 30 | #8 | 13'-8" | — |
| s(E) | 30 | #5 | 11'-3" | C |
| t(E) | 15 | #5 | 6'-8" | — |
| +1(E) | 76 | #8 | 12'-7" | — |
| v(E) | 48 | #9 | B ADD 2'-3" | — |

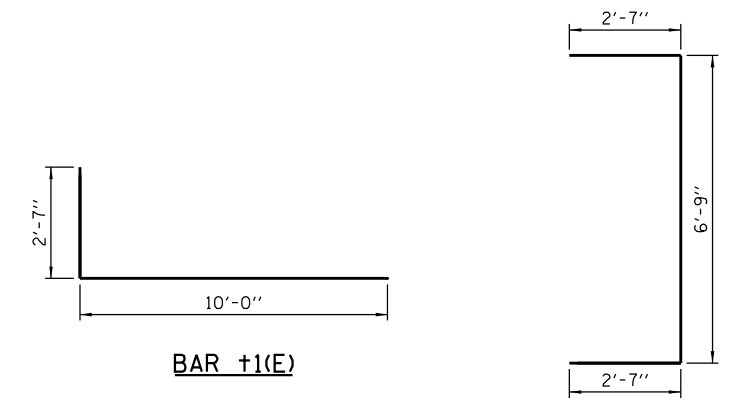
#4 BAR SPIRAL (E) - SEE SIDE ELEVATION

MEDIAN BARRIER FOUNDATION SCHEDULE

| DESIGN TRUSS TYPE | W | X | B | CLASS SI CONCRETE (CU YD) | CLASS DS CONCRETE (CU YD) | REINFORCEMENT BARS (POUNDS) | PROTECTIVE COAT (SQ YD) |
|-------------------|-------|-------|--------|---------------------------|---------------------------|-----------------------------|-------------------------|
| 120-S | 7'-4" | 1'-7" | 40'-0" | 8.5 | 59.0 | 13120 | 22.0 |
| 130-S | 7'-4" | 1'-7" | 40'-0" | 8.5 | 59.0 | 13120 | 22.0 |
| 140-S | 7'-4" | 1'-7" | 45'-0" | 8.5 | 64.2 | 14150 | 22.0 |
| 150-S | 7'-4" | 1'-7" | 50'-0" | 8.5 | 69.5 | 15170 | 22.0 |
| 160-S | 7'-4" | 1'-7" | 50'-0" | 8.5 | 69.5 | 15170 | 22.0 |

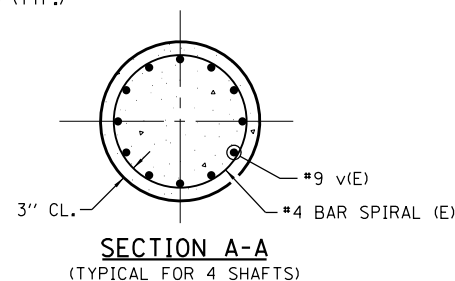


SECTION B-B



BAR +1(E)

BAR s(E)



SECTION A-A

(TYPICAL FOR 4 SHAFTS)

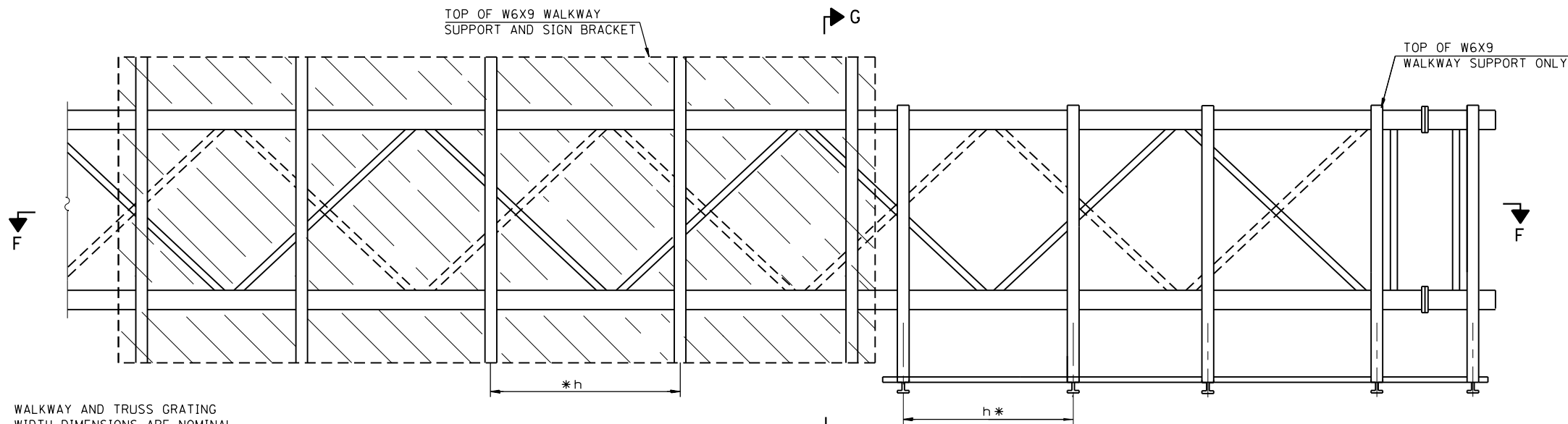
NOTES:

- SEE SHEET 7 FOR FOUNDATION NOTES AND DESIGN CRITERIA.
- FOR SIGN STRUCTURE BASE PLATE DETAIL, SEE SHEET 6 OF THIS SERIES.
- REFERENCE ILLINOIS TOLLWAY STANDARD DRAWING C5 FOR GUTTER SLOPE.
- COORDINATE CONDUIT SIZE, LOCATION AND QUANTITY WITH ELECTRICAL PLANS. CONDUITS SHALL BE PLACED TO MISS REINFORCEMENT BARS. DO NOT CUT REINFORCEMENT BARS.
- PROTECTIVE COAT SHALL BE APPLIED TO THE TRAFFIC AND TOP FACES OF BARRIER AND TOP FACE OF GUTTER.



OVERHEAD SIGN STRUCTURE
SPAN TYPE (STEEL)
STRUCTURE DETAILS

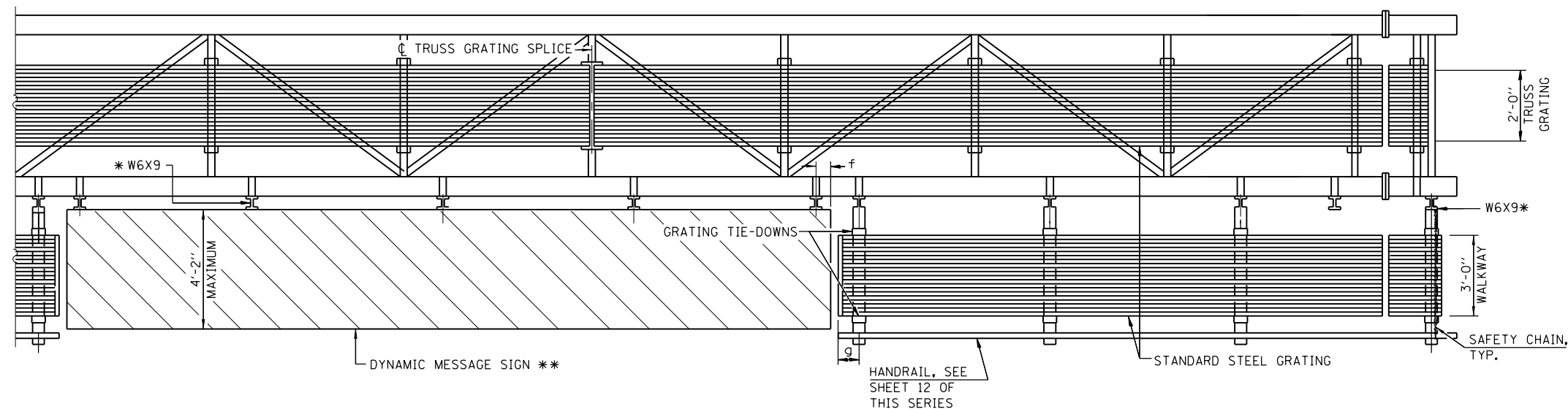
STANDARD F17-05



WALKWAY AND TRUSS GRATING WIDTH DIMENSIONS ARE NOMINAL AND MAY VARY $\pm 1/2$ " BASED ON AVAILABLE STANDARD WIDTHS.

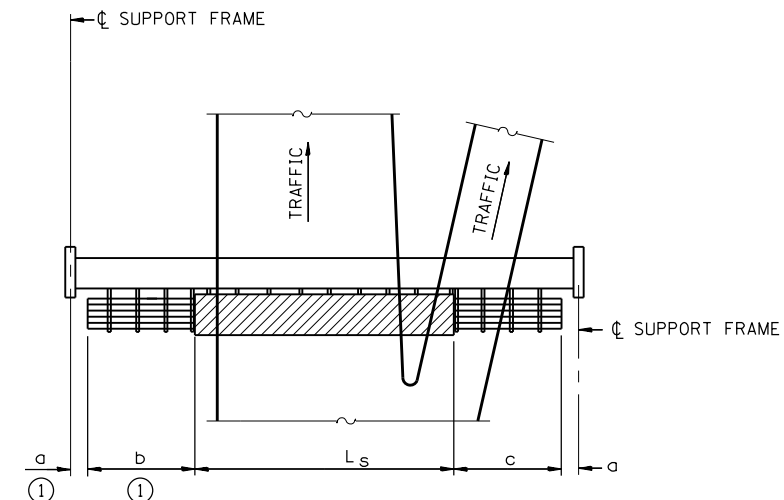
TYPICAL FRONT ELEVATION
WITH HANDRAIL OMITTED FOR CLARITY.

BRACKET AND GRATING DIMENSIONS ARE NOMINAL AND WILL VARY BASED ON ACTUAL DMS DIMENSIONS PLUS MANUFACTURER'S MOUNTING DEVICES.



SECTION F-F

HANDRAIL AND WALKWAY SHALL SPAN A MINIMUM OF THREE BRACKETS BETWEEN SPLICES AND/OR GAP JOINTS. PLACE ALL SIGN AND WALKWAY BRACKETS AS CLOSE TO PANEL POINTS AS PRACTICAL. GRATING AND HANDRAIL SPLICES PLACED AS NEEDED.



PLAN
WALKWAY AND HANDRAIL SKETCH
(ROAD PLAN BENEATH TRUSS VARIES)

BRACKET TABLE

| W6X9 | | |
|--------------|-----------------------|--------------------------|
| SIGN WIDTH | | NUMBER BRACKETS REQUIRED |
| GREATER THAN | LESS THAN OR EQUAL TO | |
| | 8'-0" | 2 |
| 8'-0" | 14'-0" | 3 |
| 14'-0" | 20'-0" | 4 |
| 20'-0" | 26'-0" | 5 |
| 26'-0" | 32'-0" | 6 |

NOTES:

* SPACE W6X9 WALKWAY BRACKETS AND SIGN BRACKETS FOR EFFICIENCY AND WITHIN LIMITS SHOWN:

f = 12" MAXIMUM, 4" MINIMUM (END OF SIGN TO ϕ OF NEAREST BRACKET)

g = 12" MAXIMUM, 4" MINIMUM (END OF WALKWAY GRATING TO ϕ OF NEAREST SUPPORT BRACKET)

h = 6'-0" MAXIMUM (ϕ TO ϕ SIGN AND/OR WALKWAY SUPPORT BRACKETS, W6X9)

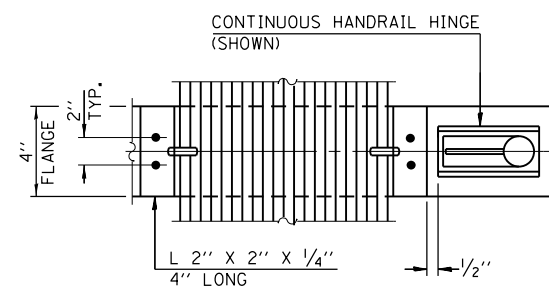
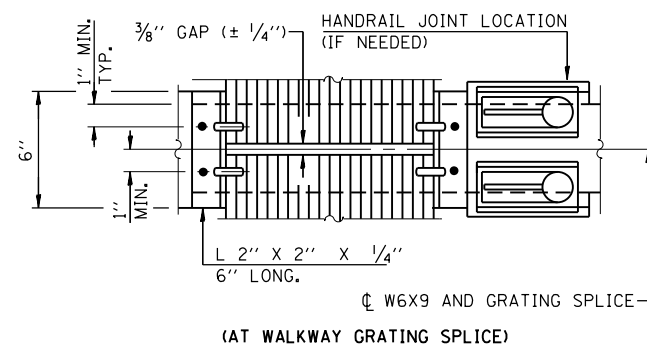
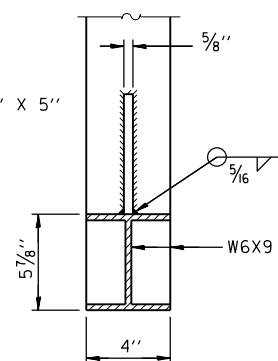
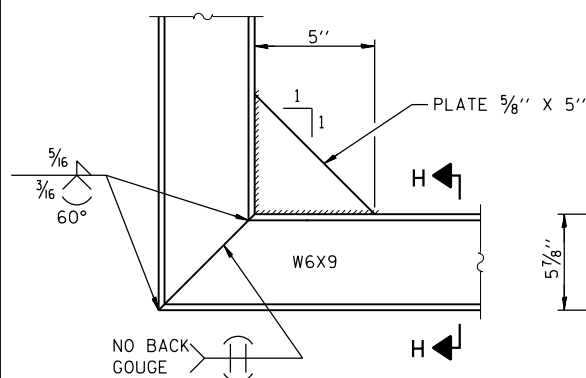
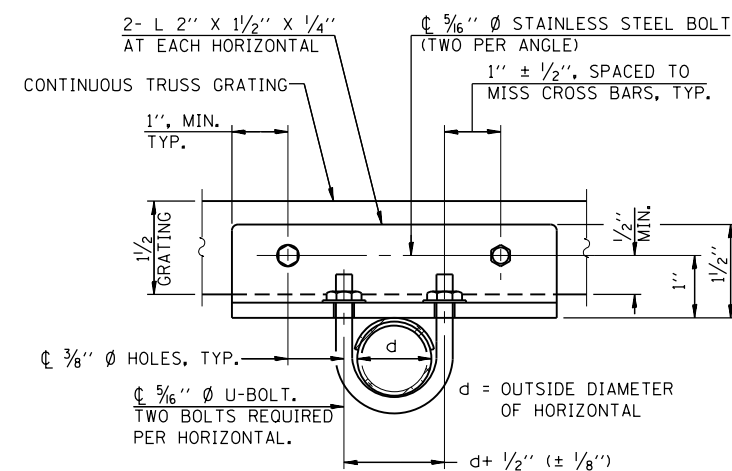
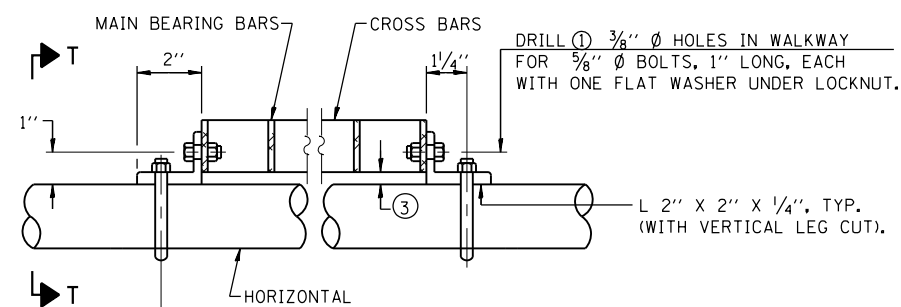
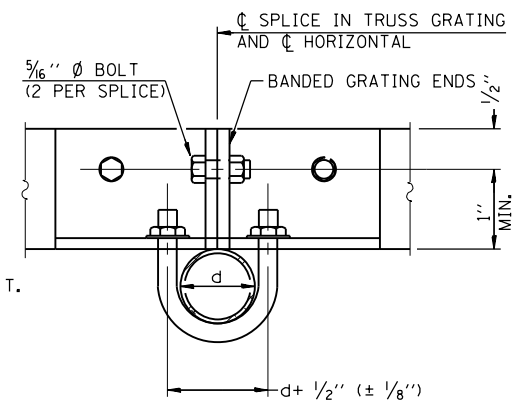
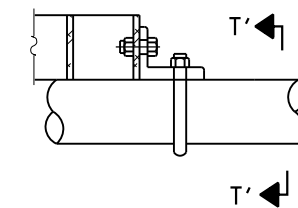
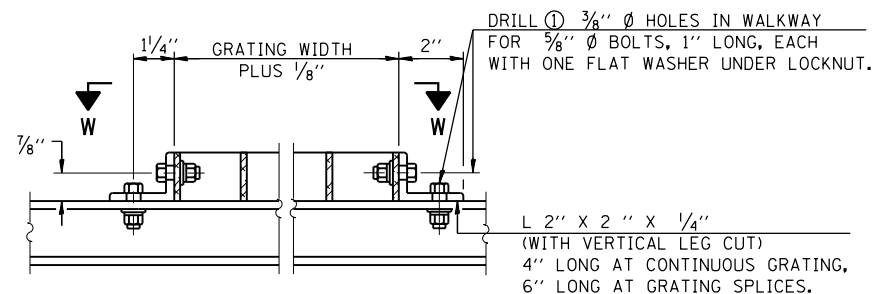
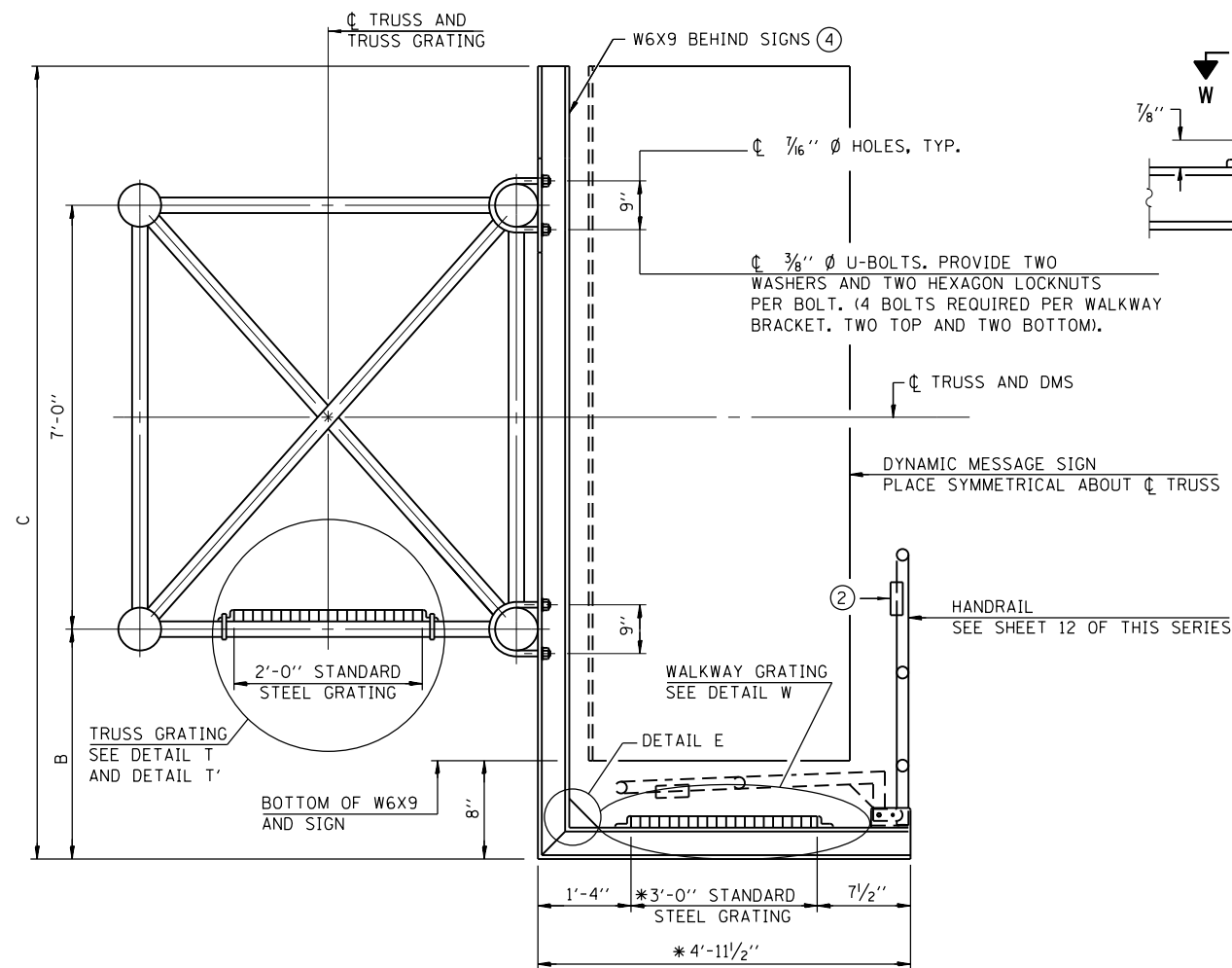
** MAXIMUM DMS WEIGHT = 5000 LBS. 4'-2" MAXIMUM THICKNESS INCLUDES THICKNESS OF DMS TYPE 1 PLUS CONNECTION TO W6X9.

FOR SECTION G-G AND GRATING SPLICE DETAILS, SEE SHEET 11 OF THIS SERIES. FOR HANDRAIL SPLICE DETAILS, SEE SHEET 12 OF THIS SERIES.

TRUSS GRATING TO FACILITATE INSPECTION SHALL RUN FULL LENGTH (CENTER TO CENTER OF SUPPORT FRAMES) ± 12 " ON OVERHEAD TRUSSES.

- ① IF WALKWAY IS REQUIRED LEFT OF THE DMS, a = 1'-6" AND b = WALKWAY LENGTHS. IF WALKWAY IS NOT REQUIRED LEFT OF THE DMS, b = 0 AND "a" IS DIMENSION FROM LEFT SUPPORT FRAME TO LEFT END OF DMS.





NOTES:

- DRILLING HOLES IN GRATING MAY BE DONE IN SHOP OR FIELD, BASED ON CONTRACTOR'S PREFERENCE AND SUBJECT TO ACCURATE ALIGNMENT.
- PL 1/8" X 1/2" X 2" WELDED TO HANDRAIL POSTS TO PROTECT LOCATIONS THAT CONTACT GRATING.
- TUBE TO GRATING GAP MAY VARY FROM 0 TO 1/2", MAX. TO ALIGN WALKWAY, ALLOW FOR CAMBER, ETC.
- DMS MANUFACTURER SHALL DESIGN AND SUPPLY HARDWARE FOR CONNECTION OF DMS TO W6X9. BOLTS SHALL BE STAINLESS STEEL OR HOT DIP GALVANIZED HIGH STRENGTH PER ILLINOIS TOLLWAY SPECIFICATIONS.

* BRACKET AND GRATING DIMENSIONS ARE NOMINAL AND WILL VARY BASED ON ACTUAL DMS DIMENSIONS PLUS MANUFACTURER'S MOUNTING DEVICES.

BARS SIZES FOR STANDARD STEEL GRATING

TRUSS GRATING: MAIN BEARING BARS 3/16" X 1 1/2" ON 1 1/6" CENTERS.
CROSS BARS 3/16" X 1 1/2" ON 4" CENTERS.

WALKWAY GRATING: MAIN BEARING BARS 3/16" X 1 1/2" ON 1 1/6" CENTERS.
CROSS BARS 3/16" X 1 1/2" ON 4" CENTERS.

APPROVED.....
CHIEF ENGINEERING OFFICER

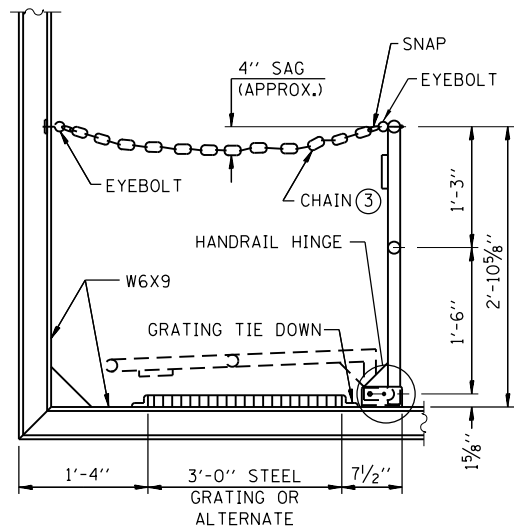
DATE 5-20-2014

SHEET 11 OF 12



OVERHEAD SIGN STRUCTURE
SPAN TYPE (STEEL)
STRUCTURE DETAILS

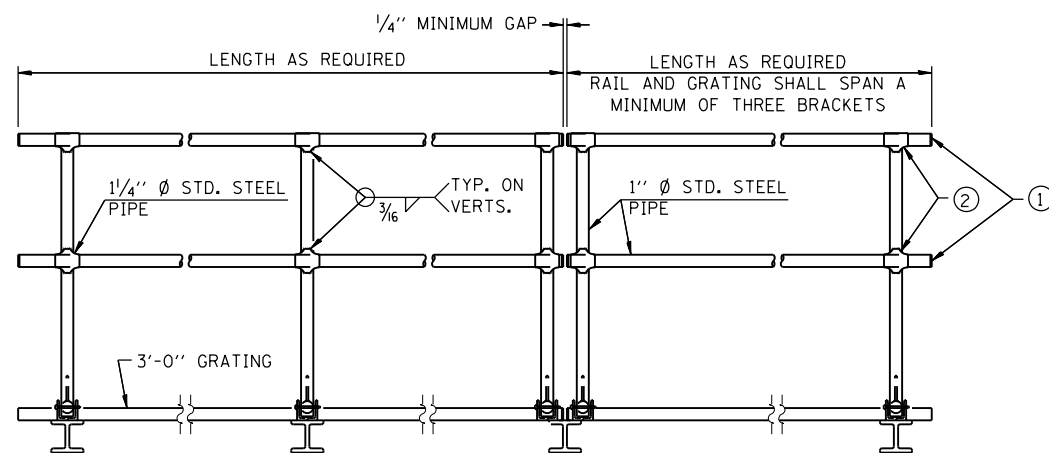
STANDARD F17-05



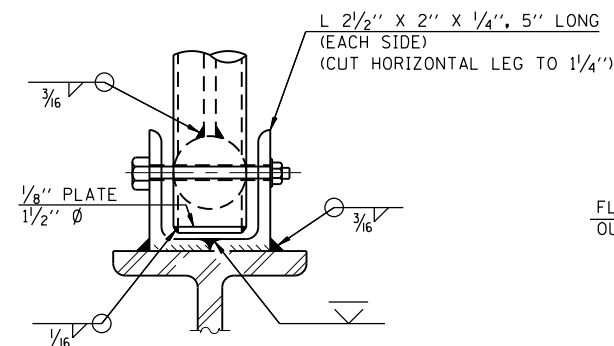
SIDE ELEVATION

(SHOWING SAFETY CHAIN W/O SIGN)

HANDRAIL DETAILS

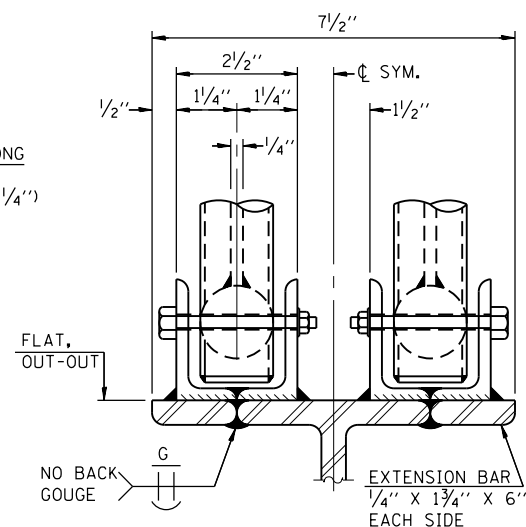


FRONT ELEVATION

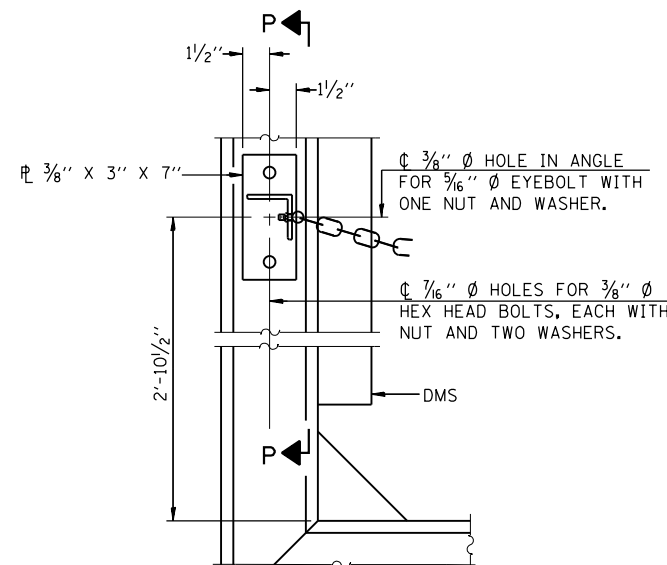


FRONT ELEVATION

SEE "ELEVATION" AT RIGHT FOR DIMENSIONS.



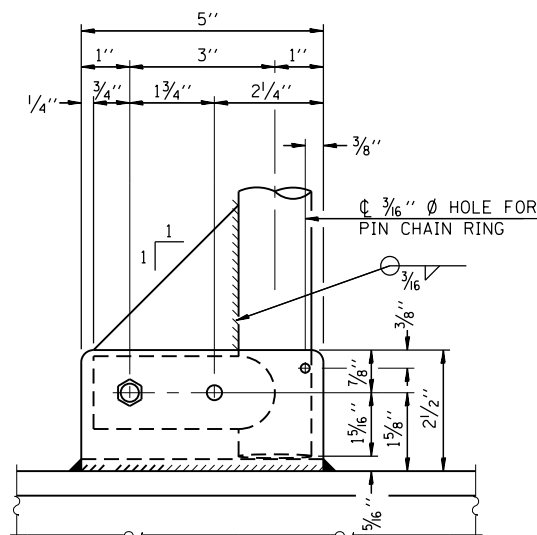
ELEVATION AT HANDRAIL JOINT



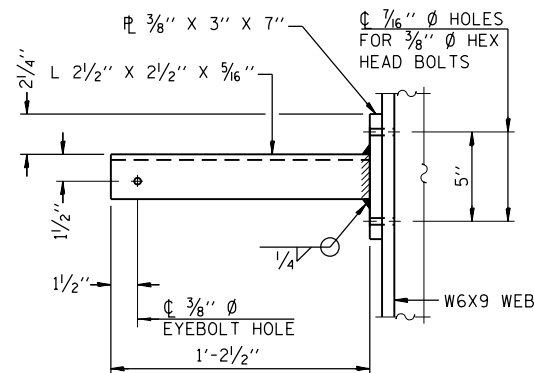
ALTERNATE SAFETY CHAIN ATTACHMENT

(WITH SIGN PRESENT)

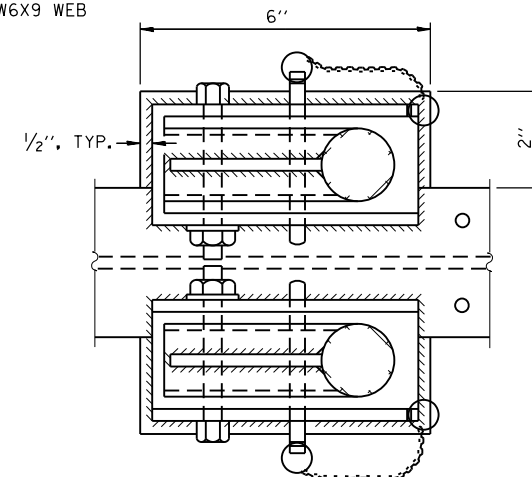
ITEMS NOT SHOWN SAME AS "SIDE ELEVATION" OF "HANDRAIL DETAILS"



SIDE ELEVATION

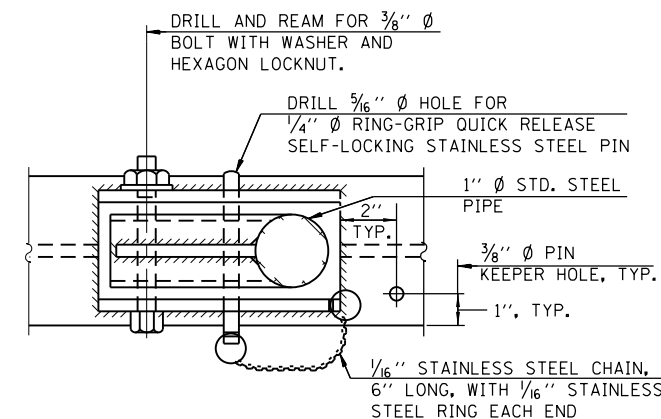


SECTION P-P

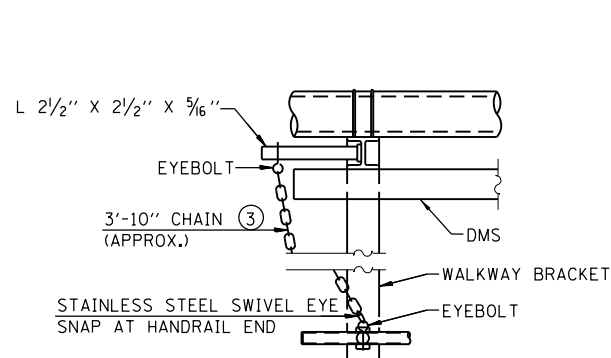


PLAN AT HANDRAIL JOINT

DETAILS NOT SHOWN SAME AS "PLAN"

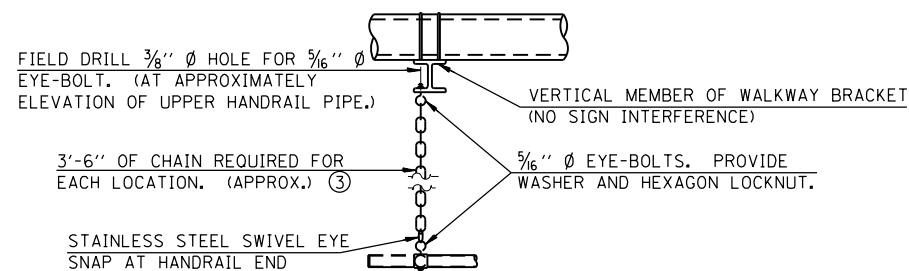


PLAN AT HANDRAIL HINGE



ALTERNATE SAFETY CHAIN ATTACHMENT

DETAILS NOT SHOWN SIMILAR TO "SAFETY CHAIN" DETAILS (WALKWAY OMITTED FOR CLARITY)

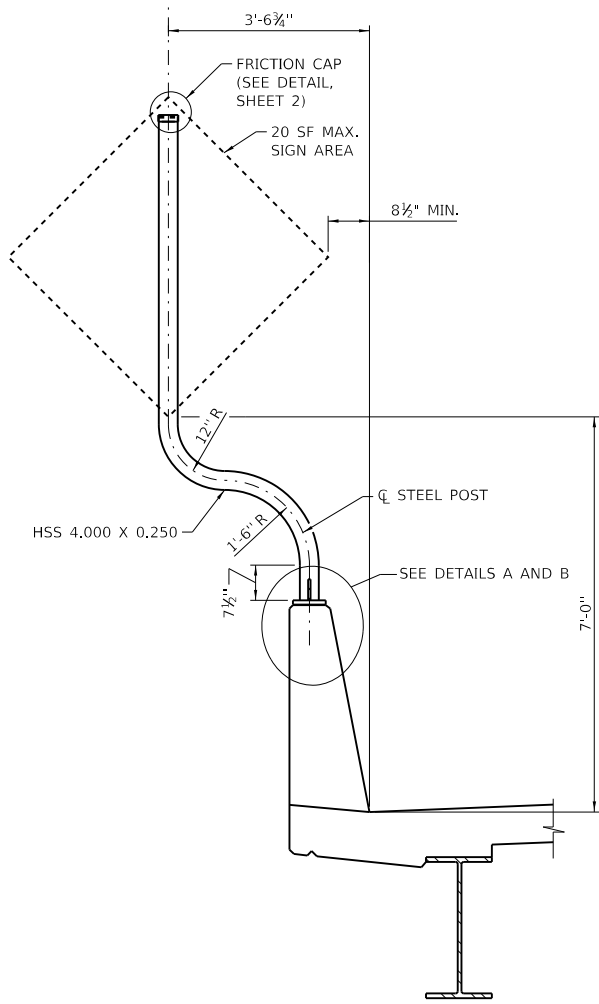


SAFETY CHAIN

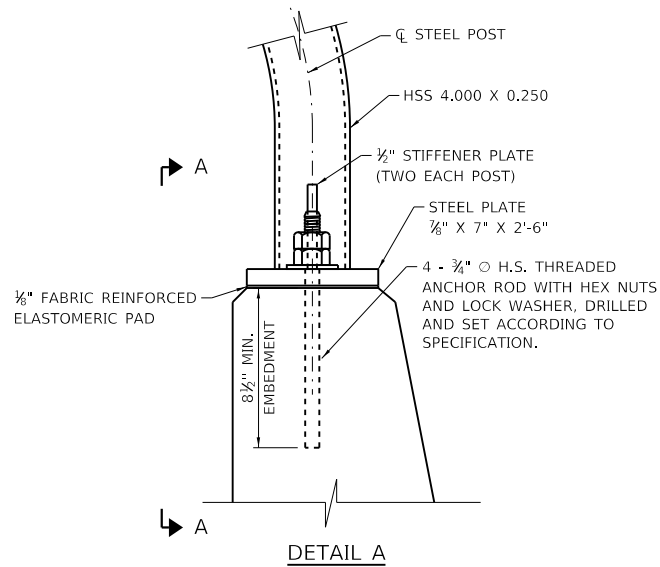
ONE REQUIRED FOR EACH END OF EACH WALKWAY.

NOTES:

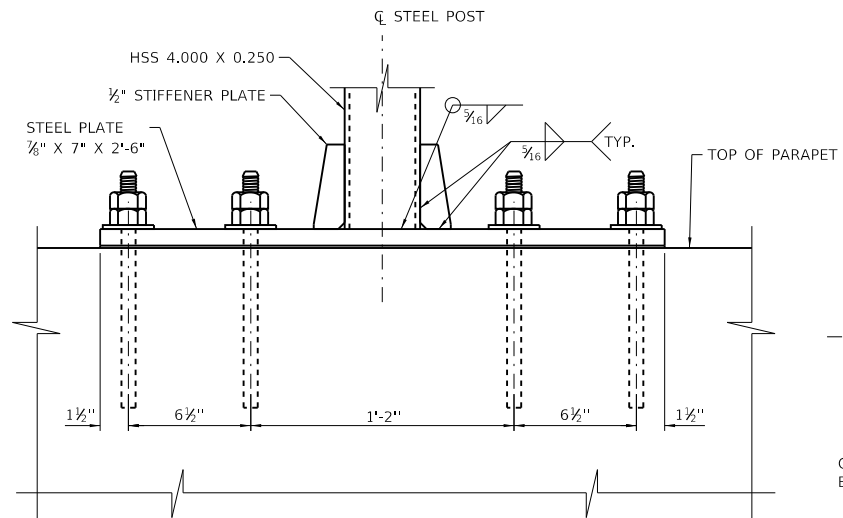
1. INSTALL STANDARD FORCE-FIT END CAPS OR WELD 1/8" END PLATES WITH 1/8" C.F.W. AND GRIND SMOOTH. (ALL RAIL ENDS)
2. HORIZONTAL HANDRAIL MEMBER SHALL BE CONTINUOUS THRU 1 1/4" DIAMETER PIPE. PROVIDE 7/16" DIAMETER HOLE IN 1 1/4" DIAMETER PIPE FOR 3/8" DIAMETER BOLT. FIELD DRILL 1/16" DIAMETER HOLE IN HORIZONTAL RAIL MEMBER. PROVIDE WASHER AND LOCKNUT FOR BOLT. (USE 3/16" EYEBOLTS IN 1/16" DIAMETER HOLES ON TOP RAIL AT ENDS ONLY.)
3. 3/16" TYPE 304L STAINLESS STEEL CHAIN, APPROXIMATELY 12 LINKS PER FOOT.



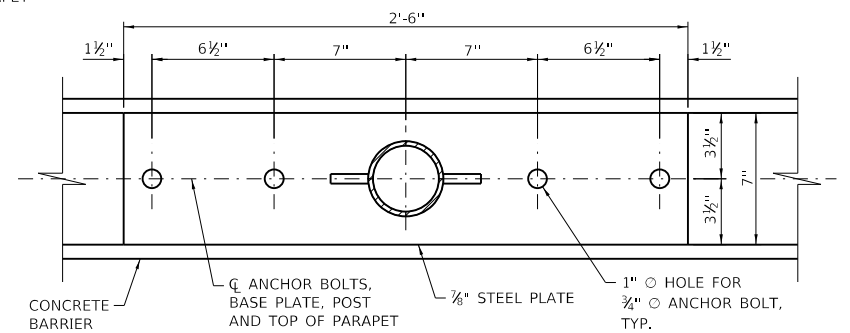
PARAPET MOUNTED SIGN
MAXIMUM SIGN AREA 20 SF



DETAIL A

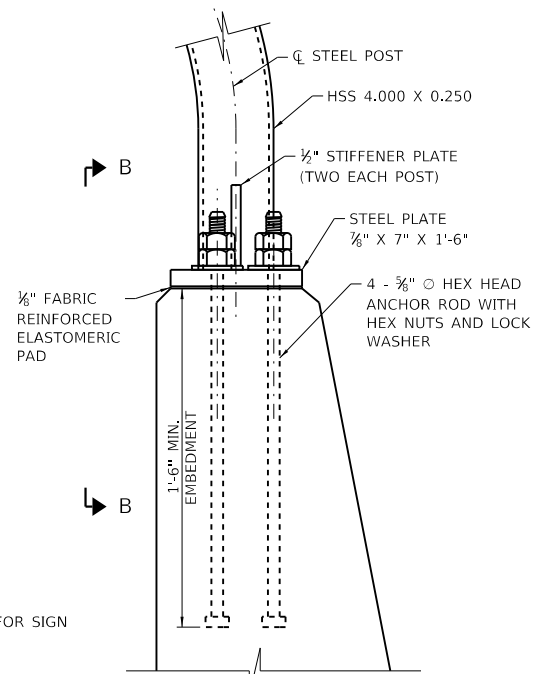


VIEW A-A

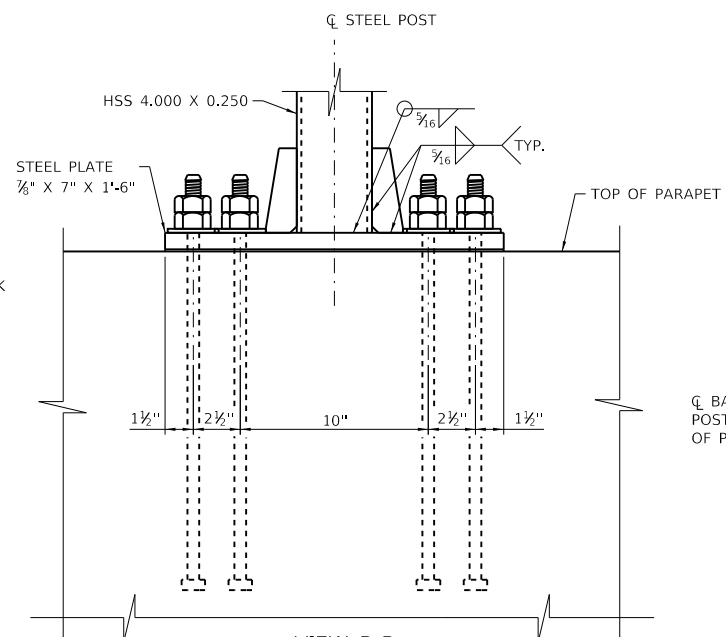


BASE PLATE DETAIL
(FOR POST INSTALLED CONDITION)

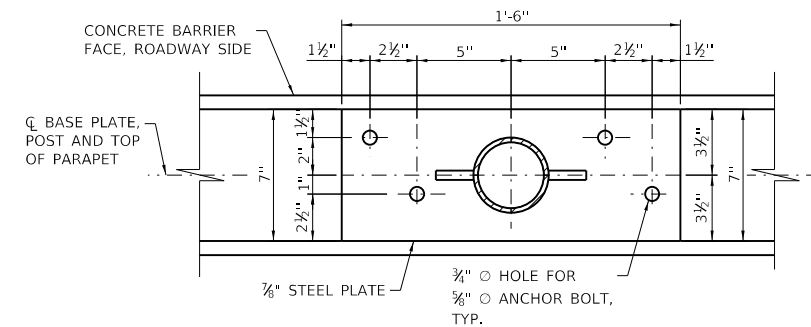
POST INSTALLED DETAIL
(MAXIMUM SIGN AREA 20 SF)



DETAIL B



VIEW B-B

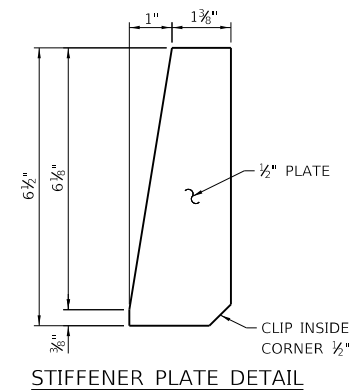


BASE PLATE DETAIL
(CAST-IN-PLACE INSTALLATION)

CAST-IN-PLACE DETAIL
(MAXIMUM SIGN AREA 20 SF)

**MATERIAL SPECIFICATIONS FOR
STRUCTURAL STEEL AND FASTENERS**

| ELEMENTS OF STRUCTURE | MINIMUM YIELD STRENGTH (K.S.I.) | MINIMUM ULTIMATE STRENGTH (K.S.I.) |
|-----------------------|---------------------------------|------------------------------------|
| STRUCTURAL STEEL TUBE | 42 | 58 |
| STEEL ANCHOR BOLTS | 36 | 58 |



STIFFENER PLATE DETAIL

NOTES:

- FOR MATERIAL, FABRICATION, ERECTION, AND OTHER REQUIREMENTS, REFER TO ILLINOIS TOLLWAY "STRUCTURAL SUPPORT FOR SIGN PANELS" SPECIAL PROVISION.
- THESE DETAILS ARE NOT INTENDED FOR PORTABLE AND/OR PRECAST BARRIER.
- DESIGN CONFORMS TO THE 2015 EDITION OF THE AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS WITH 2017 INTERIM SPECIFICATIONS THERETO. DESIGN WIND SPEEDS OF 3-5 GUST WITH SPEED OF 120 MPH PLUS 14% GUST FACTOR, AND A WIND IMPORTANCE FACTOR OF 1.0 (50 YEAR MEAN RECURRENCE INTERVAL) FOR THE SUPPORTING STRUCTURES.
- THE PARAPET WALL SHALL BE DESIGNED TO SAFELY SUPPORT THE PROPOSED SIGN PANELS IN ACCORDANCE WITH NOTE 3.
- WELDED PLATES MAY BE USED IN LIEU OF THE BENT PLATE OF MOUNTING PLATE SHOWN. ALL STEEL ELEMENTS SHALL BE GALVANIZED AFTER FABRICATION.
- EXISTING REINFORCING BARS IN THE CONCRETE STRUCTURES MAY CONFLICT WITH SPECIFIC ANCHOR LOCATIONS. THE CONTRACTOR SHALL LOCATE THE POSITION OF THE REINFORCING BARS AT THE LOCATIONS OF THE CONCRETE ANCHORS. DRILLED HOLES FOR ANCHOR RODS SHALL BE CAREFULLY PLACED TO AVOID INTERFERENCE WITH EXISTING REINFORCEMENT.
- NO ANCHOR BOLT SHALL BE PLACED CLOSER THAN 12" FROM PARAPET WALL EXPANSION JOINT.
- TWO STIFFENER PLATES (ONE ON EACH SIDE OF POST) SHALL BE WELDED AS SHOWN ON PLANS IN DIRECTION PERPENDICULAR TO SIGN.
- INSTALLATION SHALL BE DONE IN ACCORDANCE WITH ILLINOIS TOLLWAY SPECIAL PROVISION "SIGN INSTALLATION".
- THIS STANDARD SHALL BE UTILIZED TO MOUNT SIGN SUPPORT ON SINGLE FACE PARAPETS CONSTRUCTED ON BRIDGES, WALLS AND MOMENT SLABS.

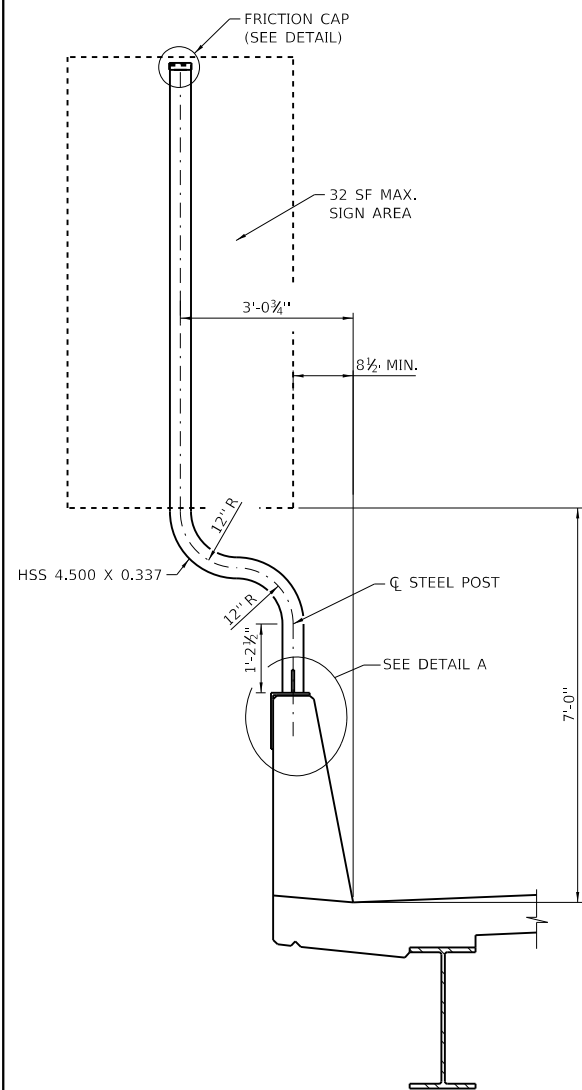
APPROVED: *Paul Kovacs* DATE 12-31-2019
CHIEF ENGINEERING OFFICER

SHEET 1 OF 2

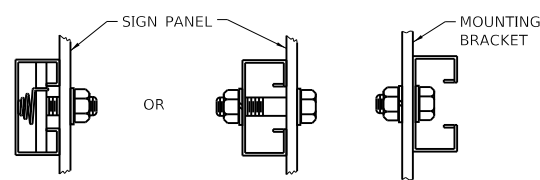


PARAPET MOUNTED
SIGN SUPPORT

STANDARD F18-01

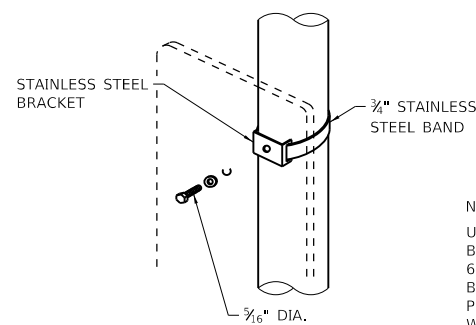


PARAPET MOUNTED SIGN
MAXIMUM SIGN AREA 32 SF



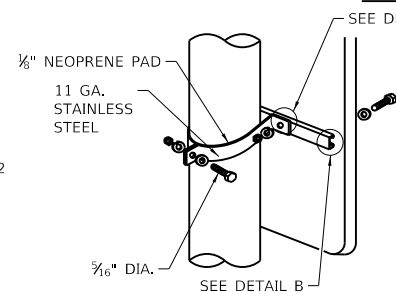
DETAIL B

DETAIL C

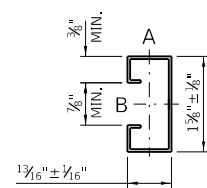


SIGN PANEL 36" WIDE OR LESS

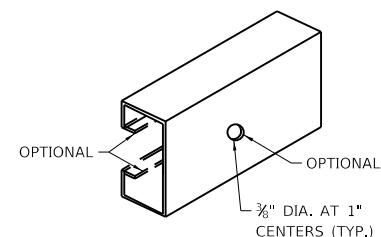
NOTE:
USE A MINIMUM OF 2
BRACKETS (LOCATED
6" FROM TOP AND
BOTTOM OF SIGN)
PER INSTALLATION
WITH MAXIMUM
SPACING OF 3'-0".



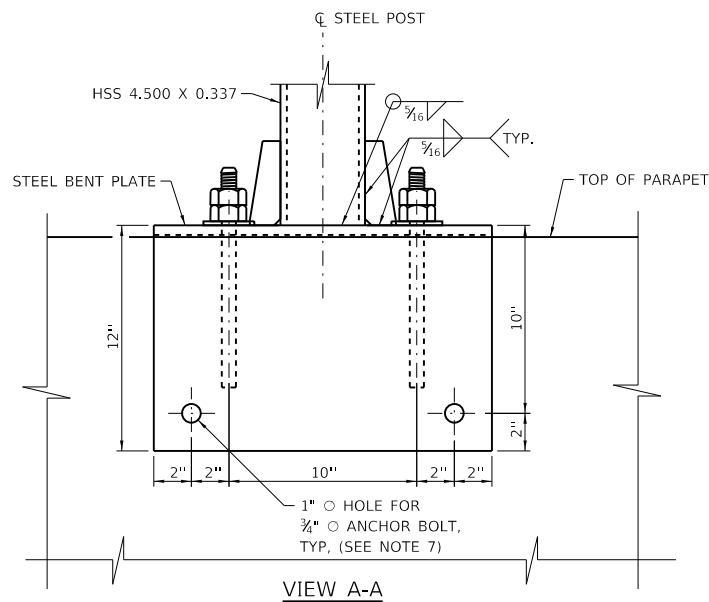
SIGN PANEL OVER 36" WIDE



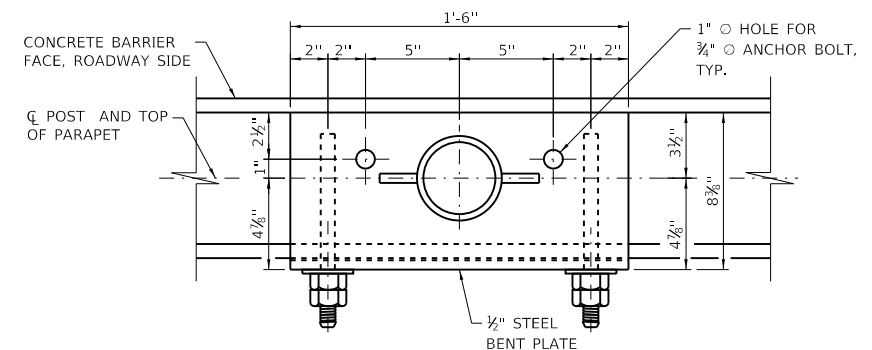
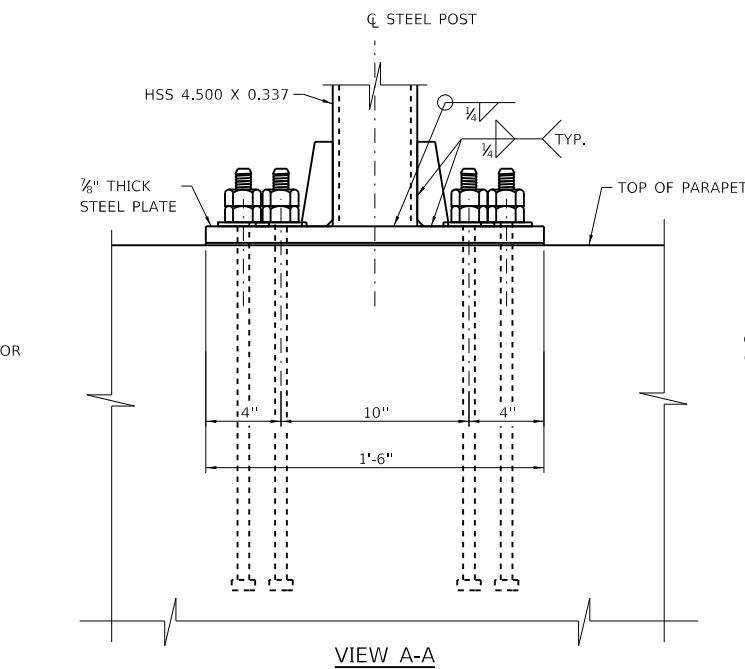
SUPPORTING CHANNEL DETAILS



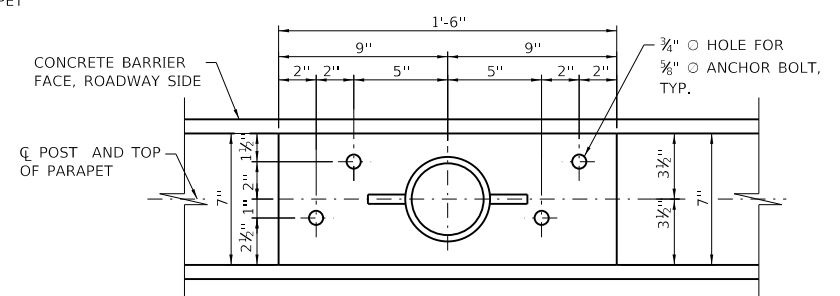
CAST-IN-PLACE DETAIL
(MAXIMUM SIGN AREA 32 SF)



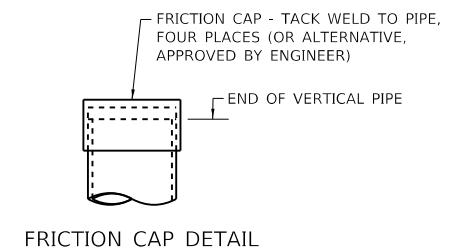
POST INSTALLED DETAIL
(MAXIMUM SIGN AREA 32 SF)



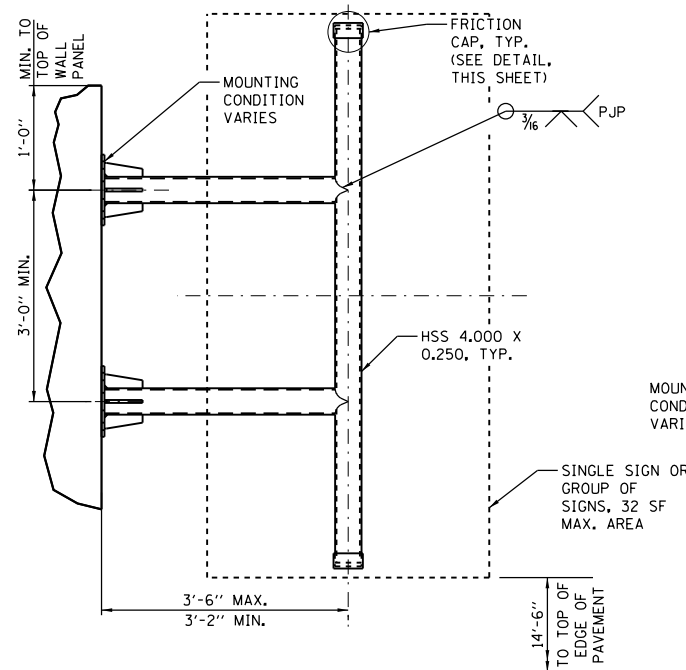
BASE PLATE DETAIL



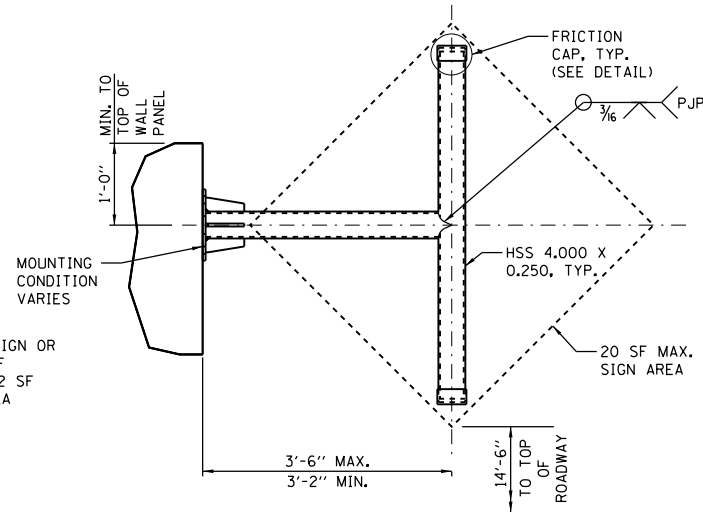
BASE PLATE DETAIL



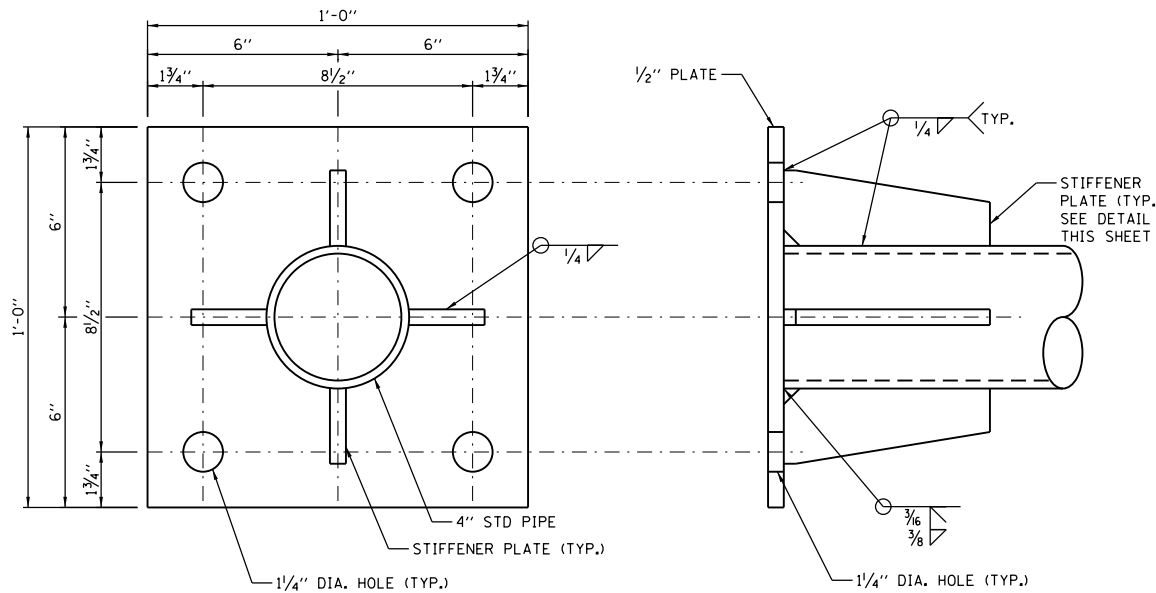
FRICTION CAP DETAIL



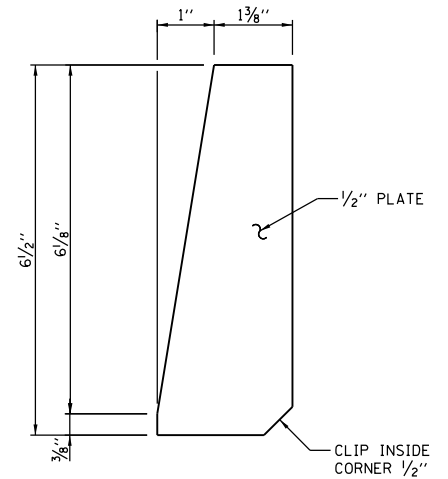
SIGN PANEL MOUNT
(MAXIMUM SIGN AREA 32 SF)



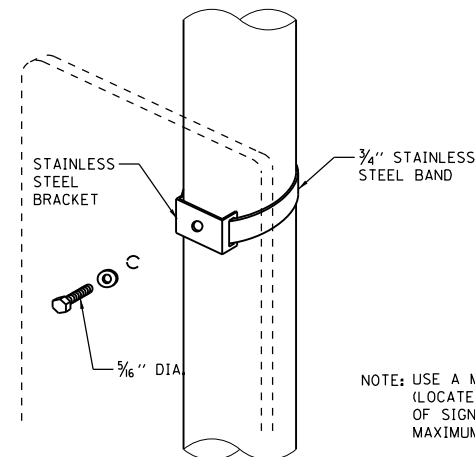
SIGN PANEL MOUNT
(MAXIMUM SIGN AREA 20 SF)



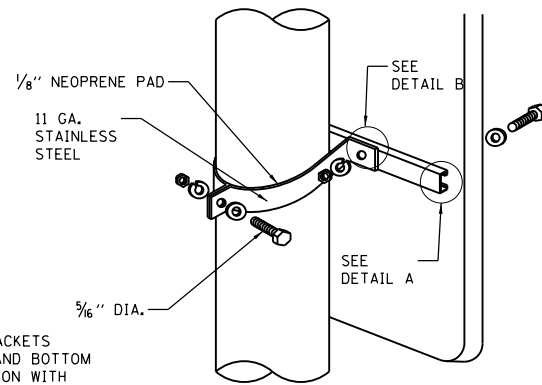
BASE PLATE DETAILS



STIFFENER PLATE DETAIL

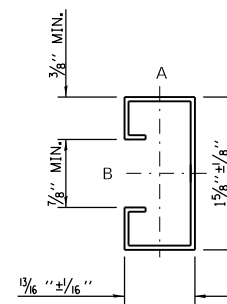


MOUNTING BRACKET DETAIL
(SIGN PANEL 36" WIDE OR LESS)

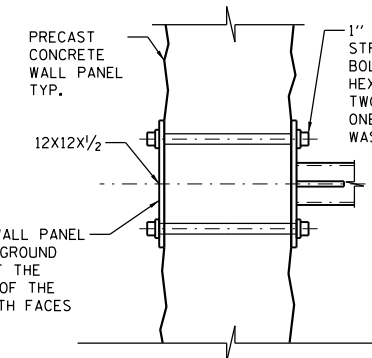
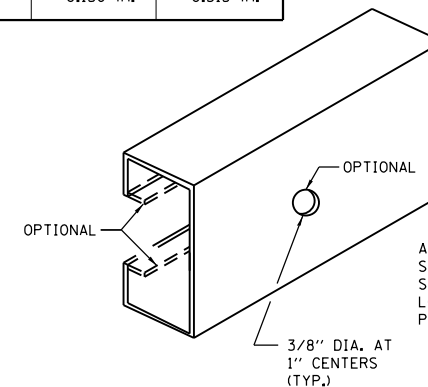


MOUNTING BRACKET DETAIL
(SIGN PANEL OVER 36" WIDE)

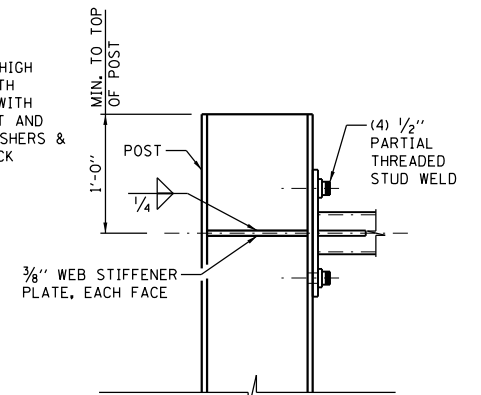
| SUPPORTING CHANNEL SECTION MODULUS (MINIMUM) | Axis A | Axis B |
|----------------------------------------------|-----------------------|-----------------------|
| STEEL | 0.050 in ³ | 0.105 in ³ |
| ALUMINUM | 0.150 in ³ | 0.315 in ³ |



SUPPORTING CHANNEL DETAILS



CONNECTION TO PANEL

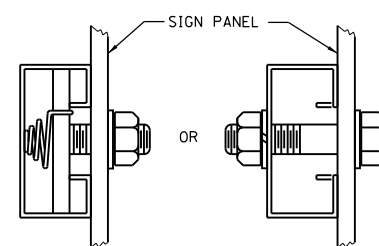


CONNECTION TO POST

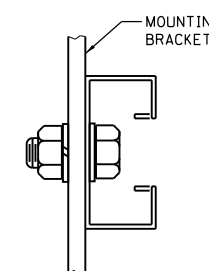
(APPLIES WHERE CONNECTION TO WALL PANEL IS NOT FEASIBLE DUE TO 14'-6" CLEARANCE REQUIREMENT)

NOTES:

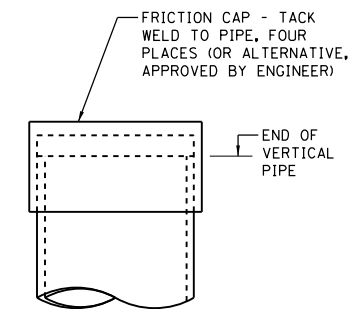
- FOR MATERIAL, FABRICATION, ERECTION, AND OTHER REQUIREMENTS, REFER TO ILLINOIS TOLLWAY "STRUCTURAL SUPPORT FOR SIGN PANELS" SPECIAL PROVISION.
- DESIGN CONFORMS TO THE 2015 EDITION OF THE AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS WITH 2017 INTERIM SPECIFICATIONS THERETO. DESIGN WIND SPEEDS OF 3-5 GUST WITH SPEED OF 120 MPH PLUS 14% GUST FACTOR, AND A WIND IMPORTANCE FACTOR OF 1.0 (50 YEAR MEAN RECURRENCE INTERVAL) FOR THE SUPPORTING STRUCTURES.
- ALL FABRICATION SHALL BE COMPLETE AND READY FOR ASSEMBLY BEFORE GALVANIZING. NO PUNCHING, DRILLING, CUTTING, NOR WELDING SHALL BE PERMITTED AFTER GALVANIZING.
- THE WALL PANELS AND/OR POSTS SHALL BE DESIGNED TO SAFELY SUPPORT THE PROPOSED SIGN PANELS IN ACCORDANCE WITH NOTE 2.
- FOR SIGN CONNECTION TO MOUNTING BRACKET, SHOP DRILL HOLES ON SIGN IN ACCORDANCE WITH THE CURRENT STANDARD HIGHWAY SIGN DESIGNS FOR ILLINOIS. ADDITIONAL HOLE(S) NEEDED TO MEET A STIPULATED TYPE MOUNTING MAY BE FIELD DRILLED.
- ALL THREADED RODS SHALL CONFIRM TO ASTM F1554 GRADE 105, EACH WITH ONE PLATE WASHER AND LOCKNUT AND BE HOT DIP GALVANIZED PER ASTM A153 (AASHTO M232). THEY SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 1211 OF ILLINOIS TOLLWAY SUPPLEMENTAL SPECIFICATIONS TO THE IDOT STANDARD SPECIFICATIONS.
- A NYLON WASHER SHALL BE PLACED BETWEEN THE SIGN FACE AND ANY OTHER WASHER REQUIRED ON SIGNS CONSTRUCTED OF ASTM TYPE III OR IV SHEETING.
- CONTRACTOR SHALL VERIFY APPLICABLE FIELD DIMENSIONS BEFORE FABRICATION. HOLES DRILLED THROUGH NOISE ABATEMENT WALL SHALL BE DRILLED WITH ROTARY (CORING OR MASONRY DRILL) TYPE EQUIPMENT. PERCUSSION (STAR) DRILLING SHALL NOT BE ALLOWED.
- CENTER LINE OF BOLTS INTO NOISE ABATMENT WALL SHALL BE AT LEAST 12" TO CENTER LINE OF OPEN JOINT IN WALL.



DETAIL A



DETAIL B



FRICTION CAP DETAIL

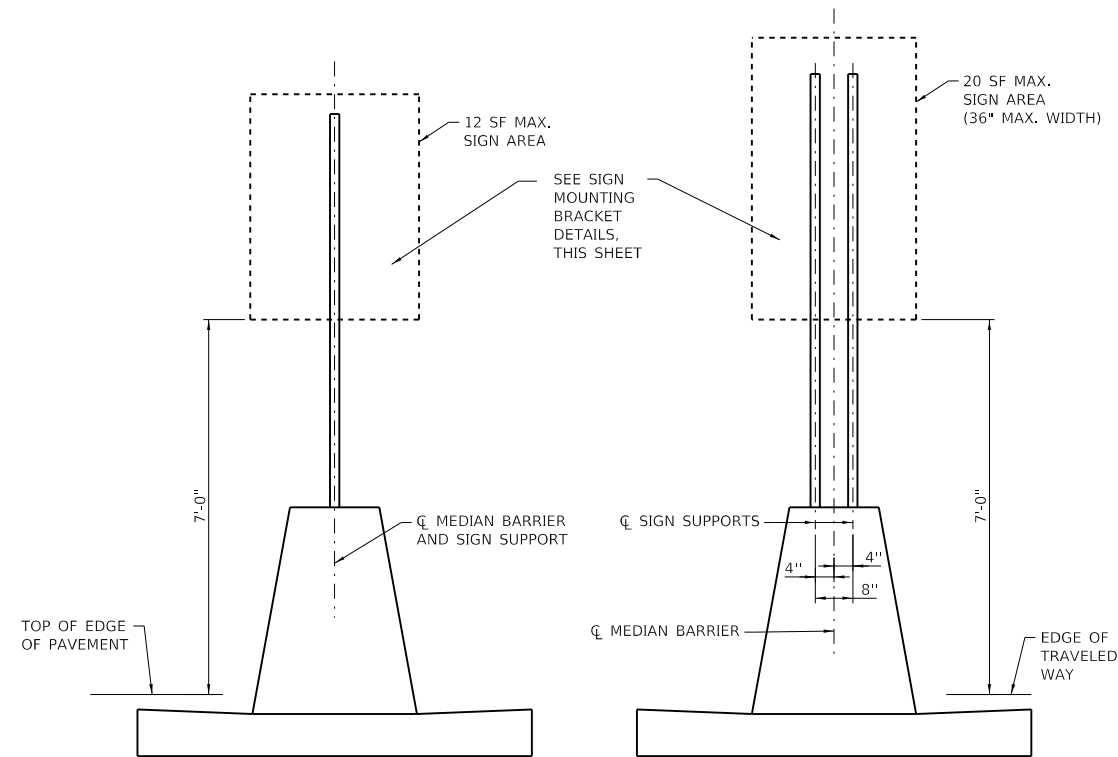
APPROVED: *Paul Kovacs* DATE 12-31-2019
CHIEF ENGINEERING OFFICER

SHEET 1 OF 1

Illinois Tollway

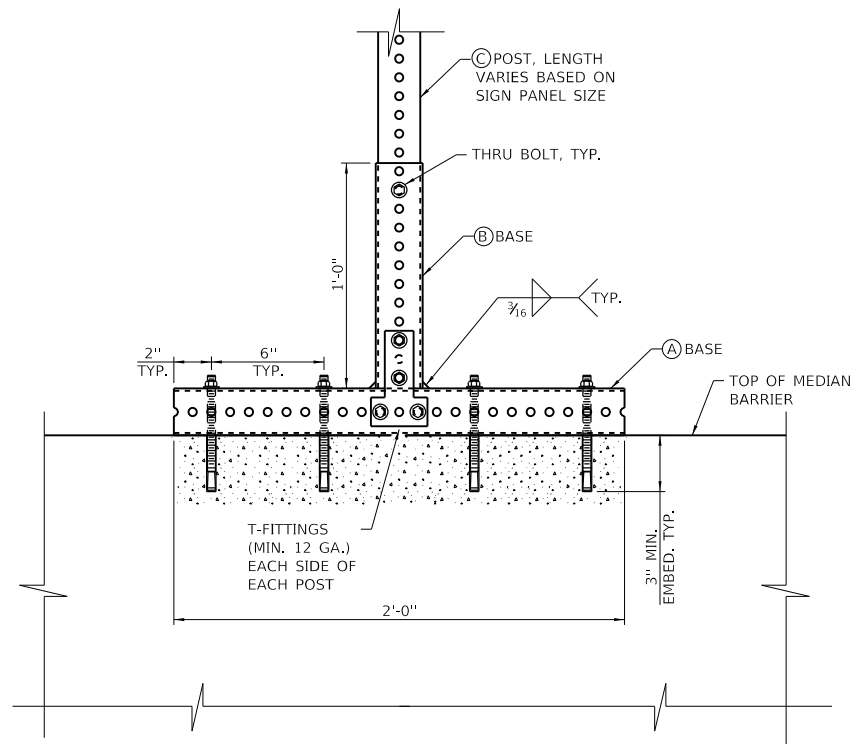
NOISE ABATEMENT WALL MOUNTED SIGN SUPPORT

STANDARD F19-01



ONE POST INSTALLATION

TWO POSTS INSTALLATION



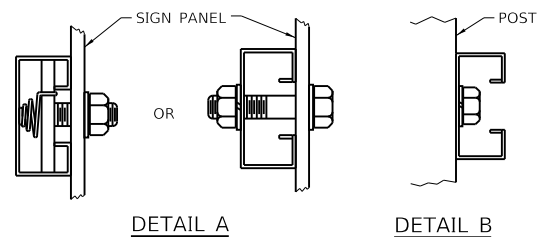
SIDE ELEVATION - BARRIER MOUNT DETAIL
(LOOKING PERP. TO TRAFFIC)

NOTES:

- ALL ANCHOR BOLTS FOR MEDIAN BARRIER MOUNTED SIGN SUPPORT ASSEMBLY SHALL BE $\frac{3}{8}$ " DIA. EXPANSION ANCHORS.
- THE TOP SECTION SHALL BE TELESOPED INTO THE BASE SECTION 12 INCHES AND FASTENED TOGETHER.
- DESIGN CONFORMS TO THE 2015 EDITION OF THE AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS WITH 2017 INTERIM SPECIFICATIONS THERETO. DESIGN WIND SPEEDS OF 3-s GUST WITH SPEED OF 120 MPH PLUS 14% GUST FACTOR, AND A WIND IMPORTANCE FACTOR OF 1.0 (50 YEAR MEAN RECURRENCE INTERVAL) FOR THE SUPPORTING STRUCTURES.
- NO ANCHOR BOLT SHALL BE PLACED CLOSER THAN 12" FROM CENTER LINE OF MEDIAN BARRIER JOINT.
- SIGN FABRICATION AND INSTALLATION SHALL BE DONE IN ACCORDANCE WITH ILLINOIS TOLLWAY SPECIAL PROVISION "SIGN INSTALLATION".
- BASE AND POST ASSEMBLY SHALL BE HOT DIP GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH AASTHO M111 OR AS SPECIFIED IN THE SPECIAL PROVISION "TELESCOPING STEEL SIGN SUPPORT, BARRIER ASSEMBLY".
- ALL MATERIALS FOR THE SIGN SUPPORT ASSEMBLY SHALL BE INCLUDED IN THE COST OF "TELESCOPING STEEL SIGN SUPPORT, BARRIER ASSEMBLY".

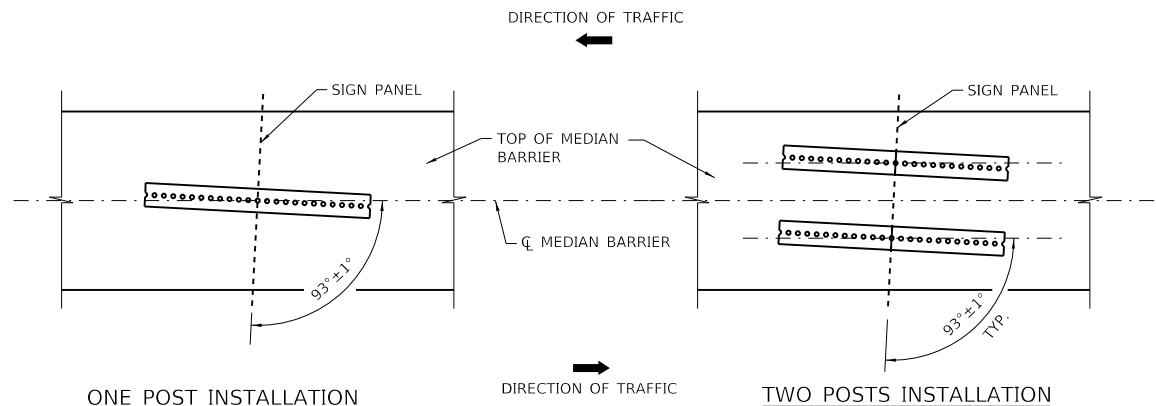
MEMBER DETAILS

| | |
|-----|-----------------------------|
| (A) | 2½" x 2½" x 1'-0" (12 GA.) |
| (B) | 2½" x 2½" x 1'-0" (12 GA.) |
| (C) | 2¼" x 2¼" x VARIES (12 GA.) |



DETAIL A

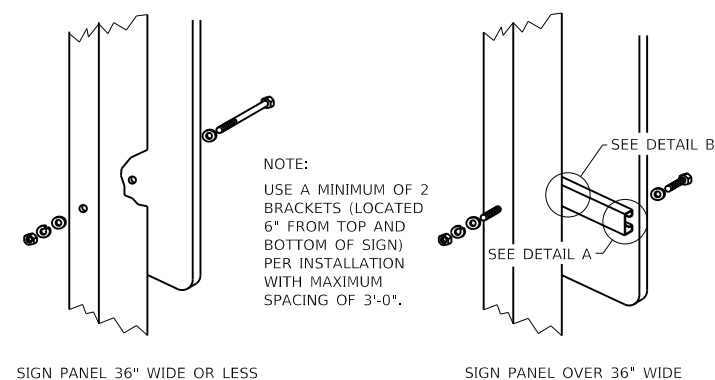
DETAIL B



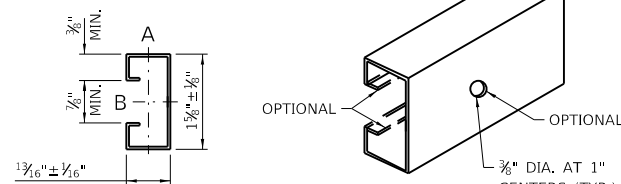
ONE POST INSTALLATION

TWO POSTS INSTALLATION

PLAN VIEW



MOUNTING BRACKET DETAILS



SUPPORTING CHANNEL DETAILS

| SUPPORTING CHANNEL SECTION MODULUS (MINIMUM) | Axis A | Axis B |
|----------------------------------------------|------------------------|------------------------|
| STEEL | 0.050 in. ³ | 0.105 in. ³ |
| ALUMINUM | 0.150 in. ³ | 0.315 in. ³ |